



STATE OF RHODE ISLAND
Department of Administration
DIVISION OF STATEWIDE PLANNING



COMPREHENSIVE PLAN CONFORMANCE REPORT

Date: March 5, 2026

Name: Chase Marina Redevelopment Project

Location: 169 Riverside Drive, Tiverton

Requested by: Rhode Island Department of Environmental Management

Introduction

In conformance with R.I. Gen. Laws § 45-22.2-10(g) and pursuant to Part 1.4.12(B) of the “Rules and Standards of the State Planning Council,” 670-RICR-00-00-1, the Rhode Island Department of Environmental Management (“RIDEM”) has requested a State Planning Council review of a proposed redevelopment project for the Chase Marina (“the Project”), a property owned by the RIDEM and located in the Town of Tiverton (“the Town”), for its consistency with the Town’s *Comprehensive Plan*.

Statewide Planning Review Process & Criteria

The Rhode Island Division of Statewide Planning (“RIDSP”) received a request from the RIDEM to review the Project for conformity with the Tiverton *Comprehensive Plan*. RIDSP reviewed material provided by the RIDEM and reviews by the Army Corps of Engineers (“ACOE”) and the Rhode Island Coastal Resources Management Council (“CRMC”) (see Appendices) to make a determination of conformity.

Review Criteria:

Pursuant to subsection 45-22.2-10(g), programs and projects of State agencies are required to conform to State approved comprehensive plans unless the agency demonstrates to the Council that:

1. The program, project, or facility conforms to the stated goals, findings, and intent of the Comprehensive Planning and Land Use Act [“the Act”];
2. The program, project, or facility is needed to promote or protect the health, safety, and welfare of the people of Rhode Island;
3. The program, project, or facility is in conformance with the relevant sections of the State Guide Plan; and whether
4. The program implementation, project, or size, scope, and design of the facility varies as little as possible from the comprehensive plan of the municipality.

In addition, information provided by the Town indicates that the Town considers the proposal to be consistent with its *Comprehensive Plan*, which was approved by the RIDSP in 2017.

State Planning Council Review

RIDEM is proposing the redevelopment of the former Chase Marina in the Town of Tiverton. The Town raised concerns regarding compliance with the legal holding in *Town of Exeter v. State*, 226 A.3d 696 (R.I. 2020). In their request (see Appendices), RIDEM states that "... for purposes of clarity and to ensure there are no delays in the proposed project, RIDEM has requested a State Planning Council review of the proposed redevelopment project for the Chase Marina property located along the waterfront in Tiverton for its consistency with the Town's Comprehensive Plan."

This finding of conformity, prepared by DSP staff, was submitted to the State Planning Council ("Council") for review on March 5, 2026. The Council has been asked to vote as to whether it concurs with the report at its meeting on March 12, 2026.

Project Description and Background

The Project site was purchased by the State in 2006 for use as a laydown yard for construction of the new Sakonnet River Bridge. The Project will be constructed on three lots owned by the State, the areas of which are:

Plat 303, Lot 126- 0.14 Acre

Plat 303, Lot 127- 0.25 Acres

Plat 303, Lot 128- 0.26 Acres

Total: 0.65 acres, or approximately 28,300 square feet

According to the CRMC Decision Worksheet (see Appendices), there have been buildings and shoreline protections in place at the site since 1939, based on aerial photographs. The site's primary use has been as a marina dating from at least 1988, based on historic aerials and permits, until approximately 2008.

RIDEM proposes repairs and improvements to the Project site, including the installation of fixed and floating piers for seventeen (17) commercial fishing vessels, the construction of timber pile fencing, and the installation of a pump-out stub.

An existing on-site building, a two-story structure of 5,671 square feet, will also be renovated as part of the Project. Most of the building will be leased to Dune Brothers Seafood for loading and unloading seafood products and the operation of a seafood market and restaurant. There will not be any on-site seafood processing. There will also be office space for marina operations and eighteen (18) on-site parking spaces.

According to the CRMC Decision Memo, the application for the Project expands the existing Marina Perimeter Limit (MPL) slightly to the south and west to maintain safe fairway distances and navigation within the facility. RIDEM also plans to fill in an unused railway basin to increase parking in the area. The CRMC Decision Worksheet also states that

...[t]he proposed slip count increases by one vessel from sixteen (16) total vessels to seventeen (17) total vessels. New docks and piles are proposed to be installed within the

revised MPL.

Another scope of the project involves filling in tidal waters at an existing derelict marine railway basin to add additional parking spaces for marina patrons and reinforce the shoreline. Additional work to be included: repairs/filling of rip rap slopes, installation of a sheet pile bulkhead, and construction of timber walkways and docks, and reinforcement of existing granite block walls.

Engagement with the Town

RIDEM attended four Tiverton Harbor Commission meetings in 2024 and 2025 to discuss the Project. The State Port Manager also met on several occasions about the project with both the current and former Town Administrators. RIDEM officials attended a Tiverton Planning Board meeting in December 2025 to present and receive feedback on the Project.

Conformity Analysis

Statewide Planning staff reviewed the materials submitted for the criteria outlined in R.I. Gen. Laws § 45-22.2-10(g) and pursuant to Part 1.4.12(B) of the “Rules and Standards of the State Planning Council,” 670-RICR-00-00-1. However, as outlined below in the conformity conclusion, these criteria are only applicable if the proposal conflicts with the Town’s *Comprehensive Plan*. Statewide Planning staff have found that the proposal is consistent with the Town of Tiverton *Comprehensive Plan*, and therefore these review criteria and findings are for informational purposes only.

1. Project’s conformance to the stated goals, findings, and intent of the Comprehensive Planning and Land Use Act:

R.I. Gen. Law § 45-22.2-3 sets forth the intent and goals of the *RI Comprehensive Planning and Land Use Act*. RIDEM’s submission to the RIDSP states that the Act’s relevant goals include:

- Goal 2: To promote an economic climate which increases quality job opportunities and overall economic well being of each municipality and the state (§ 45-22.2-3(c)(2)).
- Goal 7: To promote consistency of state actions and programs with municipal comprehensive plans, and provide for review procedures to ensure that state goals and policies are reflected in municipal comprehensive plans and state guide plans (§ 45-22.2-3(c)(7)).
- Goal 9: To ensure that municipal land use regulations and decisions are consistent with the comprehensive plan of the municipality, and to ensure state land use regulations and decisions are consistent with state guide plans (§ 45-22.2-3(c)(9)).

RIDEM’s submission also states that “the proposed project will advance each of the aforementioned goals by supporting economic development and developing currently underutilized land for active maritime use.”

Based on the documentation submitted, Statewide Planning staff find that the proposed Chase Marina Redevelopment advances the goals and intent of the Rhode Island Comprehensive Planning and Land Use Act. The project reinforces marine-dependent economic activity, preserves working waterfront infrastructure, and remains consistent with Tiverton’s designated waterfront planning framework. Environmental protections have been addressed through coastal regulatory approval.

2. Project is needed to promote or protect the health, safety, and welfare of the people of Rhode Island:

The Act has as one of its goals (Goal 1):

To promote orderly growth and development that recognizes the natural characteristics of the land, its suitability for use, the availability of existing and proposed public and/or private services and facilities, and is consistent with available resources and the need to protect public health, including drinking water supply, drinking water safety, and environmental quality (§ 45-22.2-3(c)(1)).

The Project is also intended to support Rhode Island’s working waterfront and marine-based economy, improving the welfare of the people of the state.

Based on the information provided, the project reasonably promotes the health, safety, and welfare of the people of Rhode Island by strengthening working waterfront infrastructure, supporting economic stability, enhancing environmental protections, and reinforcing regulatory oversight.

3. Project’s conformance with the relevant sections of the State Guide Plan:

The RIDEM submission states that the commercial marina and restaurant “will provide much-needed berthing infrastructure for commercial fishing vessels and to promote marine-dependent commercial activity that relies on direct access to the waterfront which is consistent with the Rhode Island State Guide Plan. (...) The marina’s design and public access elements align with these state policies to support boating, tourism, and recreational use of coastal waters.”

The proposed Chase Marina Redevelopment is generally consistent with relevant elements of the State Guide Plan. The project supports marine-dependent economic activity and supports working waterfront infrastructure, aligning with the goals and policies of Rhode Island’s Comprehensive Economic Development Strategy, *Ocean State Accelerates*.

Ocean State Outdoors: Rhode Island’s Comprehensive Outdoor Recreation Plan (2019) (State Guide Plan Element 152) includes the goal of “Meeting Critical Needs...Improve Recreation Opportunities and Resources Conservation” with policies to:

- Preserve and expand recreational boating opportunities
- Preserve and expand public access to the shoreline
- Provide a diverse, balanced system offering quality recreational opportunities that meet user needs. Strengthen and expand community-based recreation facilities and open spaces to meet close-to-home needs.
- Promote Rhode Island’s open space and recreation resources to tourists, while protecting the quality and stability of the resource base.
- Strengthen the professional capacity for recreation, conservation and open space planning at all levels.

4. The project, or size, scope, and design of the facility varies as little as possible from the comprehensive plan of the municipality

A letter dated January 29, 2026, from Town Administrator Patrick Jones to Tricia Quest, Interim Assistant Director, Chief Legal Counsel, RIDEM, (see appendices) indicates that the Town “expects the State Planning Council to find that this project conforms with the Town's Comprehensive Community Plan.”

The Town’s Comprehensive Plan identifies the Sakonnet River waterfront, where the proposed project is located, as an area intended to support marine-dependent commercial activity and preserve the community’s maritime character. Specifically, the proposed project is located within the Waterfront-Related Uses future land use designated area, which the Town’s Comprehensive Plan defines as, “the Waterfront-Related future land use designation includes certain non-residential and mixed-use areas along the Sakonnet River for which primarily water-dependent commercial uses are appropriate and rely on waterfront access for business.” (p. 50). The continuation and enhancement of marina operations, commercial fishing berthing, and seafood-related commerce are consistent with the Future Land Use and Economic Development elements of the Comprehensive Plan.

The project represents reinvestment in an existing marina site rather than a change in land use classification or expansion into residential or undeveloped areas. The size and scope, including fixed and floating piers, marina operations, and renovation of an existing building for marine-supportive uses, appear consistent with the intent of the Waterfront district to accommodate water-dependent commercial activity.

Conformity Conclusions

Based upon the material provided by the Rhode Island Department of Environmental Management and RIDSP staff's analysis of the Project, RIDSP finds that the following determinations have been made:

1. The project conforms to the stated goals, findings, and intent of the Comprehensive Planning and Land Use Act;
2. The project is needed to promote or protect the health, safety, and welfare of the people of Rhode Island;
3. The project is in conformance with the relevant sections of the State Guide Plan; and
4. The project, or size, scope, and design of the facility is consistent with the comprehensive plan of the municipality.

Conclusion

RIDSP staff have reviewed the above-mentioned items submitted as part of this review and have determined that the proposal does not conflict with the Town of Tiverton Comprehensive Plan. Accordingly, no formal finding is required. Pursuant to R.I. Gen. Laws § 45-22.2-10(g) and pursuant to Part 1.4.12(B) of the "Rules and Standards of the State Planning Council," 670-RICR-00-00-1, State Planning Council action is necessary only if the proposal is not in conformance with the Comprehensive Plan.

This comprehensive plan conformance review and all additional data are on file at the Division of Statewide Planning. Please call (401) 222-6496 or email meredith.brady@doa.ri.gov.



MEREDITH E. BRADY
Secretary, State Planning Council

cc: Roberta Groch, AICP- Assistant Chief
Caitlin Greeley, AICP- Supervising Planner



RHODE ISLAND
DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF THE DIRECTOR
235 Promenade Street, Room 425
Providence, Rhode Island 02908

January 13, 2026

Meredith Brady
Associate Director, Statewide Planning
Secretary, State Planning Council
235 Promenade Street
Suite 230, Providence, RI 02908

RE: Request for Project Review
RIDEM Chase Marina Redevelopment
169 Riverside Drive, Tiverton, RI

I. Introduction

The Rhode Island Department of Environmental Management (“RIDEM”) has been in discussions with the Town of Tiverton regarding a proposed redevelopment project for Chase Marina. The Town raised concerns regarding compliance with the legal holding in Town of Exeter v. State, 226 A.3d 696 (R.I. 2020). As such, for purposes of clarity and to ensure there are no delays in the proposed project, RIDEM consistent with R.I. Gen. Laws § 45-22.2-10(g) and pursuant to Part 1.4.12(B) of the *Rules and Standards of the State Planning Council, 670-RICR-00-00-1*, respectfully requests a State Planning Council review of the proposed redevelopment project for the Chase Marina property located along the waterfront in the Town of Tiverton for its consistency with the Town’s Comprehensive Plan.

II. The Proposed Project

Pursuant to R.I. Gen. Laws § 46-1-1 “[t]he director of [RIDEM] shall have control and supervision of all harbors and tidewaters of the state, for the preservation, development, and improvement thereof and the promotion of the interests of the state therein.” Consistent therewith, RIDEM’s proposed redevelopment project involves comprehensive improvements to both the waterfront and upland portions of the site , which is intended to support Rhode Island’s working waterfront and marine-based economy. The proposed redevelopment includes the installation of fixed and

floating piers designed to accommodate up to twenty commercial fishing vessels, the construction of nautical timber pile fencing to enhance the aesthetic character of the marina, and the installation of a pump-out stub to support environmentally responsible marina operations. In addition, the existing on-site building will be renovated to include office space necessary for marina operations, with the majority of the building leased to Dune Brothers Seafood for loading and unloading of seafood products (with no on-site processing) and the operation of a seafood market and restaurant. There will be 18 on-site parking spaces with additional parking needs proposed to be accommodated by state-owned parking areas nearby. R.I. Gen. Laws § 45-22.2-3 sets forth the intent and goals of the RI Comprehensive Planning and Land Use Act, R.I. Gen. Laws § 45-22.2 *et seq.* (the “Act”). Specifically, the Act states its goals include:

- Goal 1: To promote orderly growth and development that recognizes the natural characteristics of the land, its suitability for use, the availability of existing and proposed public and/or private services and facilities, and is consistent with available resources and the need to protect public health, including drinking water supply, drinking water safety, and environmental quality. § 45-22.2-3(c)(1).
- Goal 2: To promote an economic climate which increases quality job opportunities and overall economic well being of each municipality and the state. § 45-22.2-3(c)(2).
- Goal 7: To promote consistency of state actions and programs with municipal comprehensive plans, and provide for review procedures to ensure that state goals and policies are reflected in municipal comprehensive plans and state guide plans. § 45-22.2-3(c)(7).
- Goal 9: To ensure that municipal land use regulations and decisions are consistent with the comprehensive plan of the municipality, and to ensure state land use regulations and decisions are consistent with state guide plans. § 45-22.2-3(c)(9).

RIDEM believes the proposed project will advance each of the aforementioned goals by supporting economic development and developing currently underutilized land for active maritime use. RIDEM also believes that the project will be consistent with both the State Guide Plan and the Town of Tiverton’s Comprehensive Plan, discussed in further detail below.

III. State Guide Plan

The proposed commercial marina and associated restaurant is specifically designed to provide much-needed berthing infrastructure for commercial fishing vessels and to promote marine-dependent commercial activity that relies on direct access to the waterfront which is consistent with the Rhode Island State Guide Plan and the Rhode Island Department of Environmental Management (RIDEM) Strategic Plan FY2024–FY2026 by advancing long-range economic

development, environmental protection, and outdoor recreation policies that are established as State policy guidance. R.I. Gen. Laws §42-11-10(d).

As an adopted element of the Guide Plan, *Ocean State Outdoors: Rhode Island's Comprehensive Outdoor Recreation Plan* includes clear direction for coastal recreation, including the goal of “Meeting Critical Needs...Improve Recreation Opportunities and Resources Conservation” with policies to:

- Preserve and expand recreational boating opportunities
- Preserve and expand public access to the shoreline
- Provide a diverse, balanced system offering quality recreational opportunities that meet user needs. Strengthen and expand community-based recreation facilities and open spaces to meet close-to-home needs.
- Promote Rhode Island’s open space and recreation resources to tourists, while protecting the quality and stability of the resource base.
- Strengthen the professional capacity for recreation, conservation and open space planning at all levels. (State Guide Plan Element 152, Goals and Policies)

The marina’s design and public access elements align with these state policies to support boating, tourism, and recreational use of coastal waters.

Additionally, the proposed commercial marina and associated restaurant would be consistent with the [RIDEM Strategic Plan FY2024–FY2026](#) by advancing multiple strategic goals set forth by the Department that guide environmental stewardship, community access, and sustainable economic activity. Specifically, the Strategic Plan includes a dedicated goal to “promote and sustain outdoor recreation in Rhode Island,” recognizing that recreational and commercial access to natural settings contributes to community health, quality of life, and local economies by attracting visitors and supporting jobs. By integrating environmental protection practices with amenities that enhance visitor experience—such as the restaurant for patrons and enhanced boating access—the project directly aligns with RIDEM’s mission to “protect, restore, manage, and promote Rhode Island’s environment and natural resources to preserve and improve our quality of life,” thereby supporting state priorities for sustainable waterfront development that balances economic, recreational, and ecological concerns.

IV. Municipal Comprehensive Plan and Zoning

The proposed project is consistent with the Town of Tiverton’s Comprehensive Plan for Waterfront zones, which are intended to accommodate non-residential and mixed-use development along the Sakonnet River where water-dependent commercial activities are appropriate and reliant on waterfront access. Waterfront zoning districts are intended to support marine-dependent commercial uses on appropriately sized lots along the river.

Additionally, the proposed project site is located within a W2 Waterfront Zoning District under the Town of Tiverton Zoning Ordinance (<https://www.axisgis.com/tivertonri/>). Uses permitted in this zoning district include restaurants without entertainment, commercial docks or piers, marinas and boatyards, and the storage, repair, and sale of boats and marine accessories. The proposed marina operations, commercial dock facilities, and restaurant and seafood market uses are consistent with these allowable uses.

RIDEM is of the opinion that the proposed Chase Marina redevelopment is consistent with the Town of Tiverton's Comprehensive Plan, Land Use Regulations, and Zoning Ordinance. The Comprehensive Plan describes Tiverton's waterfront as home to "marine-dependent businesses, including boat sales, suppliers and a number of boatyards and marinas along Riverside Drive and Main Road" and notes that the establishment of a Waterfront District provides a regulatory means to encourage marine-dependent uses that can enhance both the local economy and preserve the Town's scenic mix of residential and maritime traditions. Comprehensive Community Plan, Section 10.3.2: Fishing and Marine-Dependent Industries, p. 129. The proposed project advances the purpose of the waterfront zoning district by promoting water access, supporting water-dependent commercial uses, and encouraging local marine-based businesses.

The Town's Comprehensive Plan notes that the establishment of the Waterfront District provides a regulatory means to encourage marine-related uses along the Sakonnet River shoreline in a manner that enhances the local economy and preserves the Town's maritime traditions. Comprehensive Plan, Economic Development Narrative, p. 154. Additionally, the Town's Comprehensive Plan's *Future Land Use* element directs long-range land use decisions by illustrating desired patterns of development and recognizing waterfront zones as areas where commercial and marine-oriented uses should be supported consistent with community goals. Comprehensive Plan, Future Land Use, pp. 50–51. This policy framework reflects the Town's intent that appropriately sized waterfront parcels be used for activities such as marinas, commercial docks, and other water-dependent services that provide public water access, support local marine-based businesses, and contribute to the overall economic vitality of Tiverton. Taken together, these provisions demonstrate that a commercial marina use within the Waterfront (W2) Zone is not only permitted under local zoning but also directly aligned with the Town's Comprehensive Plan's vision for promoting sustainable waterfront commerce and strengthening Tiverton's maritime economy.

The proposed office space at the Chase Marine site in Tiverton is envisioned as a functional and supportive component of the new marine infrastructure being developed at the property. Its primary purpose is to provide on-site workspace for RIDEM's Division of Law Enforcement, facilitating effective oversight and ensuring that all applicable rules and regulations governing

site use are consistently adhered to. The space would also be used by RIDEM’s Division of Coastal Resources for similar regulatory and operational functions associated with coastal and marine management at the site. In this respect, the office use is consistent with the type of administrative and operational space commonly found at private marina facilities and should be viewed as an accessory use directly tied to marina operations. RIDEM believes the proposed office use is fully compatible with the intended and customary use of the property.

Municipal and Public Outreach

RIDEM has engaged in extensive and ongoing coordination with the Town of Tiverton regarding the Chase Marina project. This outreach has included multiple appearances by the State Port Manager (head of RIDEM’s Division of Coastal Resources) at publicly noticed Harbor Commission meetings to discuss the project and respond to questions. The outreach included documented correspondence with the Town of Tiverton. On September 4, 2025, RIDEM sent a letter to the Town Administrator, which described the project, its scope and the importance of the project. The letter invited the Town to provide feedback on the proposed project as well as request a formal presentation of the project from RIDEM. Subsequently, the State Port Manager, together with senior RIDEM leadership from the Bureau of Natural Resources and the Division of Capital Development, has participated in a Tiverton Planning Board meeting to provide further project context and receive feedback. Beyond these public forums, the State Port Manager has also met on several occasions with both the current and former Town Managers, reflecting RIDEM’s continued commitment to transparent communication and collaboration with municipal leadership throughout project development. The Table below outlines the public meetings and dates attended by RIDEM.

Meeting	Date
Harbor Commission	2/22/2024
Harbor Commission	2/5/2025
Harbor Commission	3/17/2025
Harbor Commission	9/22/2025
Tiverton Planning Board	12/6/2025

RIDEM received CRMC approval for the proposed project in December 2025. (<http://www.crmcpermitdatabase.org/PADS/CrmcApp/DetailsApp/133793918>). The Tiverton Harbor Commission provided a letter of endorsement for the proposed project which was submitted as part of the application to CRMC (attached).

V. Conclusion

The Chase Marina project is a priority for RIDEM and an investment into the future of waterfront economic development in the State. RIDEM has analyzed the project for consistency with the Act, the State Guide Plan, the RIDEM Strategic Plan, as well as the Town of Tiverton's Comprehensive Plan and Zoning Ordinance. It is the opinion of RIDEM that the project will advance the goals and intentions of each of these Acts and Plans.

RIDEM believes the proposed project represents a significant investment in Tiverton's working waterfront and respectfully submits this memorandum for review in accordance with the Rules and Standards of the State Planning Council (670-RICR-00-00-1).

TOWN OF TIVERTON, RHODE ISLAND

OFFICE OF TOWN ADMINISTRATOR

VIA E-MAIL AND U.S. MAIL

January 29, 2026

Tricia Quest, Esq.
Interim Assistant Director,
Chief Legal Counsel
Rhode Island Department
of Environmental Management
235 Promenade Street
Providence, RI 02908

Re: Chase Marina, Tiverton, Rhode Island

Dear Attorney Quest:

I write concerning the status of your Department's development of the Chase Marina in Tiverton. Last Fall I had the opportunity to meet with several members of the Department's development team after they had already presented the project to the Town's Harbor Commission. Following my meeting, the Department presented its project to the Town's Planning Board at its December 2, 2025 meeting.

The Department's responses to inquiries by the Planning Board raised concerns about the project generally, and specifically about the Department's intent to follow the required approval process. The concerns about the Department's intent to comply with the necessary approval process were recently reinforced by questions posed to the Town Planner by the Statewide Planning Office.

Allow me to pause for a moment and say that the Town looks forward to working with the Department on this project and in particular addressing the Town's concerns, such as the limited available parking.

Concerning the approval process, I am advised by the Town's Solicitor that development of the project requires approval from the State Planning Council as well as the Town's Planning Board. Compliance with this procedure is undisputed and was recently confirmed in the matter of Town of Exeter v. State, 226 A.3d 696 (R.I. 2020).

As you are aware, pursuant to Exeter, when pursuing a project in a municipality, the Department is required to have the project reviewed by the State Planning Council for a determination of whether the proposed project complies with the municipality's comprehensive community plan. Exeter, 226 A.3d at 703-04. See also, R.I. Gen. Laws § 45-22.2-10(g). The Town, which has a valid comprehensive community plan, has not received notice of any presentation or hearing on this project before the Statewide Planning Council and a review of

recent agenda items does not include reference to this project.¹ This is concerning since the DEM has petitioned the State Properties Committee for certain authority, received an assent from CRMC, and has issued, and granted, an RFP to Dune Brothers to operate a restaurant at the site. Further, this step of approval was brought to the attention of the Department's development team at the December 2, 2025 Planning Board meeting. It appears that the Department is seeking authority for this project from every entity but those it is absolutely required to petition – the State Planning Council and the Town.

While the Town expects the State Planning Council to find that this project conforms with the Town's Comprehensive Community Plan, there are aspects of the project which do not comply with the Town's current Zoning Ordinances. Per Exeter, the Department is not exempt from the local zoning ordinances and is required to seek relief from the municipality. Exeter, 226 A.3d at 705. See also, R.I. Gen. Laws § 45-22.2-13(c). Again, this was brought to the attention of the Department's development team at the Planning Board meeting in December 2025.

The Town understands that the Chase Marina project will include the following uses: 1) small fish processing operation;² 2) DEM enforcement office/Harbormaster office; 3) restaurant and 4) a marina and commercial marina. The Chase Marina is located in Tiverton's Waterfront W2 Zoning District. Following is a list of those uses and whether permitted under Tiverton's Zoning Ordinance:

1. Small fish processing operation: Use Art. IV, Section 3(h) Fish and shellfish processing and storage for retail sales, Not Permitted;
2. DEM enforcement officer/Harbormaster Office: Use Art. IV, Section 4(h) Municipal or Government Building, Not Permitted;
3. Restaurant: Use Art. IV, Section 8(a)(1) Restaurant, Not Including Entertainment, Permitted by Right; with entertainment Section 8(a)(2), Special Use Permit. A restaurant is also subject to performance standards in Art. VI, Section 10, and if there is entertainment it is also subject to special use criteria of Art. XVI, Section 5.
4. Marina or Boat Yard, Use Art. IV, Section 9(m) is Permitted by Right, as is a Commercial Marina, Use Art. IV, Section 11(f). Both are subject to performance standards in Art. VI, Section 10.

¹ The local municipality is to receive notice of that application. See section 1.3.5(B) of the State Planning Council regulations.

² It is unclear what the small fish processing operation constitutes. At the December 2, 2025 Planning Board meeting it was represented that this use was limited to loading and unloading product. If that this the extent of the operation, it may not be considered a fish processing operation.

Further, this project likely constitutes a major land development project subject to review under the Tiverton Land Development and Subdivision Regulations and by the Planning Board.

Moving forward, the Town expects to receive notice from the State Planning Council of DEM's petition for this project and further expects the Department to coordinate with the Town Administrative Officer/Planner to identify what relief is required from the Town's land use ordinances and regulations, as well as what application is required to obtain that relief. I also invite you to contact our Assistant Solicitor to the Planning Board, Todd J. Romano, if you wish to discuss the approval process and the Exeter decision.

Please also be aware that should the Department continue to pursue this project without obtaining the requisite Town approvals, that the Town will take all necessary measures to halt the project until DEM obtains to required approvals.

Thank you in advance for your anticipated cooperation and we look forward to working with the Department on this project.

Very truly yours,



Patrick W. Jones

PATRICK W. JONES
Town Administrator

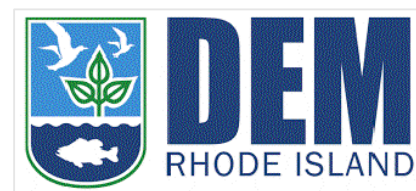
cc: Tiverton Town Council
William P. Gerlach, Tiverton Planning Board, Chair
Jonathan Womer, Director, Rhode Department of Administration
Peter F. Neronha, Rhode Island Attorney General
Senator Walter S. Felag, Jr.
Senator Louis P. DiPalma
Representative John G. Edwards
Representative Michelle E. McGaw
Tiverton Solicitors

CHASE MARINA

169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

100% DESIGN
DECEMBER, 2025

PREPARED FOR:



RHODE ISLAND DEPARTMENT
OF ENVIRONMENTAL MANAGEMENT
301 GREAT ISLAND ROAD
NARRAGANSETT, RHODE ISLAND

DESIGNED BY:



GZA GEOENVIRONMENTAL, INC.
188 VALLEY ST. SUITE 300
PROVIDENCE RHODE ISLAND, 02909

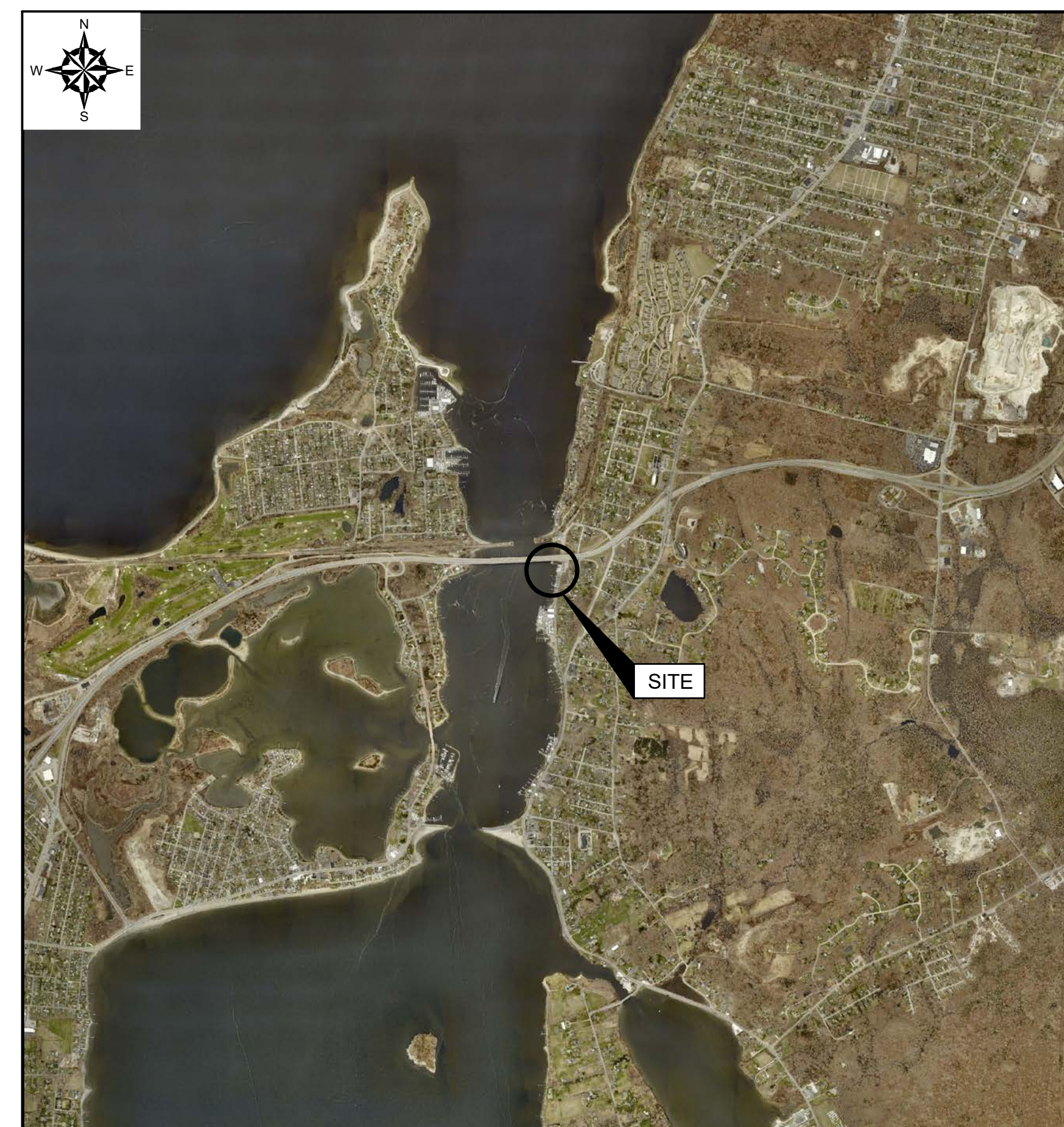
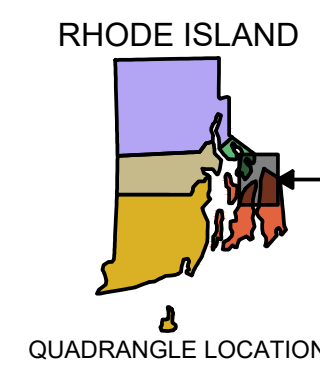


PROJECT LOCUS MAP



SOURCE: USGSSTORE.GOV
BASE MAP FROM THE FOLLOWING USGS QUADRANGLE MAP:
TIVERTON, RHODE ISLAND (2021), FALL RIVER, MASSACHUSETTS (2021)
DIGITAL TOPOGRAPHIC MAPS PROVIDED BY USGSSTORE.GOV.

CONTOUR ELEVATIONS REFERENCE NAVD 88.
CONTOURS ARE SHOWN IN FEET AT 10 FOOT INTERVALS.



PROJECT VICINITY MAP



BASE MAP DEVELOPED FROM RIGIS AERIAL IMAGERY
PUBLISHED IN APRIL 2019.

INDEX OF DRAWINGS	
SHEET No.	SHEET TITLE
1	COVER SHEET AND INDEX OF DRAWINGS
2	GENERAL NOTES
3	EXISTING CONDITIONS PLAN
4	EXISTING CONDITIONS SECTIONS
5	TEMPORARY CONTROLS PLAN - PHASE I
6	TEMPORARY CONTROLS DETAILS
7	DEMOLITION PLAN
8	DEMOLITION SECTIONS
9	PROPOSED CONDITIONS PLAN - PHASE I
10	PROPOSED CONDITIONS SECTIONS - PHASE I
11	PROPOSED TIMBER DETAILS - PHASE I
12	PROPOSED GANGWAY-DOCK DETAILS - PHASE I
13	PROPOSED BULKHEAD-OUTFALL DETAILS - PHASE I
14	PROPOSED CONDITIONS AND FINAL CONDITIONS PLAN - PHASE II
15	PROPOSED CONDITIONS SECTIONS - PHASE II

100% DESIGN DRAWINGS
NOT FOR CONSTRUCTION

UNLESS SPECIFICALLY STATED BY WRITTEN AGREEMENT, THIS DRAWING IS THE SOLE PROPERTY OF GZA GEOENVIRONMENTAL, INC. (GZA). THE INFORMATION SHOWN ON THE DRAWING IS SOLELY FOR USE BY GZA'S CLIENT OR THE CLIENT'S DESIGNATED REPRESENTATIVE FOR THE SPECIFIC PROJECT AND LOCATION IDENTIFIED ON THE DRAWING. THE DRAWING SHALL NOT BE TRANSFERRED, REUSED, COPIED, OR ALTERED IN ANY MANNER FOR USE AT ANY OTHER LOCATION OR FOR ANY OTHER PURPOSE WITHOUT THE PRIOR WRITTEN CONSENT OF GZA. ANY TRANSFER, REUSE, OR MODIFICATION TO THE DRAWING BY THE CLIENT OR OTHERS, WITHOUT THE PRIOR WRITTEN EXPRESS CONSENT OF GZA, WILL BE AT THE USER'S SOLE RISK AND WITHOUT ANY RISK OR LIABILITY TO GZA.

REFERENCE NOTES

EXISTING CONDITIONS PLAN BASE MAP DEVELOPED FROM THE FOLLOWING:

- 1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16, PREPARED BY DOUGET SURVEY AND TRANSMITTED TO GZA ON SEPTEMBER 16, 2024..
2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024..
3. LOCATION OF OVERHEAD BRIDGE APPROXIMATED USING GOOGLE EARTH IMAGERY.

GENERAL NOTES

- 1. LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY, AND ARE NOT WARRANTED TO BE CORRECT. ADDITIONAL UTILITIES MAY EXIST WHICH ARE NOT INDICATED ON THESE PLANS. ALL EXISTING UTILITIES SHALL BE VERIFIED FOR SERVICE, SIZE, INVERT ELEVATION, LOCATION, ETC. PRIOR TO ANY CONSTRUCTION WORK IN THE VICINITY THEREOF. THE CONTRACTOR MUST NOTIFY DIGSAFE (PHONE NUMBER: 811) AT LEAST 3 FULL WORKING DAYS PRIOR TO ANY CONSTRUCTION. APPROPRIATE MUNICIPAL DEPARTMENTS MUST ALSO BE NOTIFIED. NOTIFY OWNER AND ENGINEER IN WRITING OF ANY AND ALL DISCREPANCIES PRIOR TO COMMENCING WORK.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINDING, VERIFYING, AND RE-SETTING (IF NECESSARY) CONTROL POINTS AND BENCHMARKS FOR THE WORK OF THE CONTRACT OR OTHERWISE ESTABLISHING THEIR OWN.
3. THE RESPONSIBILITY FOR SAFETY IN, ON, OR ABOUT THE JOBSITE SHALL BE THAT OF THE CONSTRUCTION CONTRACTOR. THESE DRAWINGS DO NOT INCLUDE COMPONENTS WHICH MAY BE NECESSARY FOR CONSTRUCTION SAFETY.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION, EXCEPT WHERE SPECIFICALLY DETAILED IN THE PLANS. LIKEWISE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SEQUENCE OF THE WORK, EXCEPT WHERE SPECIFICALLY DETAILED IN THE PLANS.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SURFACE AND GROUNDWATER CONTROL DURING THE WORK OF THE CONTRACT. TEMPORARY WATER CONTROL MEASURES SHALL BE, AT MINIMUM, AS REQUIRED BY THE PROJECT PLANS, SPECIFICATIONS, AND PERMIT CONDITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ADDITIONAL MEASURES NECESSARY FOR WATER CONTROL NECESSARY TO EXECUTE THE WORK OF THE CONTRACT "IN THE DRY". WATER CONTROL MEASURES ARE SUBJECT TO SPECIFIC LIMITS AND CONDITIONS AS MAY BE SHOWN ON THE PLANS.
6. ACCESS TO THE SITE SHALL BE FROM RIVERSIDE DRIVE. THE CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED BY CONSTRUCTION AS PER THE PLANS AND SPECIFICATIONS. WHERE NO SPECIFIC INSTRUCTION IS GIVEN, RESTORATION SHALL BE TO THE ORIGINAL CONDITION OR BETTER AND AT NO ADDITIONAL COST TO THE OWNER. THE CONTRACTOR IS SPECIFICALLY INFORMED THAT THE RESTORATION REQUIREMENT APPLIES TO ALL AREAS DISTURBED AS A RESULT OF THE PROJECT, INCLUDING ACCESS ROADS.
7. CONTRACTOR STAGING AREAS SHALL BE WITHIN LIMITS OF WORK AS DELINEATED ON THE PLANS. THE CONTRACTOR SHALL USE THESE AREAS AND THESE AREAS ONLY FOR ON-SITE PARKING, OFFICE TRAILERS (IF NECESSARY), EQUIPMENT AND MATERIAL STORAGE, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY NECESSARY SIGNAGE, FENCING, SAFETY, SEDIMENT/EROSION CONTROL IMPROVEMENTS, RESTORATIONS, ETC. IN THESE AREAS. AREAS WITHIN THE LIMITS OF THE WORK MAY BE USED FOR TEMPORARY STORAGE, HAUL ROADS, PARKING, ETC.; HOWEVER, NO ADDITIONAL CONSIDERATIONS OR PAYMENT WILL BE MADE FOR WORK NECESSARY TO RE-GRADE AND RESTORE SUCH AREAS TO PRE-CONSTRUCTION CONDITIONS OR RELOCATE ANY MATERIALS OR EQUIPMENT TEMPORARILY STORED WITHIN THE LIMITS OF THE WORK. IF THE CONTRACTOR REQUIRES AND IDENTIFIES ADDITIONAL STAGING AREAS ON THE OWNER'S PROPERTY, THE CONTRACTOR SHALL MAKE A WRITTEN REQUEST TO THE OWNER AND ENGINEER DESCRIBING THE NEED AND LOCATION OF THE PROPOSED AREA. NO GUARANTEE IS MADE THAT ADDITIONAL STAGING AREAS WILL BE MADE AVAILABLE.
8. PRIOR TO THE START OF WORK, THE CONTRACTOR SHALL DEVELOP, SUBMIT, AND MAINTAIN AN EMERGENCY CONTACT LIST WITH NAMES AND PHONE NUMBERS (DAY AND NIGHT) OF ALL KEY PERSONNEL INVOLVED WITH THE PROJECT. THE LIST SHALL SPECIFICALLY INCLUDE THE PERSON FROM THE CONTRACTOR WHO SHALL BE RESPONSIBLE FOR ENVIRONMENTAL COMPLIANCE. THE LIST SHALL BE PROVIDED TO THE OWNER, ENGINEER, AND RIDEM AND UPDATED AS NEEDED.
9. THE CONTRACTOR IS RESPONSIBLE FOR ALL PENALTIES AND DELAYS DUE TO NON-COMPLIANCE WITH PERMIT CONDITIONS.
10. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS IN THE FIELD.

EROSION AND SEDIMENTATION CONTROL

- 1. TEMPORARY CONTROL OF EROSION AND SEDIMENT DISCHARGE IS REQUIRED THROUGHOUT THE DURATION OF THE PROJECT AND UNTIL PROPOSED STABILIZATION IS ACHIEVED. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROTECT THE AREAS WITHIN THE LIMITS OF WORK AND BEYOND FROM SEDIMENT AND/OR POLLUTANTS ORIGINATING FROM ANY WORK DONE ON OR IN SUPPORT OF THE PROJECT, INCLUDING SEDIMENT DUE TO EROSION FROM STORMWATER RUNOFF.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING ALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES NECESSARY TO EXECUTE AND COMPLETE THE WORK OF THE CONTRACT, IN COMPLIANCE WITH THE TERMS AND CONDITIONS CONTAINED IN THE CONTRACT, PROJECT PERMITS AND ALL STATE AND LOCAL ORDINANCES THAT APPLY. CONTROLS SHOWN ON THE CONTRACT DRAWINGS SHALL BE CONSIDERED MINIMUM REQUIREMENTS. THE CONTRACTOR SHALL EMPLOY WHATEVER SUPPLEMENTARY MEASURES NECESSARY TO PROTECT WATER, AND ADJACENT AREAS FROM DISTURBANCE OR DISCHARGE OF SEDIMENTS.
3. ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO PREVENT MIGRATION INTO WATER BY SILT, SEDIMENT, FUELS, SOLVENTS, LUBRICANTS, CONCRETE, OR ANY OTHER POLLUTANTS ASSOCIATED WITH CONSTRUCTION PROCEDURES.
4. ACTUAL LOCATION OF EROSION CONTROLS AND BEST MANAGEMENT PRACTICES (BMP) MAY VARY DUE TO FIELD CHANGES, ONGOING CONSTRUCTION, ACCESS NEEDS, WEATHER, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IDENTIFYING THESE CHANGES AND ADJUSTING EROSION CONTROLS AND BMP LOCATIONS ACCORDINGLY. IN PARTICULAR, THE CONTRACTOR SHALL COORDINATE THE INSTALLATION AND RELOCATION OF BMPS WITH PROJECT PHASING, AS NECESSARY.
5. ALL EROSION CONTROLS AND BMPS SHALL REMAIN IN PLACE, EXCEPT AS OTHERWISE NECESSARY, UNTIL CONSTRUCTION IS COMPLETED AND PROPOSED STABILIZATION IS ACHIEVED. ALL CONTROLS AND BMPS SHALL BE SUBJECT TO INSPECTION BY THE OWNER AND THEIR REPRESENTATIVE AT ANYTIME THEREAFTER.
6. ADDITIONAL EROSION CONTROL BARRIERS SHALL BE INSTALLED AT THE DIRECTION OF THE ENGINEER TO MINIMIZE THE THREAT OF ADVERSE IMPACT DURING THE CONSTRUCTION PROCESS. AN ADEQUATE SUPPLY OF REPLACEMENT EROSION CONTROL BARRIERS WILL BE AVAILABLE ON-SITE FOR EMERGENCY PURPOSES.
7. PERIODIC INSPECTION, MAINTENANCE, AND CLEANING OF TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES AND BMPS ARE REQUIRED. ALL CONTROLS AND BMPS SHALL BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF RAINFALL EVENTS OF 0.5 INCHES OR GREATER. ROUTINE INSPECTION AND MAINTENANCE WILL REDUCE THE CHANCE OF POLLUTING STORMWATER BY FINDING AND CORRECTING PROBLEMS BEFORE THE NEXT RAIN EVENT. THE CONTRACTOR WILL BE REQUIRED TO KEEP A WRITTEN, UPDATED SITE MAINTENANCE LOG DOCUMENTING INSPECTION AND MAINTENANCE ACTIVITY.
8. CONTRACTOR SHALL PROVIDE AND MAINTAIN TURBIDITY CURTAINS AND IS RESPONSIBLE FOR DETERMINING LAYOUT AND INSTALLATION PROCEDURES IN ACCORDANCE WITH CONTRACT DOCUMENTS.
9. TURBIDITY CURTAINS SHALL BE INSTALLED TO SURROUND THE LIMIT OF WORK, AS NEEDED, INCLUDING BUT NOT LIMITED TO DEMOLITION AND WALL CONSTRUCTION.

WATER CONTROL NOTES

- 1. TEMPORARY WATER CONTROL BY THE CONTRACTOR SHALL BE PERFORMED AS SPECIFIED IN THE CONTRACT DOCUMENTS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TEMPORARY WATER CONTROL, SURFACE WATER AND GROUNDWATER CONTROL, NECESSARY TO EXECUTE AND COMPLETE THE WORK OF THE CONTRACT SUBJECT TO THE RESTRICTIONS CONTAINED IN THE CONTRACT AND PROJECT PERMITS. CONTROLS SHOWN IN THE CONTRACT DRAWINGS SHALL BE CONSIDERED MINIMUM REQUIREMENTS. THE CONTRACTOR SHALL EMPLOY WHATEVER SUPPLEMENTARY MEASURES NECESSARY TO PROTECT THE SITE AND THE WORK.
3. ALL TEMPORARY WATER CONTROL MEASURES SHALL BE IMPLEMENTED IN CONJUNCTION WITH APPROPRIATE SEDIMENT AND EROSION CONTROL MEASURES SO AS TO MINIMIZE TO THE GREATEST EXTENT POSSIBLE RELEASE OF SEDIMENT INTO WATER BODIES AND POTENTIAL EROSION OF SOIL.
4. ANY TEMPORARY PUMPS USED AT THE SITE MUST BE PROPERLY BAFFLED AGAINST EXCESSIVE NOISE. PUMPS OR GENERATORS WHICH USE LIQUID FUEL MUST BE PLACED WITHIN AN IMPERMEABLE SECONDARY CONTAINMENT AREA WITH SUFFICIENT CAPACITY TO CONTAIN THE FULL VOLUME OF THE FUEL TANK.
5. WATER PUMPED FROM THE EXCAVATIONS MUST BE PASSED THROUGH A PUMPED WATER FILTER BAG OR OTHER SUCH BEST MANAGEMENT PRACTICE FEATURE PRIOR TO BEING DISCHARGED BACK TO A WATER BODY. DISCHARGE WATER SHALL MEET APPROPRIATE WATER QUALITY STANDARDS.
6. PUMPED WATER DISCHARGE AREAS MUST BE PROPERLY PROTECTED TO PREVENT EROSION BY HIGH VELOCITY FLOW.

CONSTRUCTION NOTES:

- 1. THE CONTRACTOR AND/OR SUBCONTRACTORS MUST SHOW EVIDENCE OF PRIOR EXPERIENCE AND SUCCESSFUL COMPLETION OF THE TYPE OF REPAIR WORK INDICATED.

LAYOUT NOTES:

- 1. ALL LINES ARE PERPENDICULAR OR PARALLEL TO THE LINES FROM WHICH THEY ARE MEASURED UNLESS OTHERWISE INDICATED.
2. WALL LAYOUTS SHALL BE FIELD ADJUSTED TO PROVIDE THE MINIMUM WALKWAY OR GROUND FEATURE DIMENSIONS AS SHOWN ON THE PLANS.

LOAM AND SEED NOTES:

- 1. CONTRACTOR SHALL APPLY FERTILIZER AND LOAM TO PREPARE TOPSOIL SURFACE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
2. CONTRACTOR SHALL SEED ENTIRE DISTURBED AREA AS INDICATED IN THE PROJECT DOCUMENTS.
3. CONTRACTOR SHALL MULCH ENTIRE SEEDED AREA WITH STRAW MULCH TO HELP DEVELOP AN ACCEPTABLE STAND OF GRASS.
4. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE SEEDED AREA THROUGH ONE GROWING SEASON, MAINTENANCE INCLUDES MOWING, WATERING, AND RE-SEEDING AS REQUIRED TO DEVELOP A SATISFACTORY STAND OF HEALTHY GRASS.

DEMOLITION NOTES:

- 1. EXCAVATED MATERIALS SUSPECTED OF CONTAMINATION SHALL BE SEPARATED, AND WRAPPED IN POLYTHENE SHEETING STOCKPILED ON-SITE FOR EVALUATION BY THE ENGINEER.
2. CONTRACTOR SHALL EXERCISE EXTREME CAUTION TO AVOID DAMAGE TO ANY EXISTING UTILITIES AND STRUCTURES TO REMAIN IN PLACE DURING CONSTRUCTION AND/OR AFTER CONSTRUCTION IS COMPLETE.
3. ALL MATERIALS REMOVED AND NOT SPECIFIED TO BE SALVAGED OR REUSED SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND LEGALLY DISPOSED OF OFF-SITE.

MATERIAL NOTES:

- 1. MATERIAL AND PRODUCT REQUIREMENTS, WHERE REFERENCED AS RIDOT, THE REQUIREMENT REFERS TO THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE DESIGN "BLUE BOOK", LATEST EDITION.

GRAVEL BORROW: M.01.02.1

CRUSHED STONE: M.01.06

COMMON FILL: M.01.01

LOAM: L.01.01.1 / M.18.01

FILTER FABRIC: NON-WOVEN FABRIC, INERT TO UV LIGHT, EQUAL IN PROPERTIES TO MIRAFI 180N OR APPROVED EQUAL

2. REINFORCING SHALL HAVE A MINIMUM OF 2 INCHES OF COVER WHEN CONCRETE IS CAST AGAINST FORMWORK. FOR CONCRETE CAST AGAINST EARTH, THE COVER SHALL BE 3 INCHES MINIMUM.

CONCRETE NOTES:

CONCRETE - 28-DAY COMPRESSIVE STRENGTH OF 5,000 PSI MINIMUM
3/4 INCH MAXIMUM AGGREGATE SIZE
AIR CONTENT OF 5.0% - 7.0%
COLD WEATHER OR HOT WEATHER REQUIREMENTS AS APPLICABLE AND AS DETERMINED BY THE ENGINEER

CEMENT - PORTLAND CEMENT, ASTM C 150, TYPE II

REINFORCING STEEL NOTES:

REINFORCING BAR: ASTM A 615, GRADE 60, GALVANIZED ASTM A 123, ASTM A 153

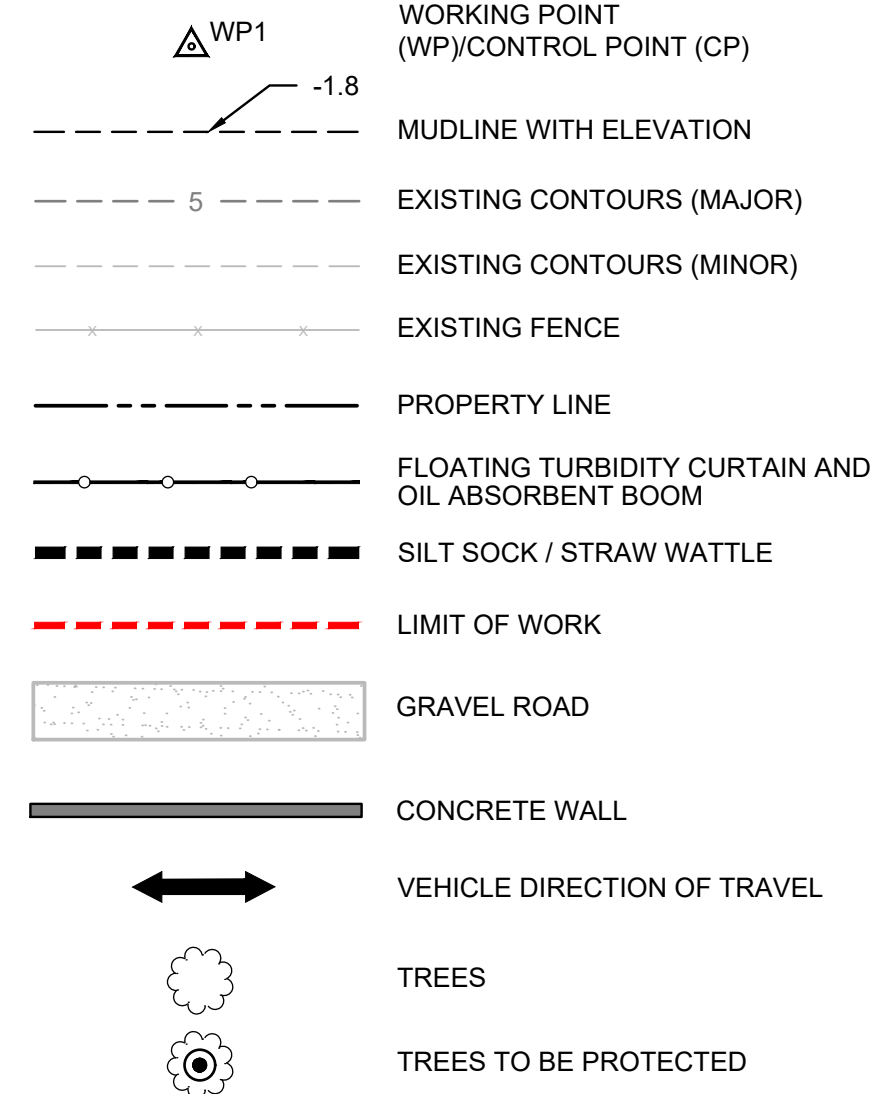
DOWELS: ASTM A 615, GRADE 60, GALVANIZED ASTM A 123, ASTM A 153

GROUT: TWO-COMPONENT TYPE EPOXY ADHESIVE SIKADUR 32, HI MOD OR APPROVED EQUAL

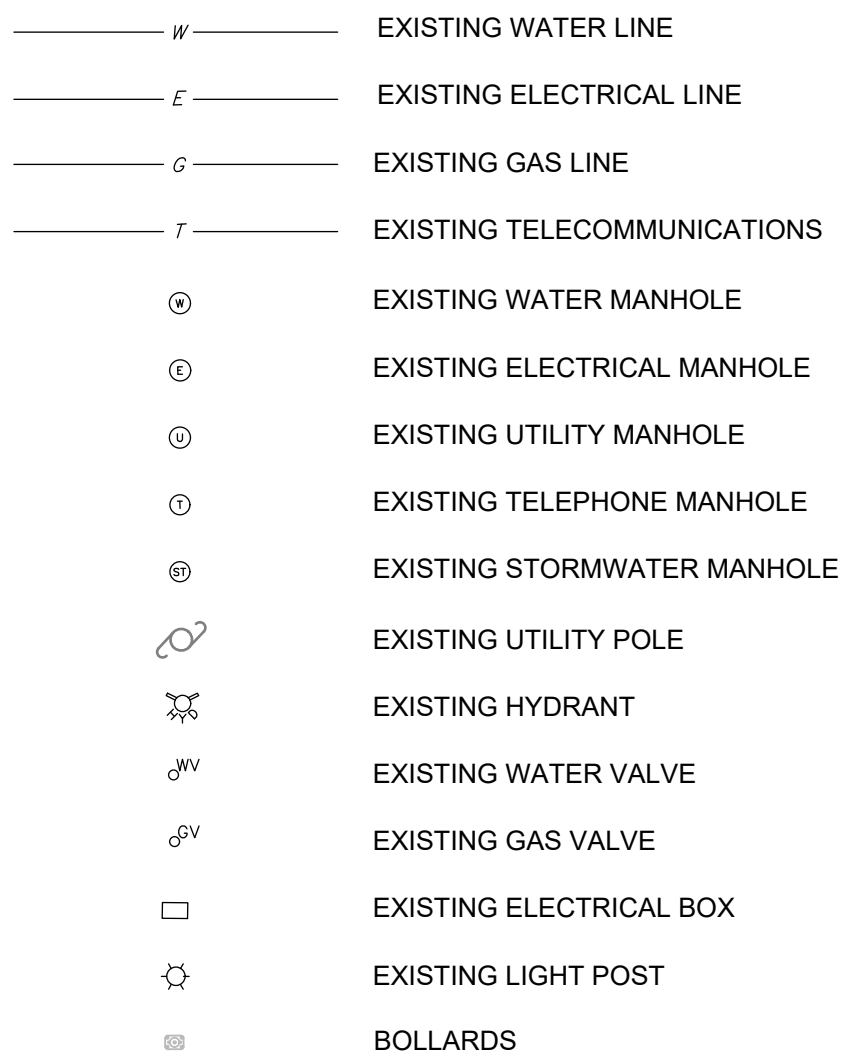
DESIGN CRITERIA:

- A. AMERICAN CONCRETE INSTITUTE, ACI 318, (LATEST EDITION)
B. STEEL CONSTRUCTION MANUAL, AMERICAN INSTITUTE OF STEEL CONSTRUCTION (LATEST EDITION)
C. WELDING OF STRUCTURAL AND MISCELLANEOUS STEEL - STRUCTURAL WELDING CODE (LATEST EDITION)
D. RHODE ISLAND BUILDING CODE

LEGEND



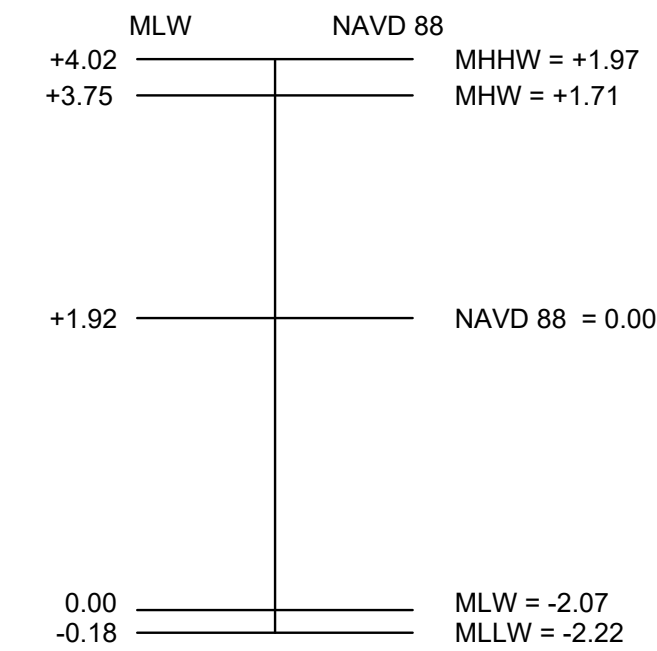
UTILITIES LEGEND:



ABBREVIATIONS AND ACRONYMS

Table with 2 columns: Abbreviation and Full Name. Includes CON (City of Newport), DIA (Diameter), D.I. (Ductile Iron), EL (Elevation), HASP (Health and Safety Plan), HDPE (High-Density Polyethylene), INV (Invert), MHHW (Mean High, High Water), MHW (Mean High Water), MIN (Minimum), MLLW (Mean Low, Low Water), MLW (Mean Low Water), NOAA (National Oceanic and Atmospheric Administration), OHW (Overhead Wires), PCP (Pre-cast Pipe), STA (Station), TYP. (Typical), N / F (Non or Formerly).

TIDAL ELEVATIONS

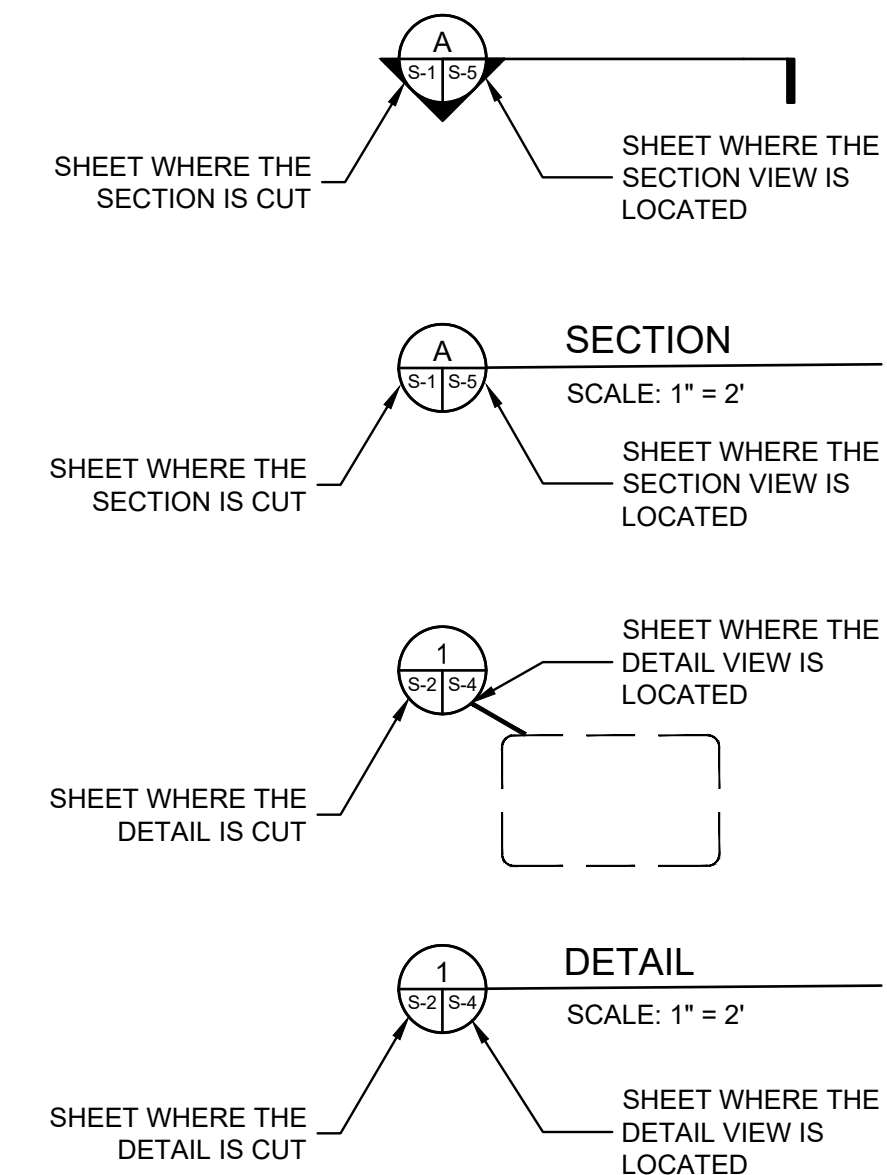


NOTE: TIDAL ELEVATIONS ARE REFERENCED TO NOAA STATION 8450948, ANTHONY POINT, TIVERTON, RI.

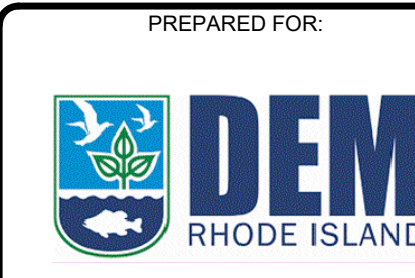
SURVEY AND PROJECT DATUM:

- 1. ALL ELEVATIONS SHOWN ON PLANS ARE IN U.S. FEET AND REFERENCE THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
2. COORDINATES ARE BASED ON NORTH AMERICAN DATUM OF 1983 (NAD83), RHODE ISLAND STATE PLANE, EASTERN ZONE, U.S. FEET (RI83-EF).
3. CONTRACTOR SHALL MAINTAIN ADEQUATE SURVEY CONTROL AT ALL TIMES TO ESTABLISH AND MAINTAIN ALL LINES AND ELEVATIONS.

ANNOTATIONS AND LABELS



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100% DESIGN DRAWINGS NOT FOR CONSTRUCTION

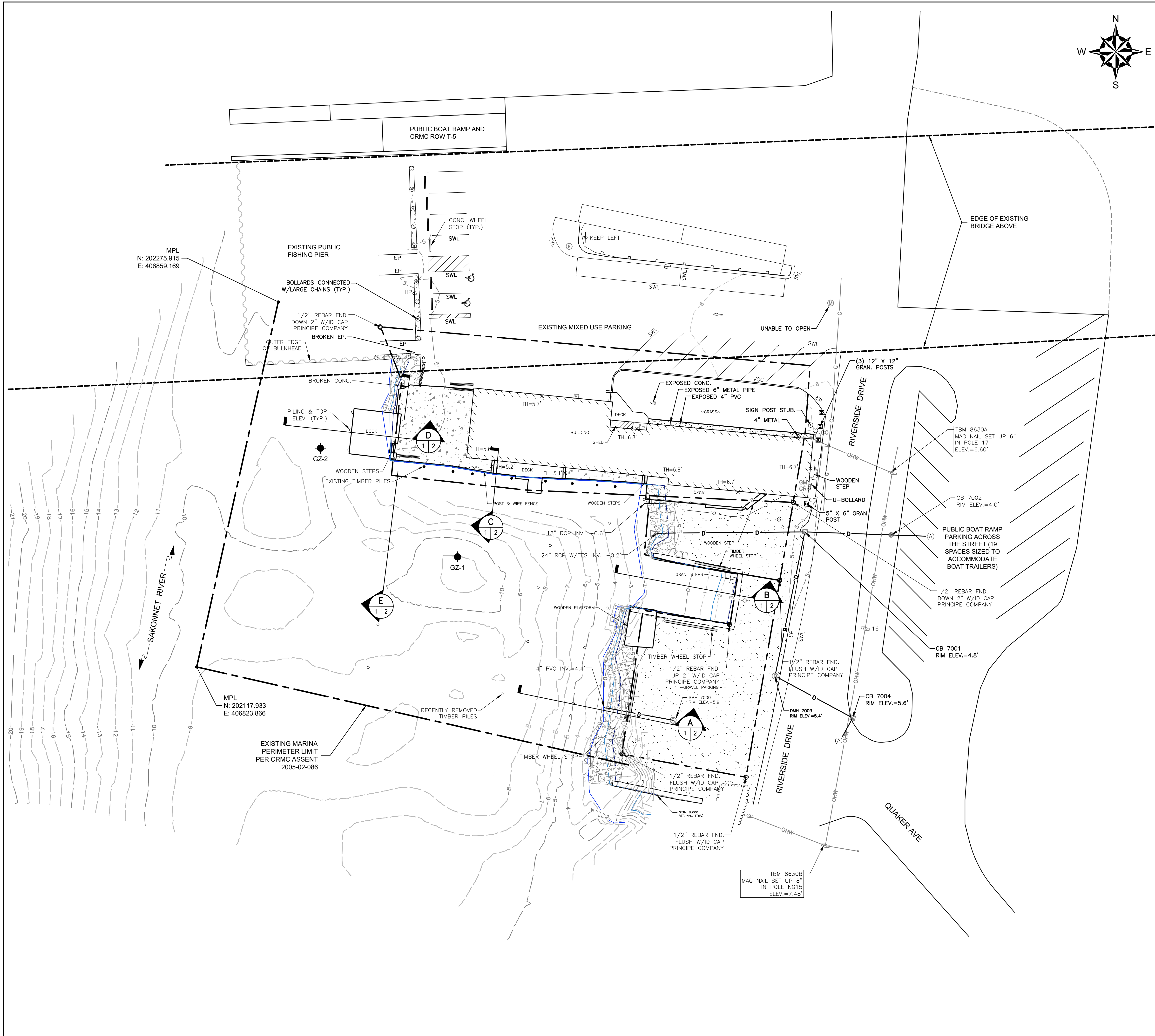
Table with columns: REV, DESCRIPTION, DATE, BY

Table with columns: PROJECT NO., DATE, PROJECT MGR., DESIGNED BY, DRAWN BY, CHECKED BY, REVIEWED BY, SCALE, REVISION NO.

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND
GENERAL NOTES

DRAWING 2 SHEET NO. 2 OF 15

APPROVED:

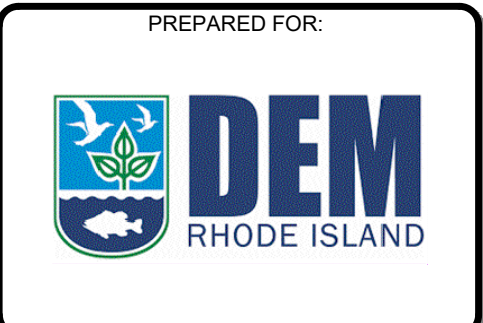


REFERENCE NOTES:

1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DOUCET SURVEY TRANSMITTED TO GZA ON SEPTEMBER 16, 2024.
2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024.

LEGEND

- APPROXIMATE LOT LINE PER REF. PLAN
- - - MAJOR CONTOUR LINE
- - - MINOR CONTOUR LINE
- RETAINING WALL
- CHAIN LINK FENCE
- HANDRAIL
- GUARDRAIL
- OVERHEAD WIRE
- DRAIN LINE
- GAS LINE
- PROPERTY BOUNDARY
- HIGH TIDE LINE (HTL) = EL. 3.2'
- MEAN HIGH WATER (MHW) = EL. 1.71'
- MEAN LOW WATER (MLW) = EL. -2.07'
- SHRUB LINE
- CONCRETE
- RIP RAP
- CRUSHED STONE
- X 5.2 SPOT GRADE
- PIPE/ROD FOUND
- UTILITY POLE
- UTILITY POLE & GUY WIRE
- UTILITY POLE STUB
- DRAIN MANHOLE
- CATCH BASIN
- FLARED END SECTION
- SEWER MANHOLE
- CLEANOUT
- GAS METER
- GAS REGULATOR VALVE
- ELECTRIC MANHOLE
- MANHOLE
- SIGN
- SQUARE POST
- POST
- WOODEN PILING & TOP ELEVATION
- ACCESSIBLE PARKING SPACE
- FLOW DIRECTION
- JERSEY BARRIER
- CONC. CONCRETE
- EP EDGE OF PAVEMENT
- GRAN. GRANITE
- PVC POLYVINYL CHLORIDE PIPE
- RCP REINFORCED CONCRETE PIPE
- RET. WALL RETAINING WALL
- SWL SINGLE WHITE LINE
- SYL SINGLE YELLOW LINE
- TH THRESHOLD ELEVATION
- TYP. TYPICAL
- VCC VERTICAL GRANITE CURB
- (X) INVERT I.D. CONNECTION UNKNOWN
- GZ-1 / GZ-2 TEST BORINGS PERFORMED BY REAGAN MARINE CONSTRUCTION ON FEBRUARY 25 AND 26, 2025 AND OBSERVED BY GZA.



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REV.	DESCRIPTION	DATE	BY

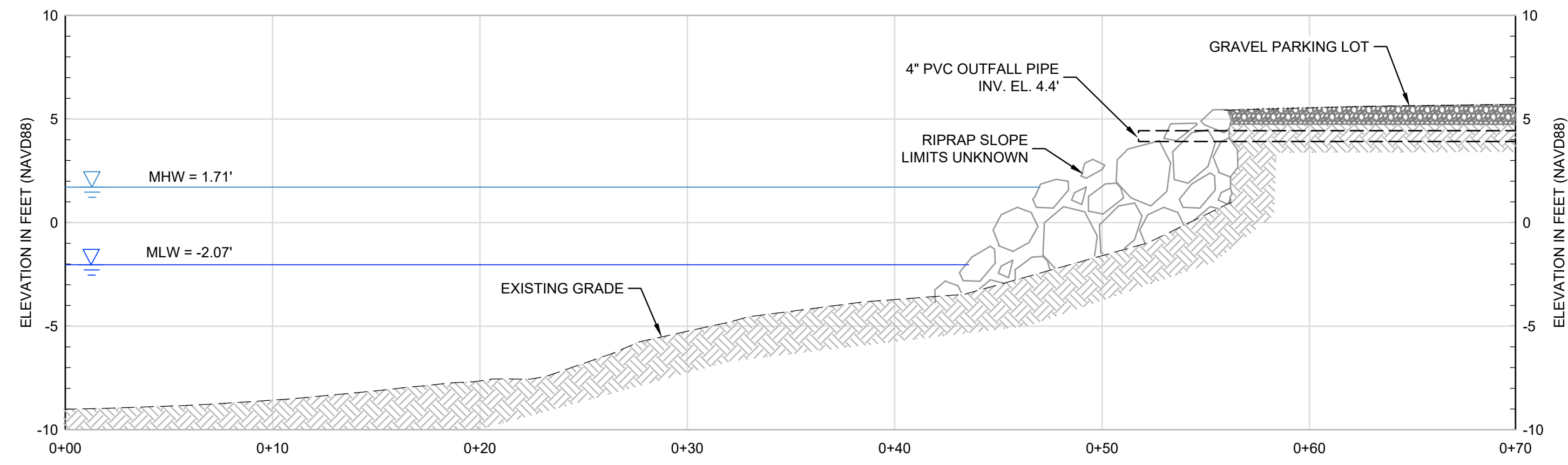
PROJECT NO:	35429.00
DATE:	DECEMBER, 2025
PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

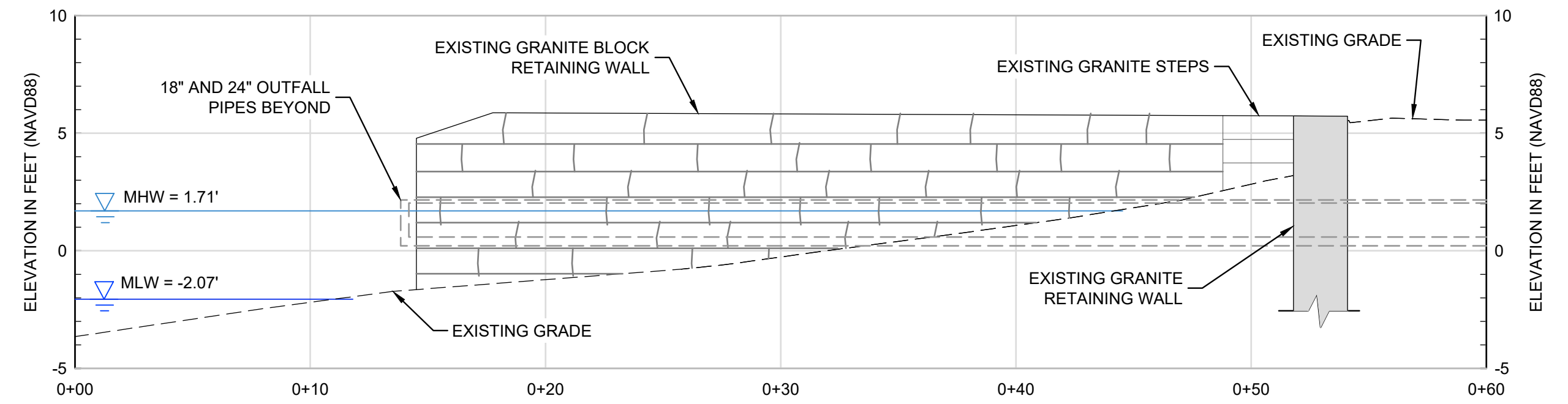
EXISTING CONDITIONS PLAN



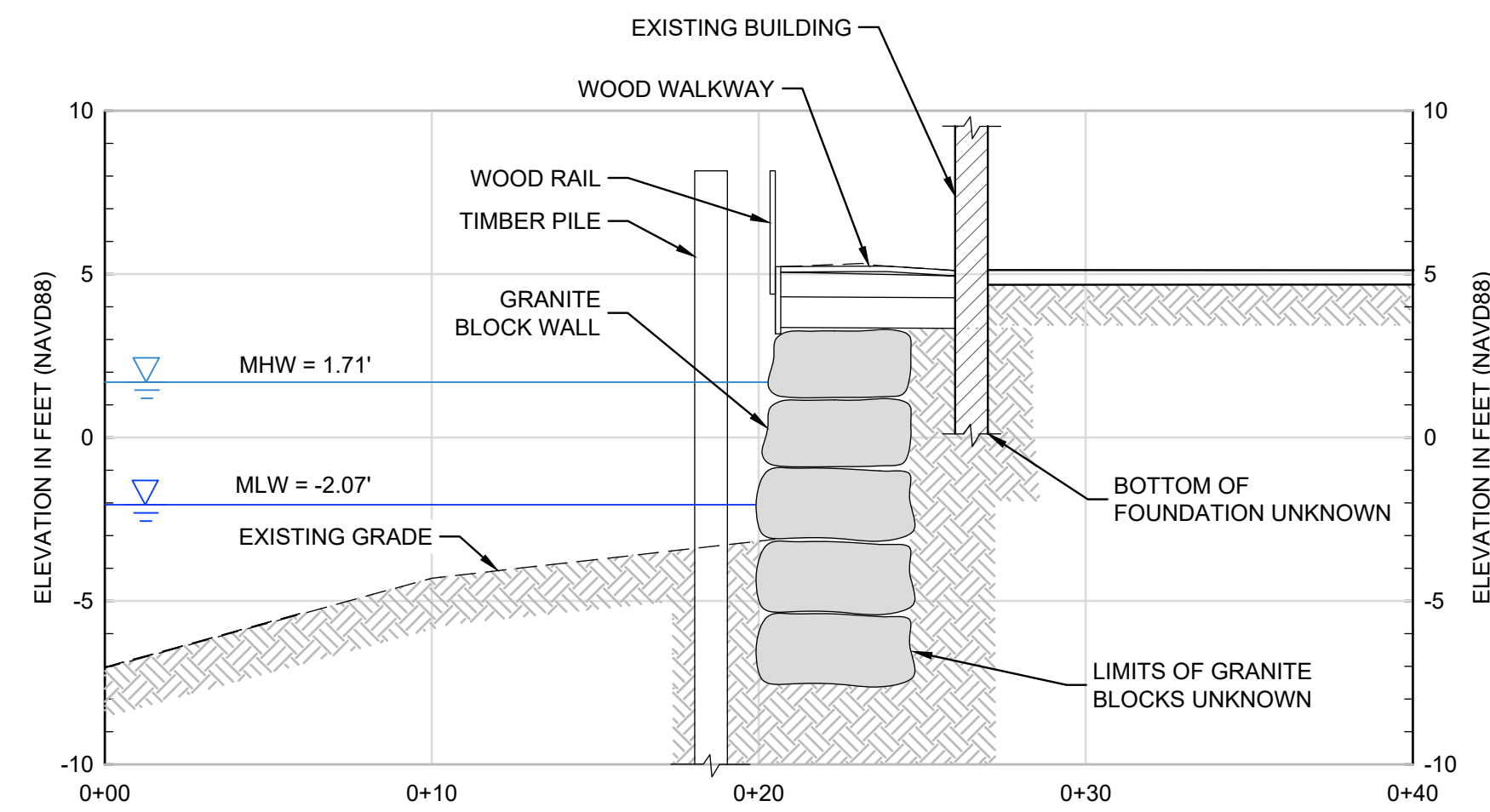
© 2025 - GZA GeoEnvironmental, Inc. GZA-J:\GEO\35429-TWS\FIGURES\CAD\DWGS\CURRENT\PLANS\90% DESIGN_SET\35429.00_4_EXISTING COND SECTIONS.DWG 4 July 30, 2025 GARY BASTIEN



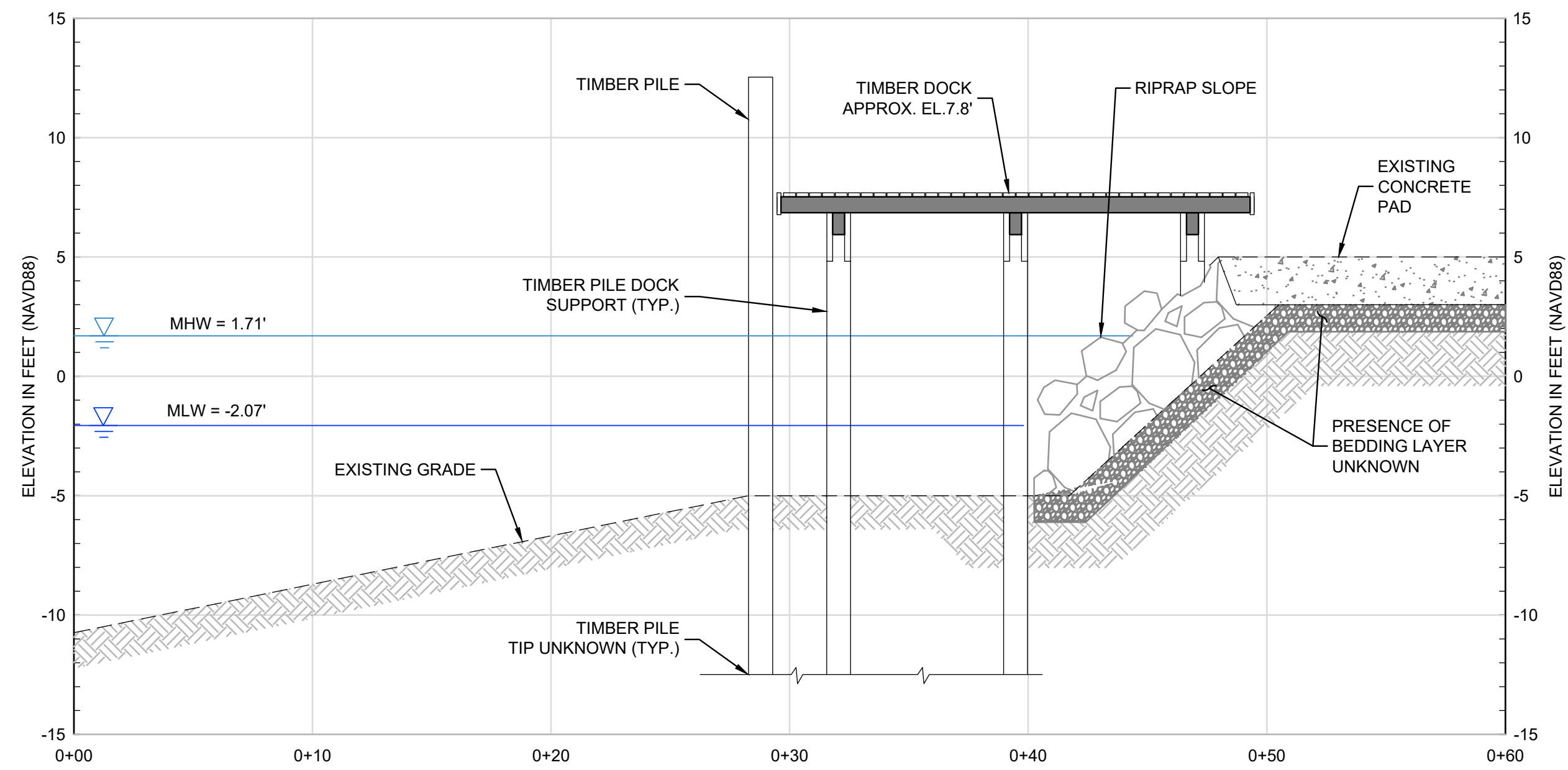
A RIPRAP SLOPE / PARKING LOT - SECTION
SCALE: 1"=5'



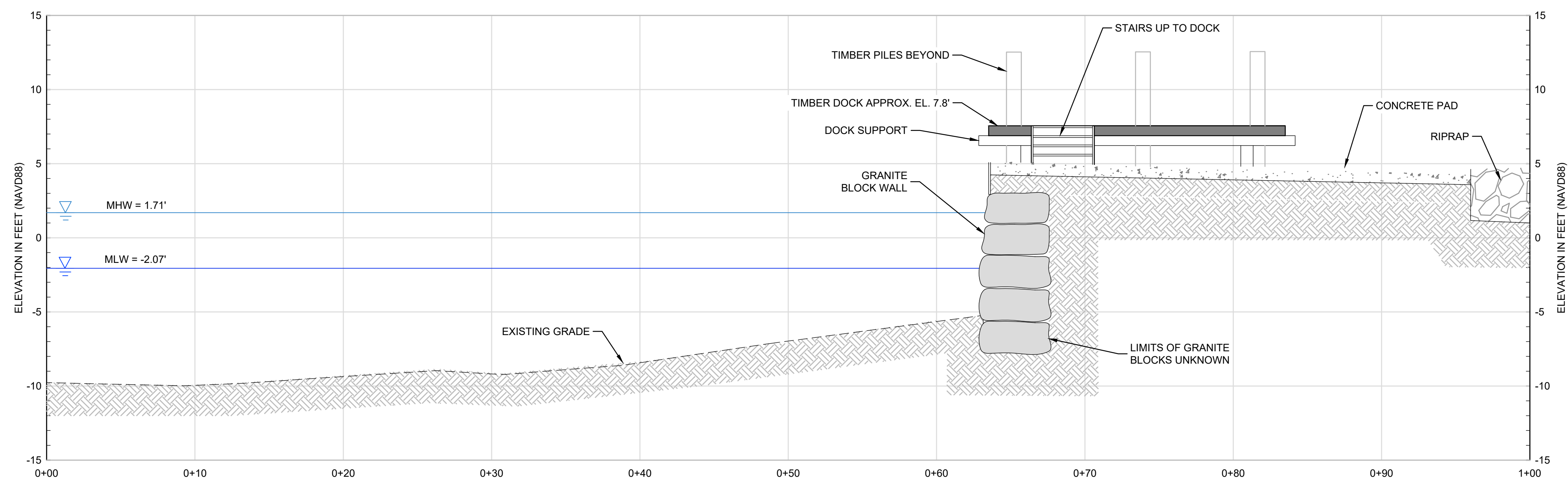
B GRANITE BLOCK WALL - SECTION
SCALE: 1"=5'



C GRANITE BLOCK WALL - SECTION
SCALE: 1"=5'



D DOCK PLATFORM - SECTION
SCALE: 1"=5'



E DOCK PLATFORM - SECTION
SCALE: 1"=5'



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PREPARED FOR:



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CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

EXISTING CONDITIONS SECTIONS

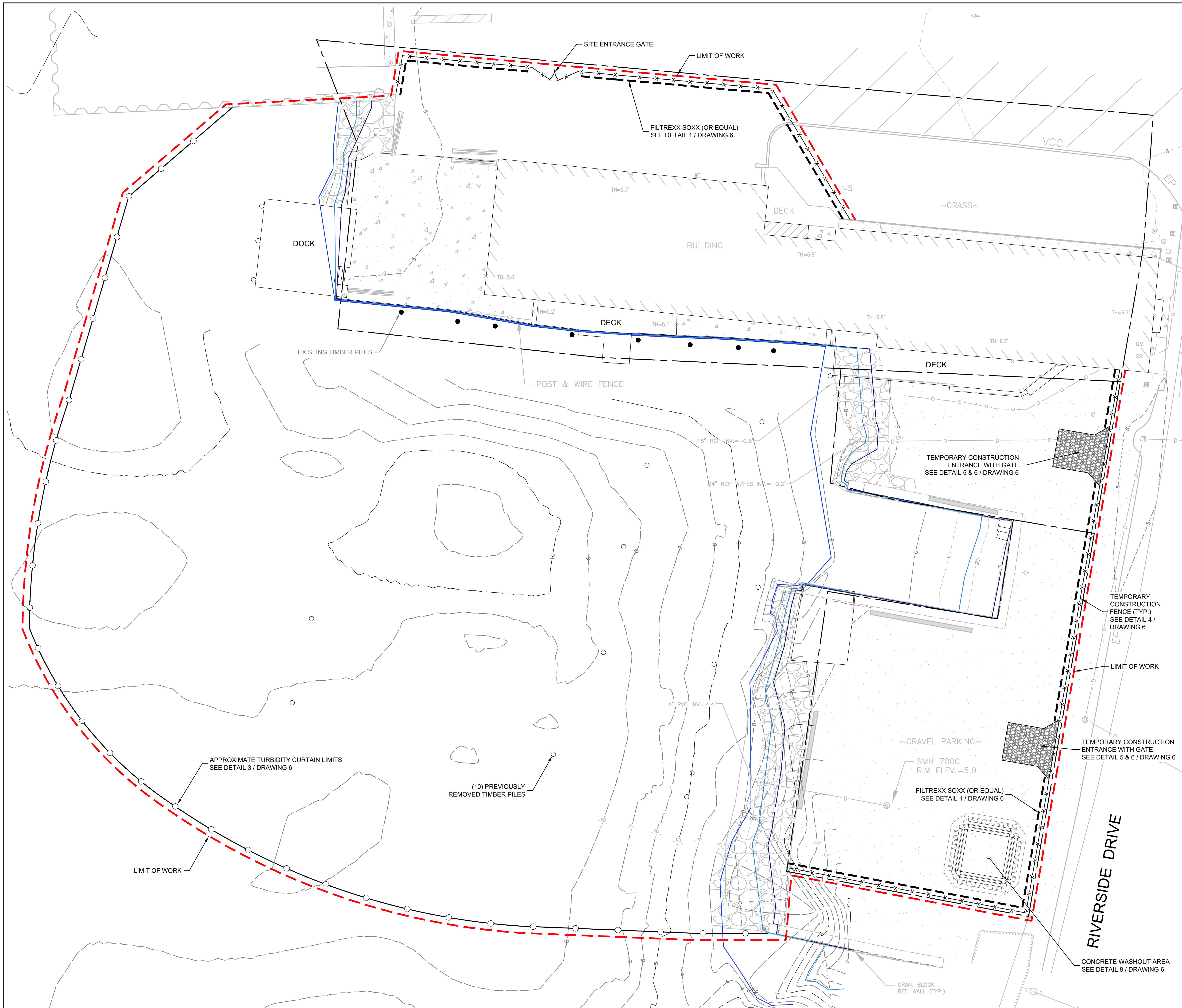
DRAWING

4

SHEET NO. 4 OF 15



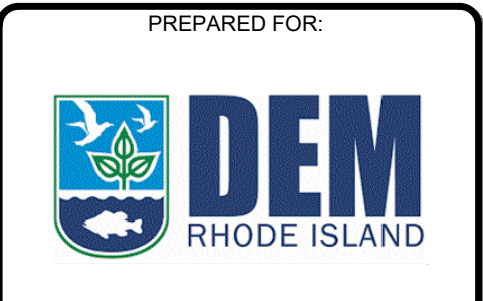
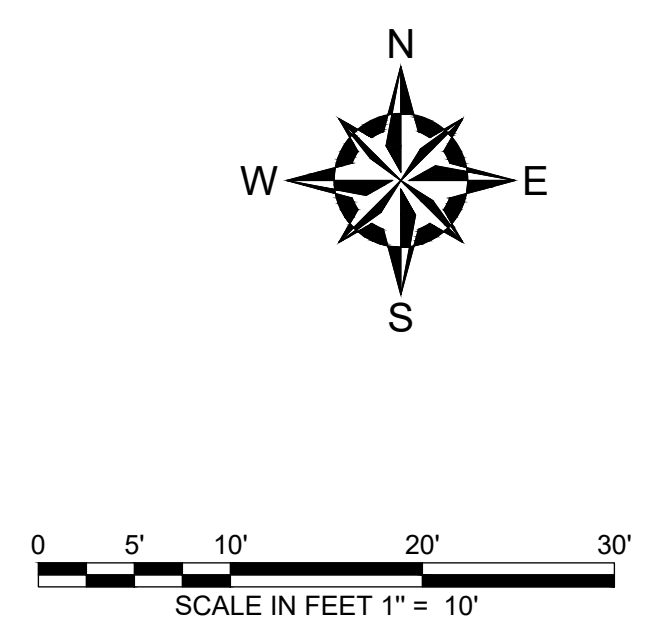
© 2025 - GZA GeoEnvironmental, Inc. GZA-1\GEO\35429\TWS\FIGURES\CAD\DWGS\CURRENT\PLANS\90% DESIGN SET\35429.00_5_TEMPORARY CONTROLS PLAN_PHASE 1.DWG 5 September 3, 2025 GARY BASTIEN



- REFERENCE NOTES:**
1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DORCET SURVEY TRANSMITTED TO GZA ON SEPTEMBER 16, 2024.
 2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024.

- TEMPORARY CONTROL NOTES:**
1. CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING AND MAINTAINING ALL TEMPORARY EROSION AND SEDIMENT CONTROLS IN ACCORDANCE WITH PERMITS AND CONTRACT DOCUMENTS.
 2. INSTALL EROSION AND SEDIMENT CONTROLS BEFORE DISTURBING ANY SOILS, SEDIMENT OR VEGETATION AT THE SITE.
 3. ADJUST, RELOCATE OR PROVIDE ADDITIONAL EROSION AND SEDIMENT CONTROLS AS WORK PROGRESSES INTO PREVIOUSLY UNDISTURBED AREAS OF THE SITE.
 4. MAINTAIN EROSION AND SEDIMENT CONTROLS UNTIL THE SITE IS RESTORED AND SITE IMPROVEMENTS INCLUDING LANDSCAPING, IF ANY, ARE COMPLETE WITH UNDERLYING SOILS PERMANENTLY STABILIZED.
 5. CARE SHALL BE TAKEN NOT TO PLACE REMOVED SEDIMENTS WITHIN THE PATH OF EXISTING, NEWLY CREATED OR PROPOSED AREAS THAT ARE SUBJECT TO STORMWATER FLOW.
 6. LIMITS OF TURBIDITY CURTAIN ARE SHOW FOR ILLUSTRATION PURPOSES ONLY. CONTRACTOR SHALL PROVIDE AND MAINTAIN TURBIDITY CURTAINS AND IS RESPONSIBLE FOR DETERMINING LAYOUT AND INSTALLATION PROCEDURES IN ACCORDANCE WITH CONTRACT DOCUMENTS.
 7. TURBIDITY CURTAINS SHALL BE INSTALLED TO SURROUND THE LIMIT OF WORK, AS NEEDED, INCLUDING BUT NOT LIMITED TO DEMOLITION AND WALL CONSTRUCTION.
 8. DEWATER LANDWARD OF BULKHEAD USING A PUMP WITH A FILTER BAG. PUMP WATER INTO SAKONNET RIVER.
 9. PROTECTING DEVICES SHALL BE USED AROUND TREES AND TRANSFORMERS ON SITE.

- LEGEND - EROSION CONTROL**
- APPROXIMATE PROPERTY BOUNDARY
 - - - - - LIMIT OF WORK
 - - - - - FILTREXX (R) SILT SOXX (OR EQUAL)
 - x x x x x TEMPORARY FENCE LINE
 - o o o o o FLOATING TURBIDITY CURTAIN



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**100% DESIGN DRAWINGS
NOT FOR CONSTRUCTION**

REV.	DESCRIPTION	DATE	BY

PROJECT NO:	35429.00
DATE:	DECEMBER, 2025
PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

TEMPORARY CONTROLS PLAN

DRAWING
5
SHEET NO. 5 OF 15

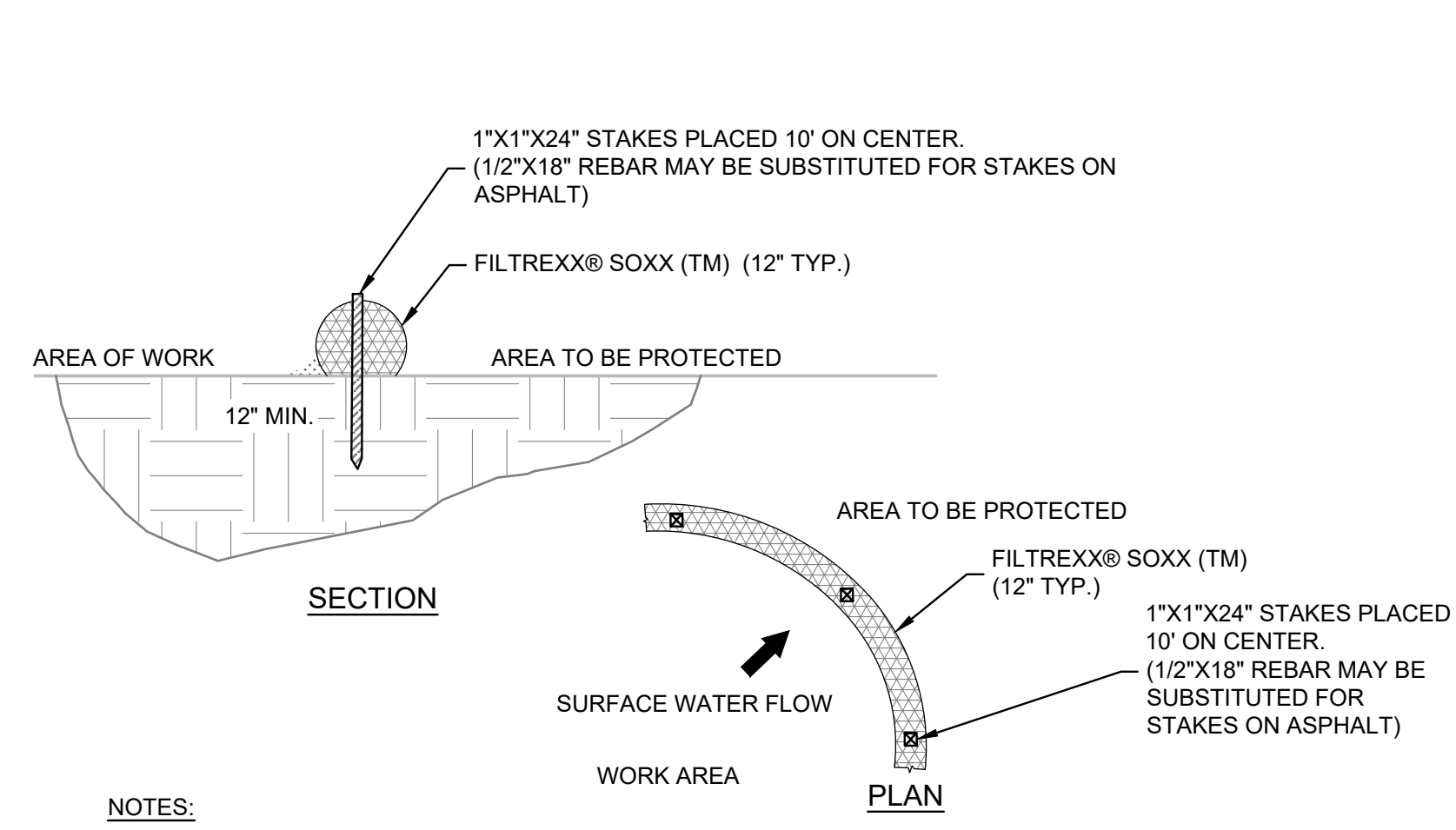


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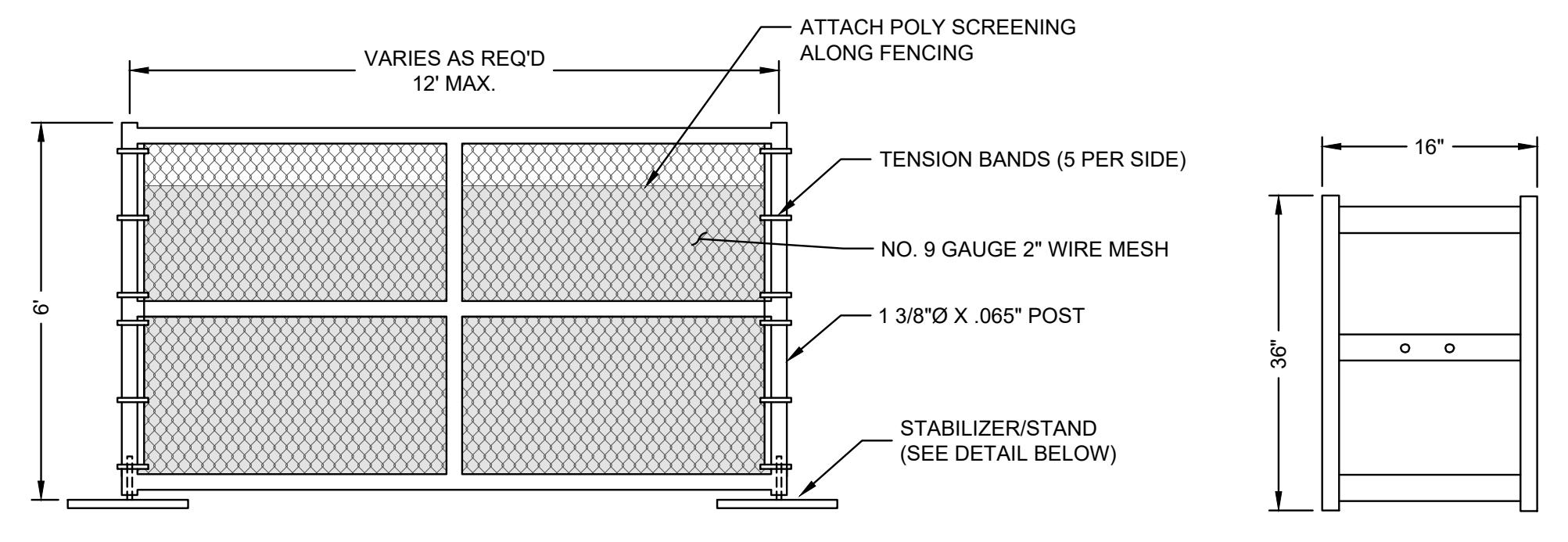


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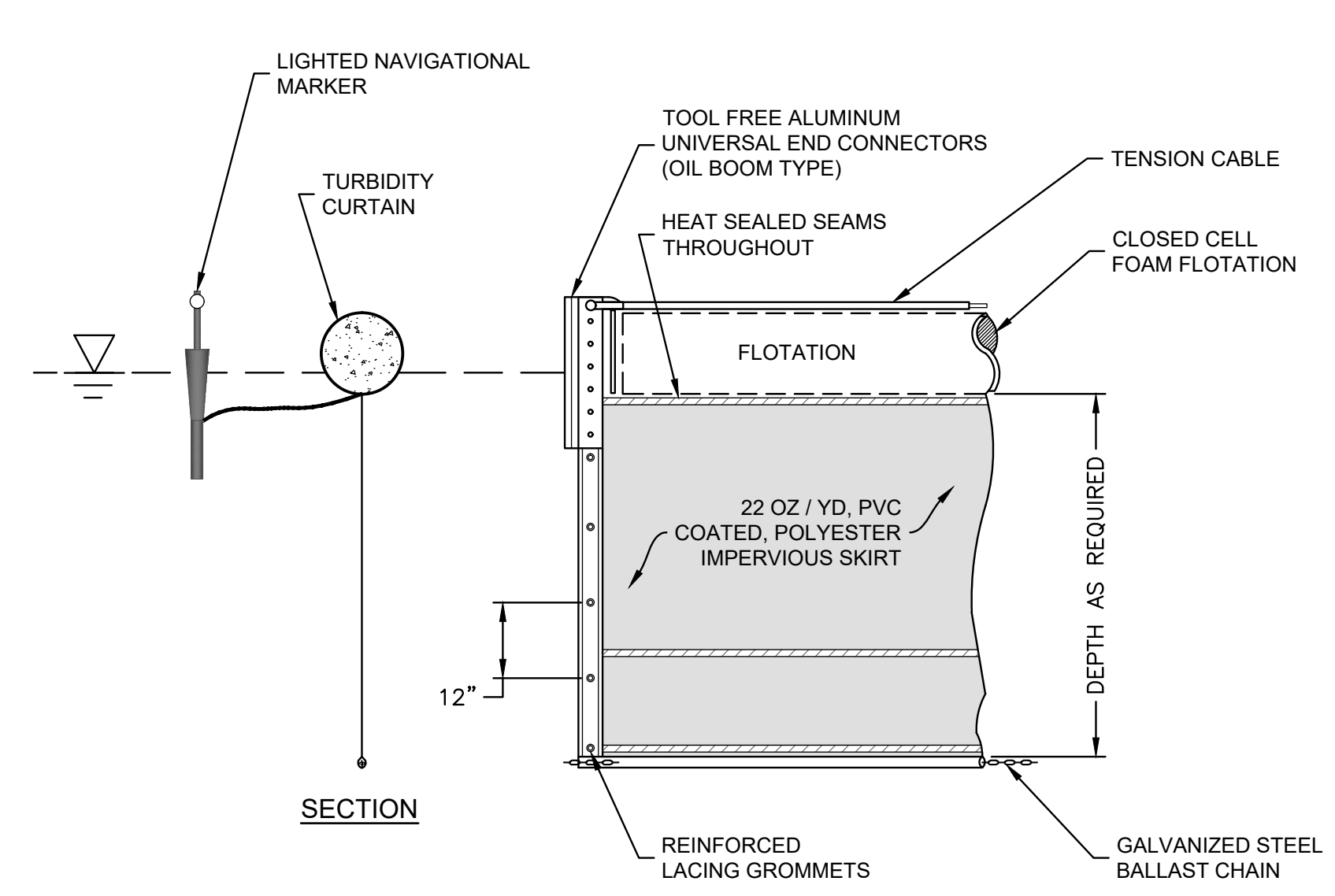


- NOTES:**
1. ALL MATERIAL TO MEET FILTREXX® SPECIFICATIONS.
 2. STAKES/REBAR PINS SHALL HAVE PROTECTIVE CAPS INSTALLED TO PREVENT FALL INJURY.

1 FILTREXX® SOXX SEDIMENT CONTROL DETAIL
5 6 NOT TO SCALE



5 TYPICAL TEMPORARY CONSTRUCTION DOUBLE SWING GATE
5 6 NOT TO SCALE

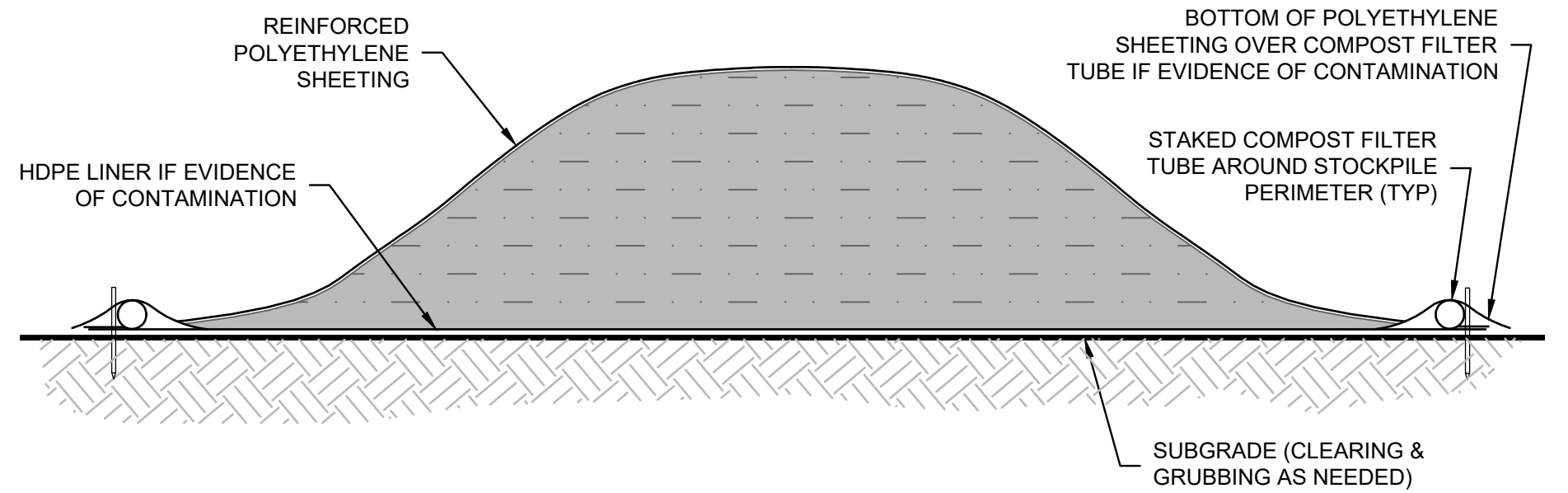


- NOTES:**
1. REFER TO SECTION 01 57 00 - TEMPORARY CONTROLS FOR MINIMUM REQUIREMENTS OF TURBIDITY CURTAINS.
 2. END OF CURTAIN SHALL BE ANCHORED SECURELY AT THE SHORELINE ABOVE MEAN HIGH WATER ELEVATION IN ACCORDANCE WITH MANUFACTURERS SPECIFICATIONS.
 3. TURBIDITY CURTAINS SHALL BE INSPECTED REGULARLY TO DETERMINE IF ALL COMPONENTS ARE FUNCTIONING PROPERLY.

3 TURBIDITY CURTAIN
5 6 NOT TO SCALE

- TEMPORARY CONSTRUCTION FENCING NOTES:**
1. FENCES SHALL BE CONSTRUCTED WITH A TOP AND BOTTOM RAIL.
 2. GATES MUST REMAIN IN GOOD WORKING ORDER AND MUST BE CLOSED AND SECURED DURING NON-WORKING HOURS.
 3. GATES SHALL BE CONSTRUCTED SO THAT THEY SWING IN TOWARDS THE CONSTRUCTION SITE.
 4. GATES MUST BE CONSTRUCTED WITH THE SAME DESIGN CHARACTERISTICS AS THE TEMPORARY CONSTRUCTION FENCE.
 5. FABRIC SHALL BE ANCHORED IN EACH GROMMET WITH ALUMINUM TIES.

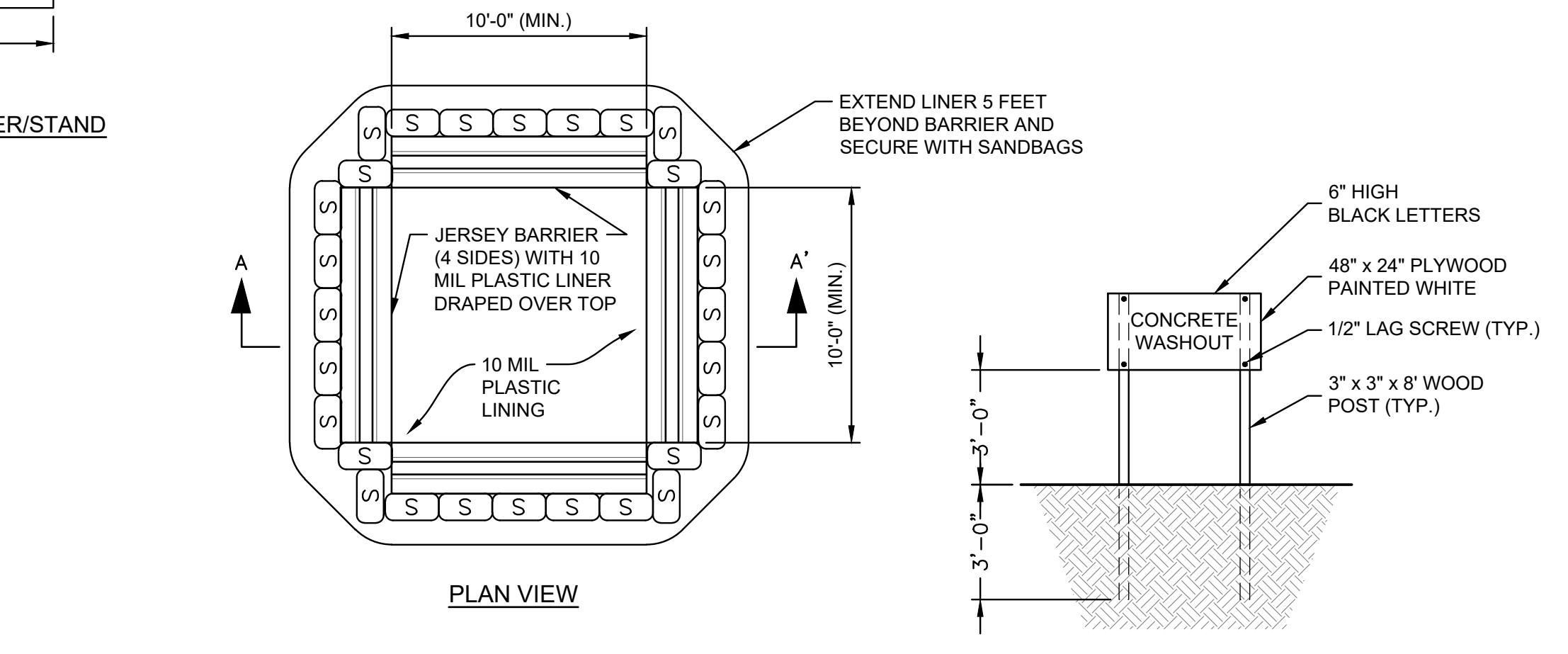
4 TYPICAL TEMPORARY CONSTRUCTION FENCE PANEL
5 6 NOT TO SCALE



TYPICAL FENCE STABILIZER/STAND
NOT TO SCALE

7 TYPICAL MATERIAL STOCKPILE DETAIL
5 6 NOT TO SCALE

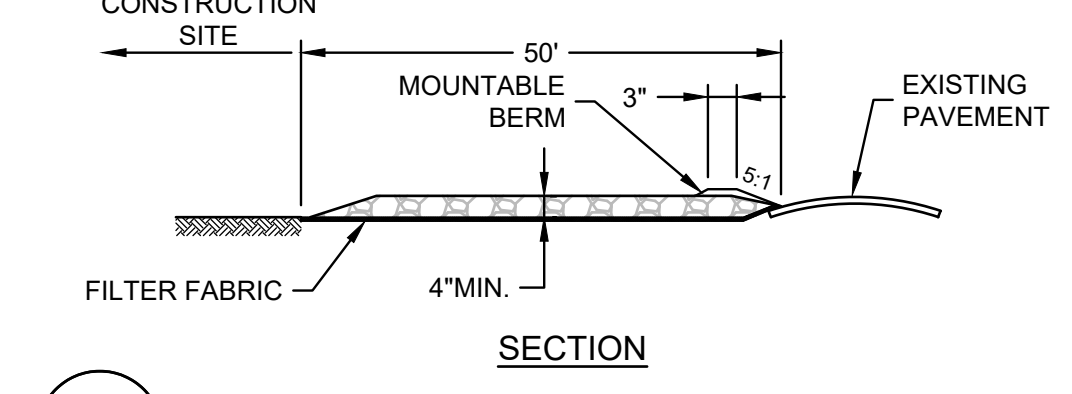
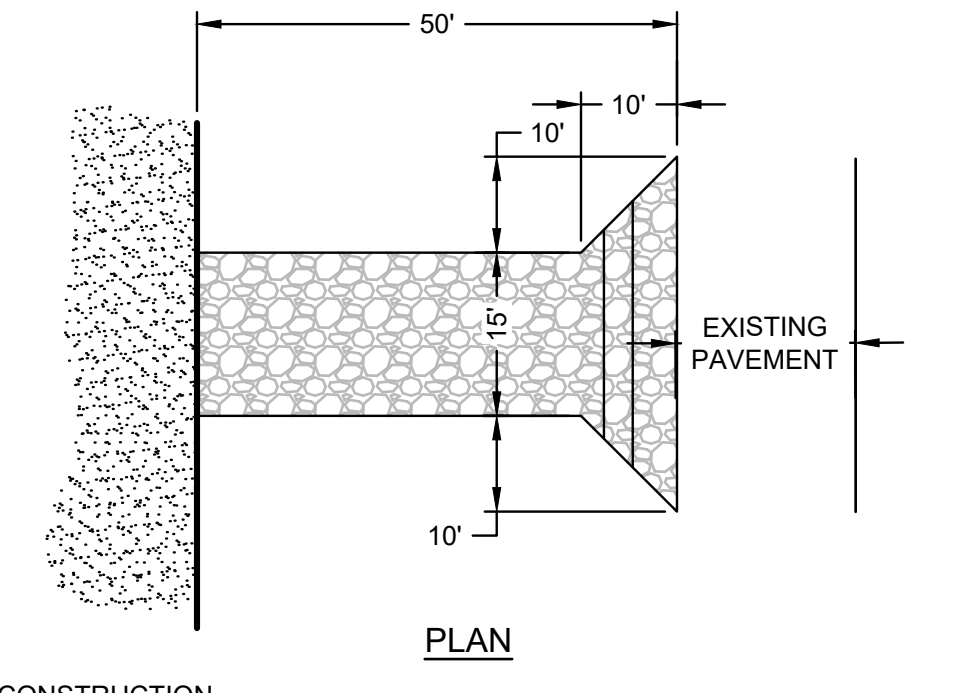
- SOIL STOCKPILE NOTES:**
1. SOIL STOCKPILES SHALL BE PLACED ON AND COVERED WITH POLYETHYLENE SHEETING AT ALL TIMES TO PREVENT DUST OR IS SUSPECTED OF CONTAMINATION.
 2. SHEETING COVERING STOCKPILE SHALL BE MAINTAINED TIGHTLY IN PLACE BY USING SANDBAGS ON ROPES WITH A MAXIMUM 10'-0\"/>



CONCRETE WASHOUT SIGN DETAIL (OR EQUIVALENT)

- NOTES:**
1. SLUMPS TO BE CLEANED AND WASTE CONCRETE AND WATER REMOVED AND PROPERLY DISPOSED OF UPON COMPLETION OF WORK.

8 CONCRETE WASHOUT AREA
5 6 NOT TO SCALE



6 TEMPORARY CONSTRUCTION ENTRANCE / EXIT
5 6 NOT TO SCALE

- NOTES:**
1. STONE SIZE - USE 2\"/>

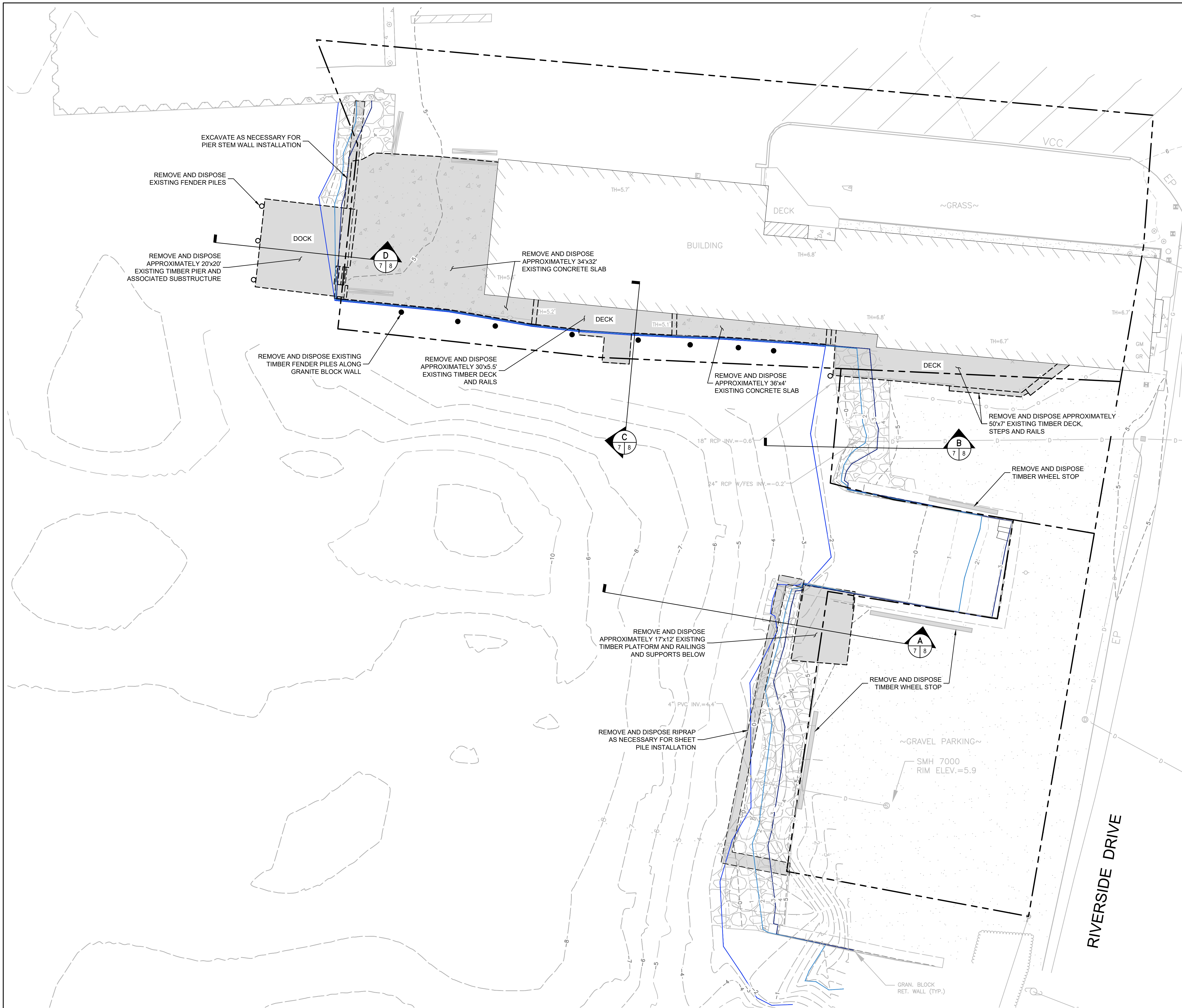
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PROJECT MGR:	TWS
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DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

TEMPORARY CONTROLS DETAILS

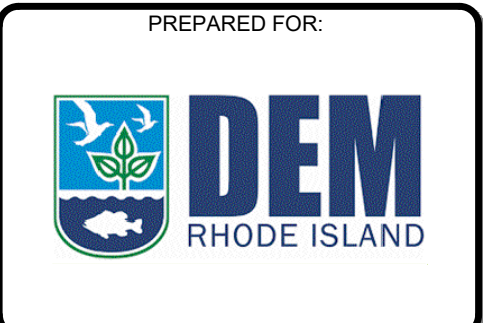


REFERENCE NOTES:

1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DORCET SURVEY SHIPPED TO GZA ON SEPTEMBER 16, 2024.
2. BATHYMETRIC SURVEY OF SITE CREATED BY GZA - AMESBURY.

LEGEND

- APPROX. LOT LINE PER REF. PLAN
- MAJOR CONTOUR LINE
- MINOR CONTOUR LINE
- RETAINING WALL
- CHAIN LINK FENCE
- HANDRAIL
- GUARDRAIL
- OVERHEAD WIRE
- DRAIN LINE
- GAS LINE
- SHRUB LINE
- CONCRETE
- RIP RAP
- CRUSHED STONE
- SPOT GRADE
- PIPE/ROD FOUND
- UTILITY POLE
- UTILITY POLE & GUY WIRE
- UTILITY POLE STUB
- DRAIN MANHOLE
- CATCH BASIN
- FLARED END SECTION
- SEWER MANHOLE
- CLEANOUT
- GAS METER
- GAS REGULATOR VALVE
- ELECTRIC MANHOLE
- MANHOLE
- SIGN
- SQUARE POST
- POST
- WOODEN PILING & TOP ELEVATION
- ACCESSIBLE PARKING SPACE
- FLOW DIRECTION
- JERSEY BARRIER
- CONC.
- EP
- GRAN.
- PVC
- RCP
- RET. WALL
- SWL
- SYL
- TH
- TYP.
- VCC
- INVERT I.D. CONNECTION UNKNOWN
- REMOVE AND DISPOSE EXISTING GUIDE PILES AND FENDER PILES
- REMOVE AND DISPOSE EXISTING TIMBER DECK AND PLATFORMS / CONCRETE DECK



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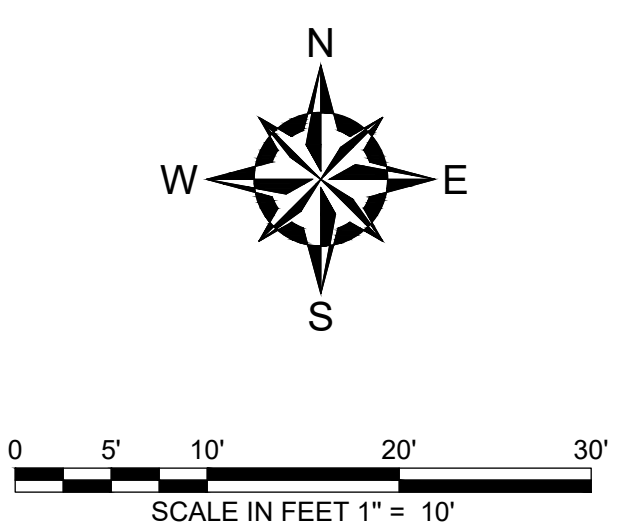
100% DESIGN DRAWINGS
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PROJECT NO: 35429.00
 DATE: DECEMBER, 2025
 PROJECT MGR: TWS
 DESIGNED BY: JTF
 DRAWN BY: GRB
 CHECKED BY: TWS
 REVIEWED BY: MJP
 SCALE: AS NOTED
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CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

DEMOLITION PLAN





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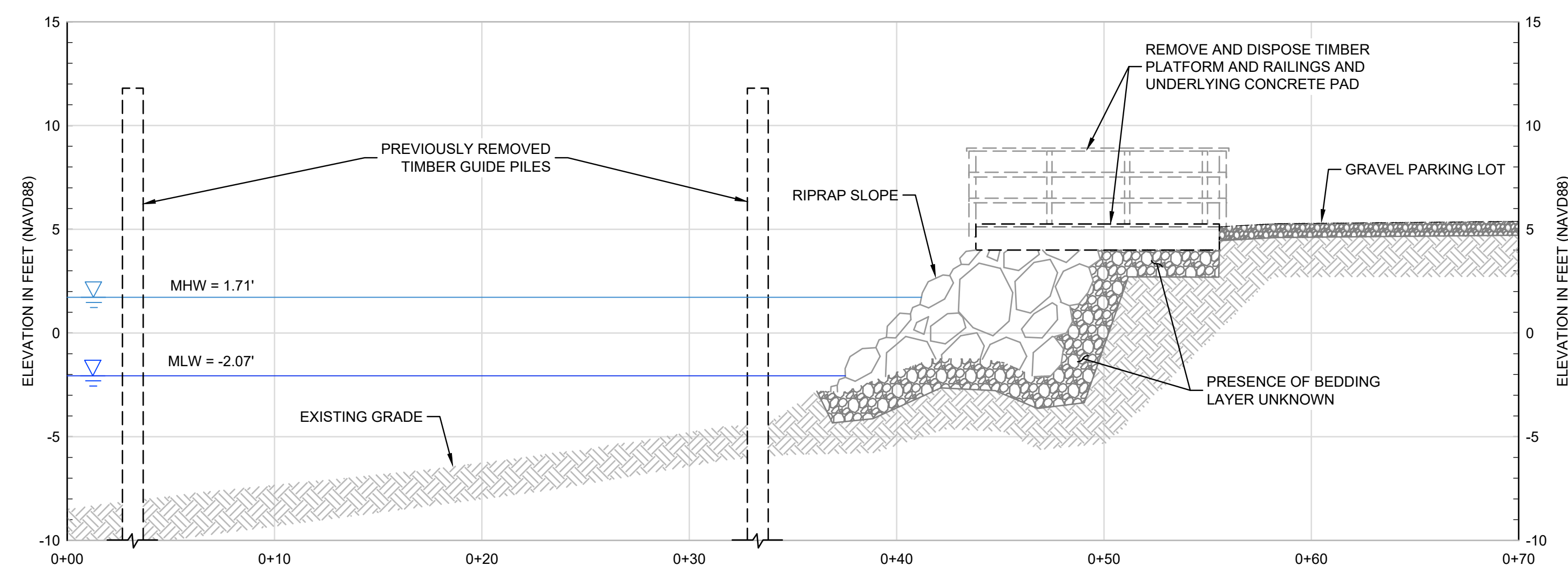
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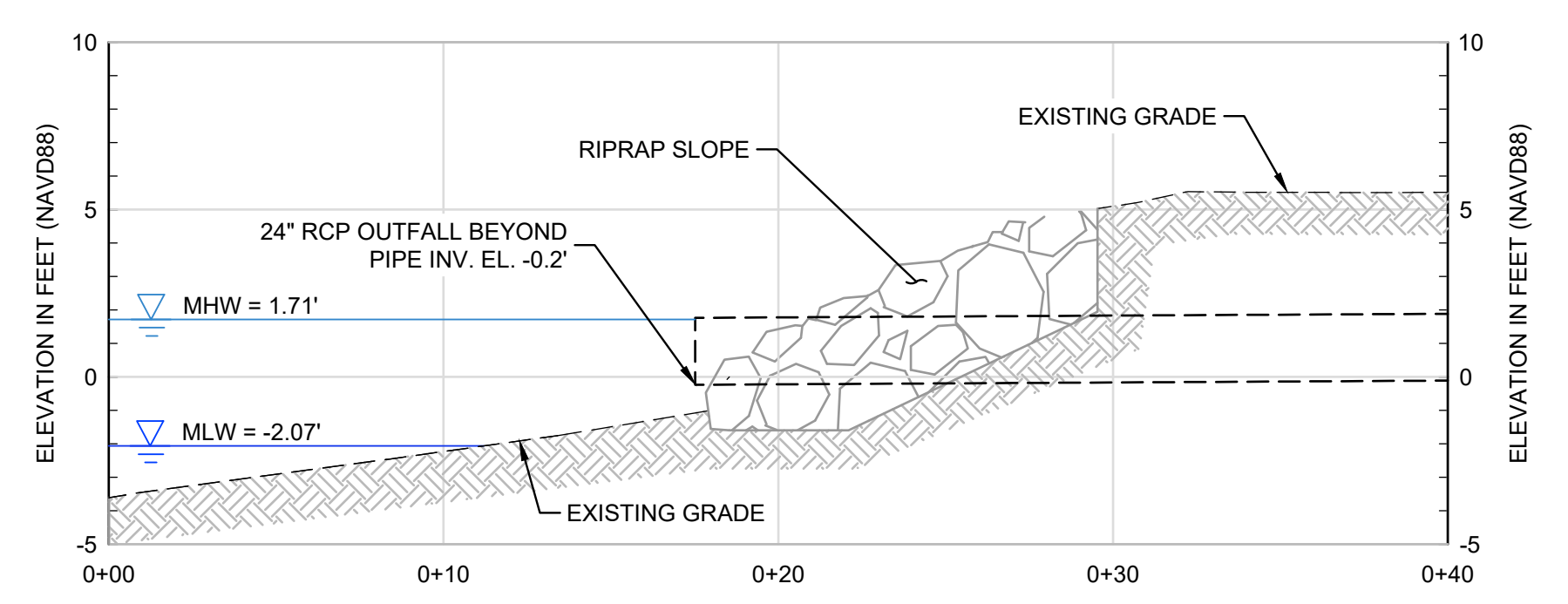
CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

DEMOLITION SECTIONS

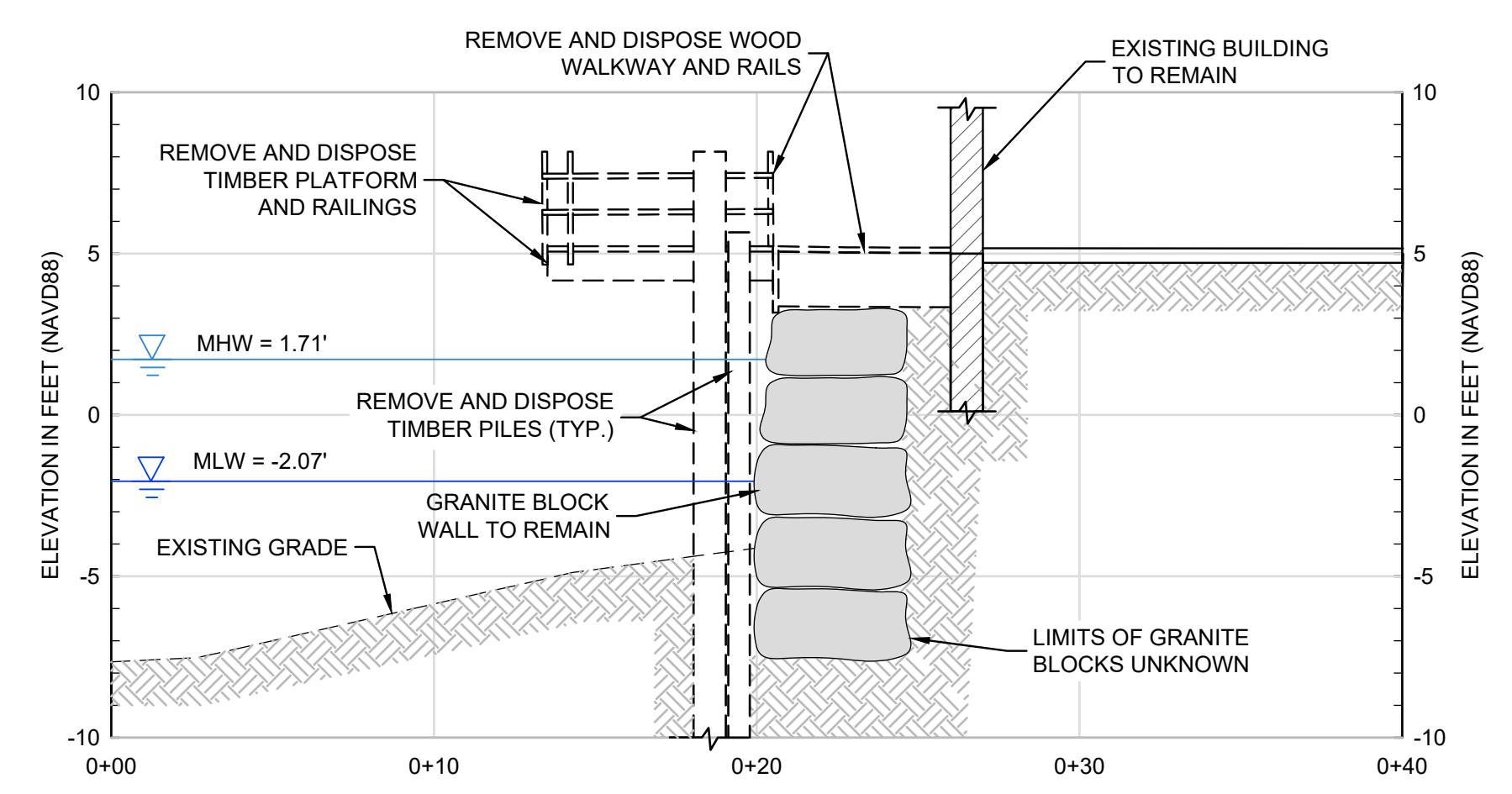
DRAWING
8
SHEET NO. 8 OF 15



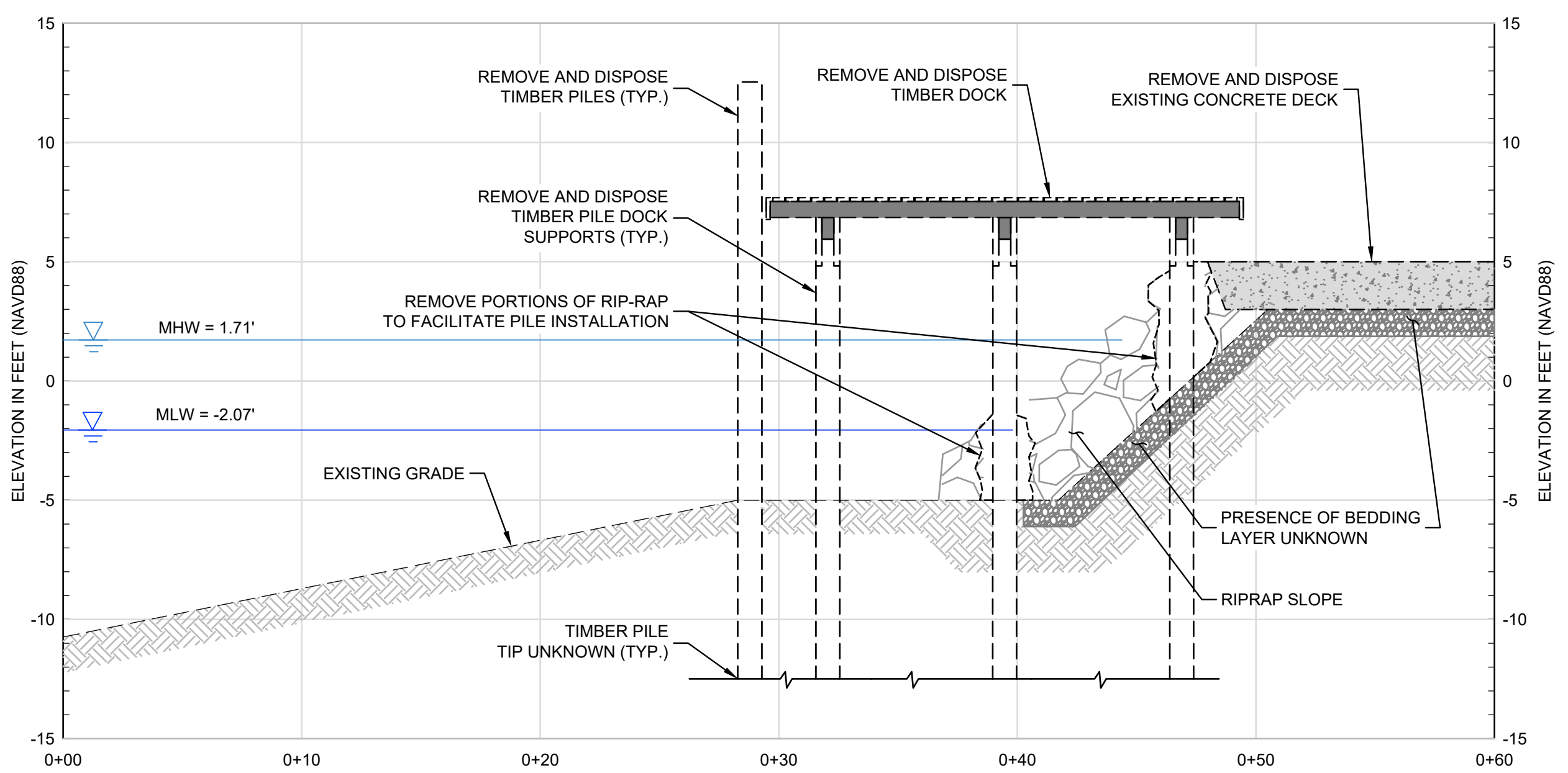
A RIPRAP SLOPE / PARKING LOT - SECTION
SCALE: 1"=5'



B RIPRAP SLOPE / PARKING LOT - SECTION
SCALE: 1"=5'



C GRANITE BLOCK WALL - SECTION
SCALE: 1"=5'



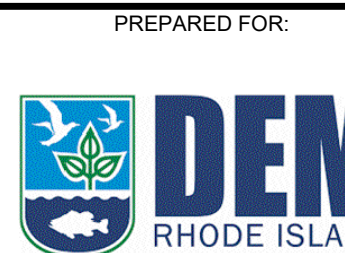
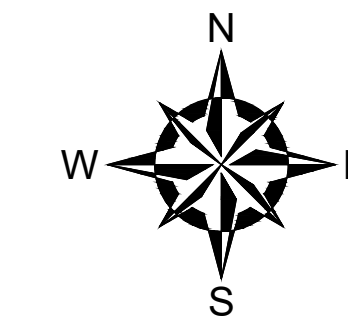
D DOCK PLATFORM - SECTION
SCALE: 1"=5'

NOTES:

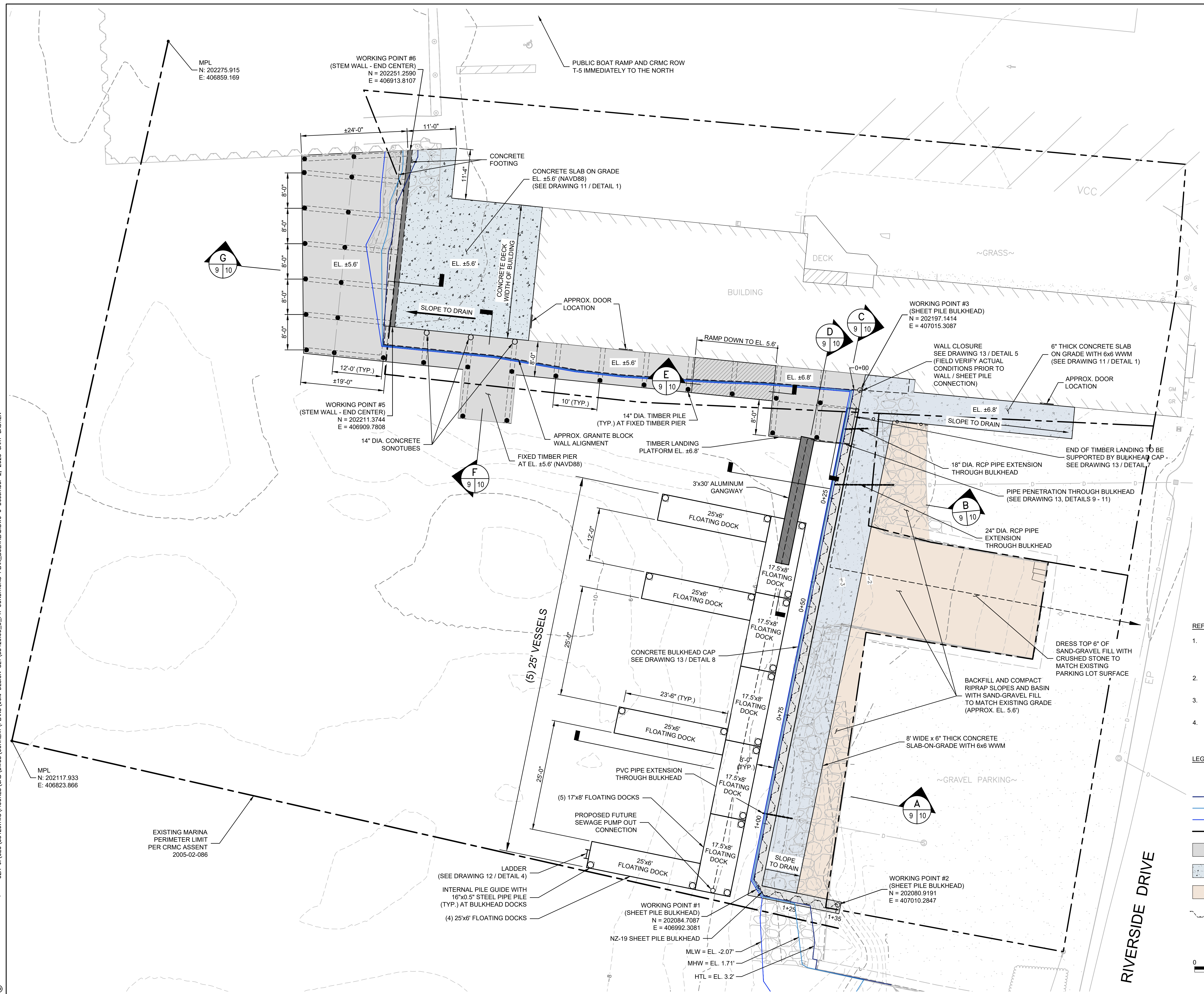
- LIMITS AND EXTENT OF BELOW GRADE RIPRAP, BEDDING LAYERS, BUILDING FOUNDATIONS, AND STONE WALLS ARE UNKNOWN.



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REFERENCE NOTES:

- BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DORSET SURVEY TRANSMITTED TO GZA ON SEPTEMBER 16, 2024.
- BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024.
- PROVIDE CONSTRUCTION JOINTS AT 20-FOOT O.C. MINIMUM.
- CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING AND ESTABLISHING NEW SITE CONTROL AND WORKING POINTS IF NECESSARY.

LEGEND:

- STEEL GUIDE PILE
- TIMBER DECK SUPPORT PILE
- HIGH TIDE LINE
- MEAN HIGH WATER
- MEAN LOW WATER
- PROPERTY LINE
- TIMBER DECK
- CONCRETE DECK
- SAND - GRAVEL FILL
- STEEL SHEETPILE BULKHEAD



100% DESIGN DRAWINGS
NOT FOR CONSTRUCTION

REV.	DESCRIPTION	DATE	BY

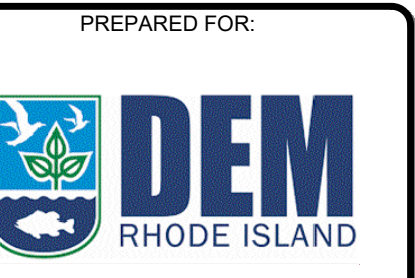
PROJECT NO:	35429.00
DATE:	DECEMBER, 2025
PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
 169 RIVERSIDE DRIVE
 TIVERTON, RHODE ISLAND
**PROPOSED CONDITIONS PLAN
 PHASE 1**

DRAWING
9
SHEET NO. 9 OF 15



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100% DESIGN DRAWINGS NOT FOR CONSTRUCTION

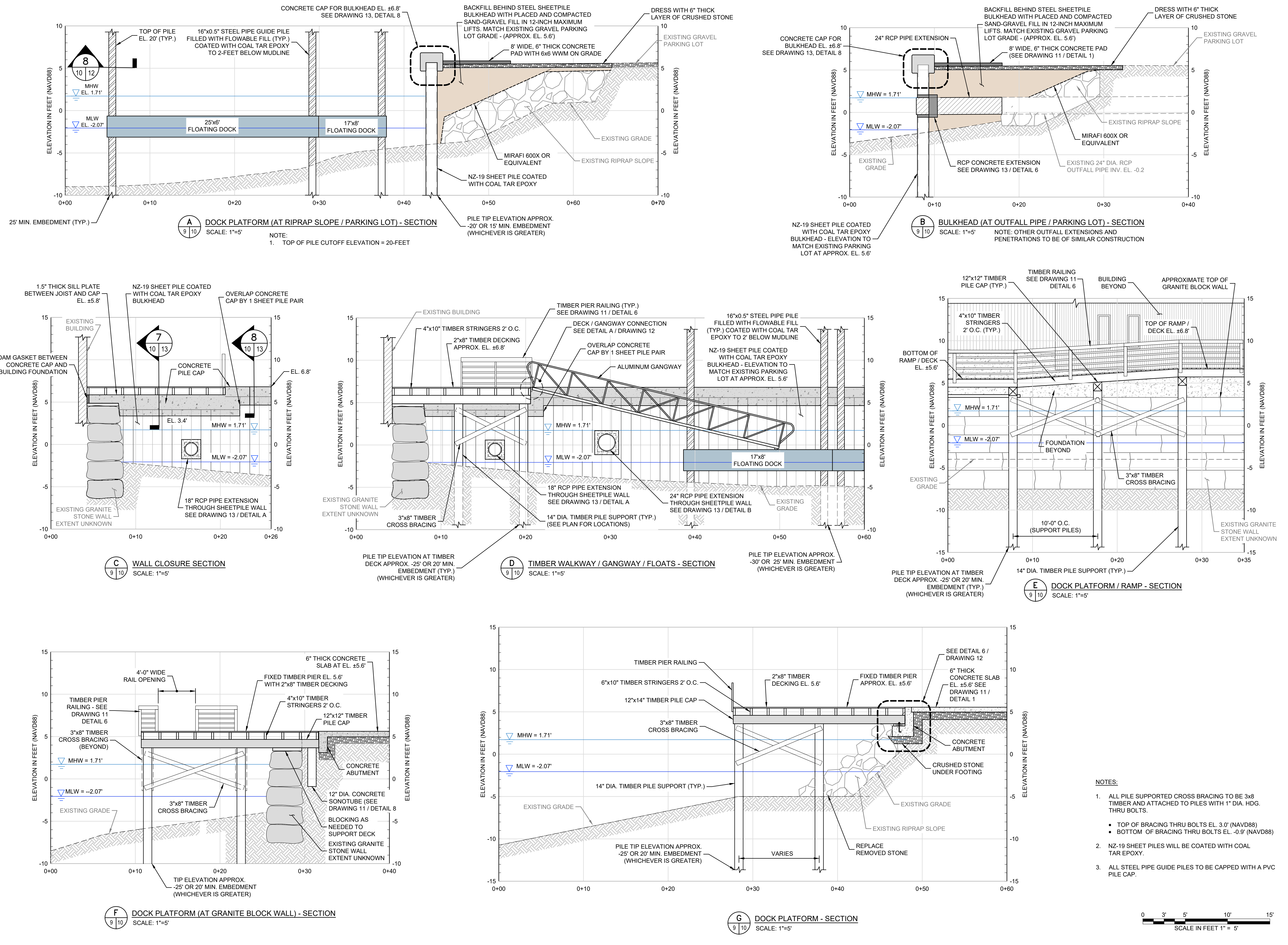
REV DESCRIPTION DATE BY

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CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

PROPOSED CONDITIONS SECTIONS
PHASE 1

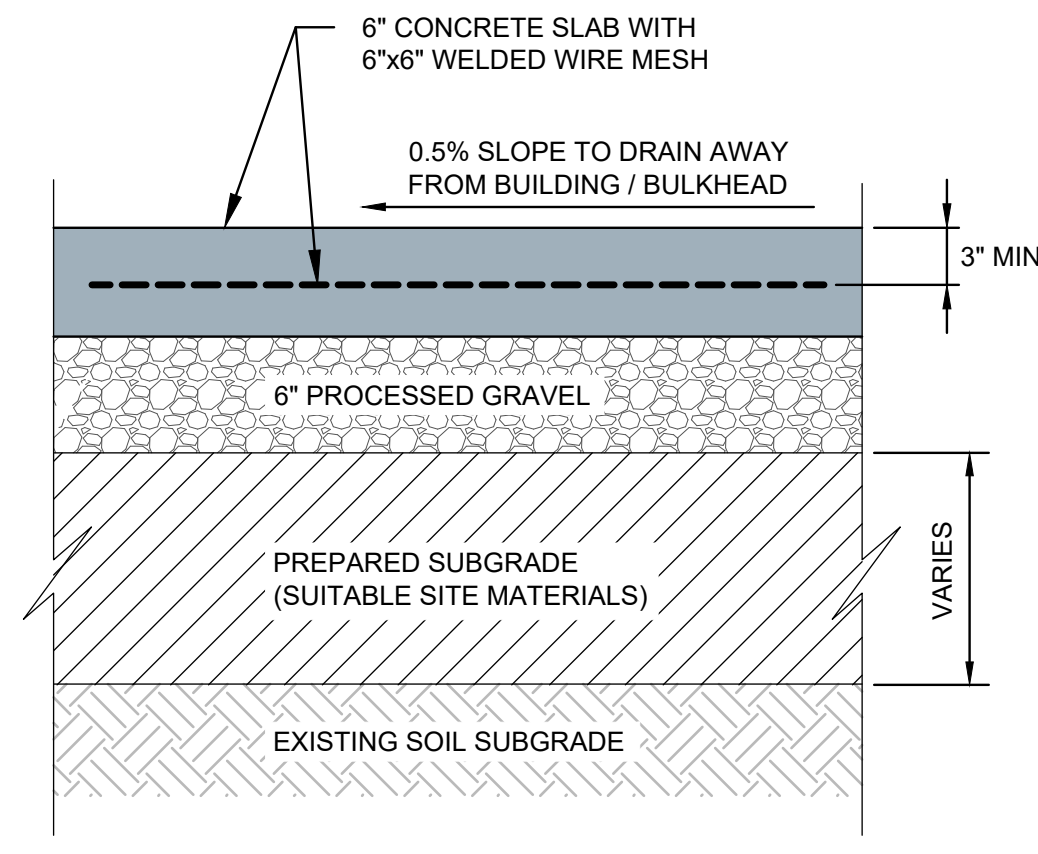
DRAWING
10
SHEET NO. 10 OF 15



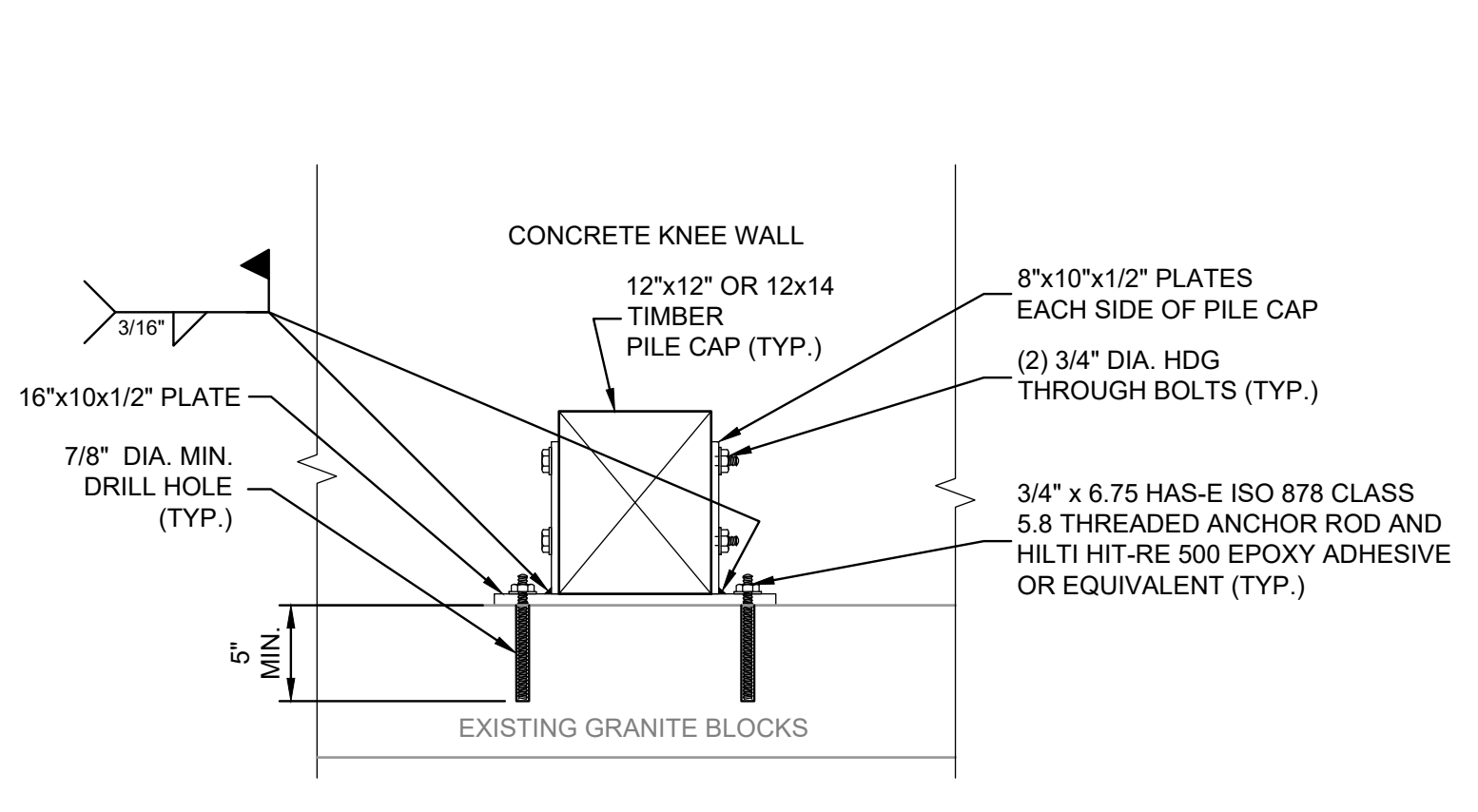
- NOTES:
- ALL PILE SUPPORTED CROSS BRACING TO BE 3x8 TIMBER AND ATTACHED TO PILES WITH 1" DIA. HDG. THRU BOLTS.
 - TOP OF BRACING THRU BOLTS EL. 3.0' (NAVD88)
 - BOTTOM OF BRACING THRU BOLTS EL. -0.9' (NAVD88)
 - NZ-19 SHEET PILES WILL BE COATED WITH COAL TAR EPOXY.
 - ALL STEEL PIPE GUIDE PILES TO BE CAPPED WITH A PVC PILE CAP.



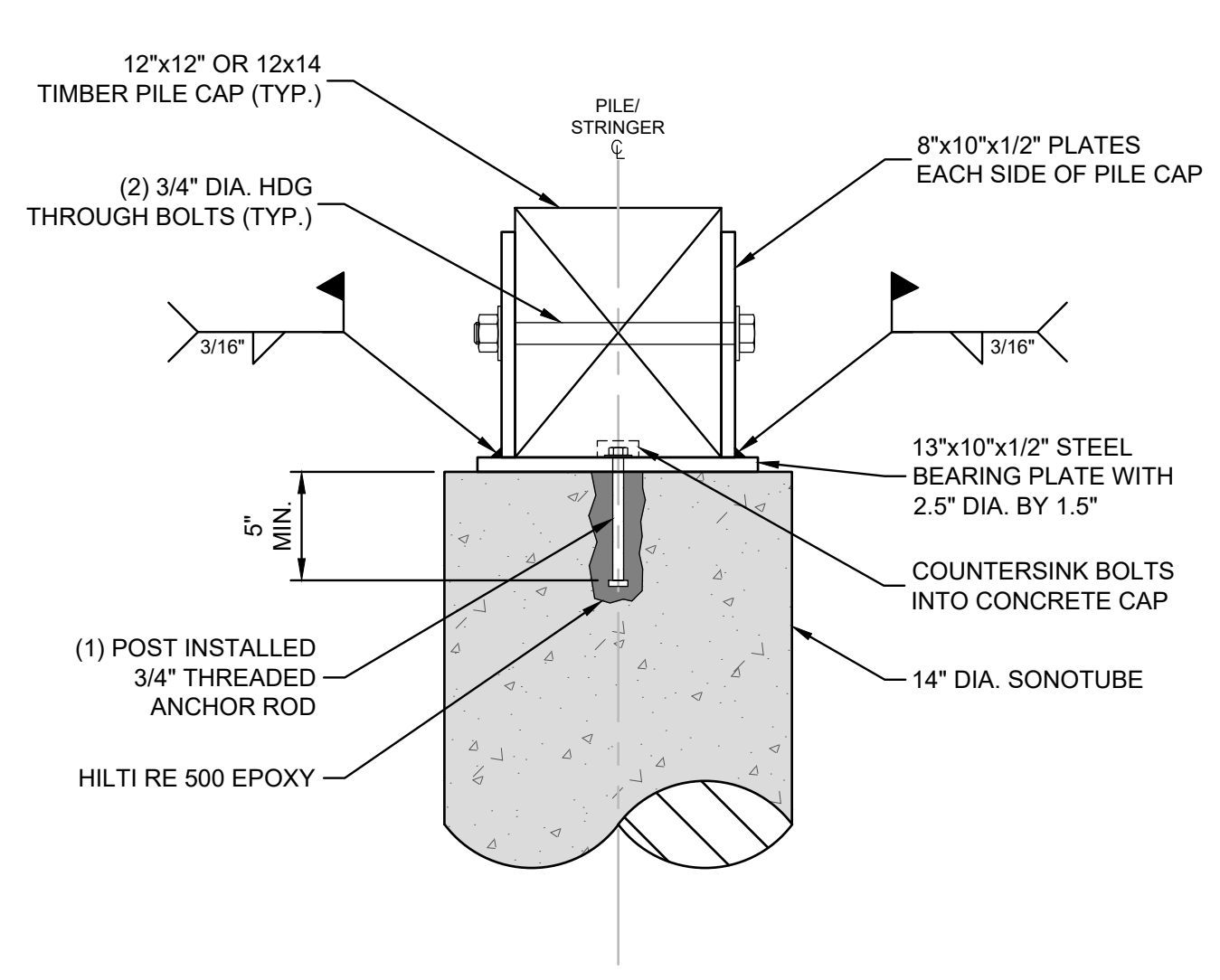
© 2025 - GZA Geoenvironmental, Inc. GZA-JTF (GEO) 35429-TWS (FIGURES) CAD (DWGS) CURRENT PLANS PERMITTING SET_10-23-25 35429.00_10_FR CONDITIONS SECTIONS_PHASE 1.DWG 10 December 3, 2025 GARY BASTEN



1 CONCRETE SLAB-ON-GRADE DETAIL
SCALE: 1" = 4'

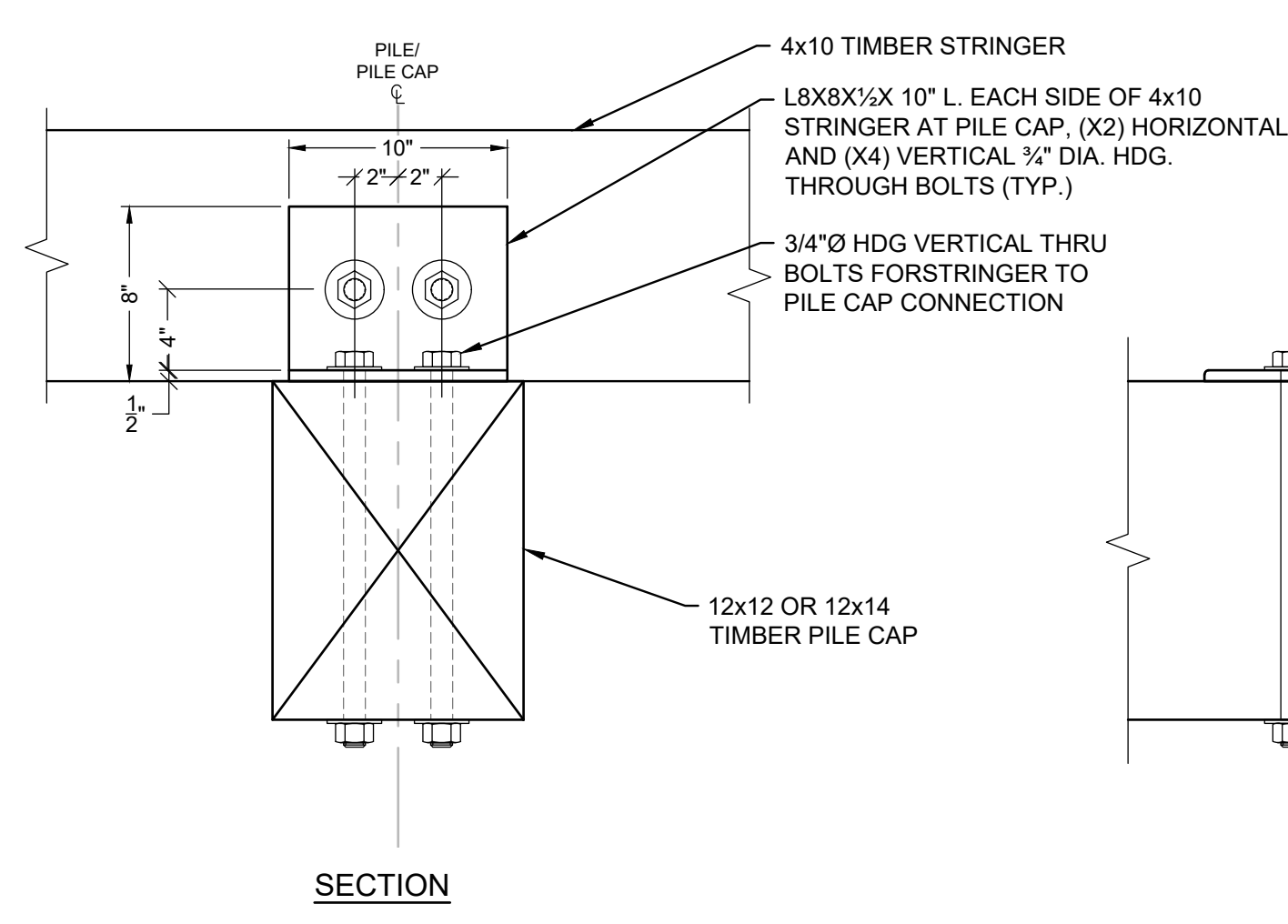


2 TIMBER PILE CAP TO GRANITE BLOCK CONNECTION DETAIL
NOT TO SCALE

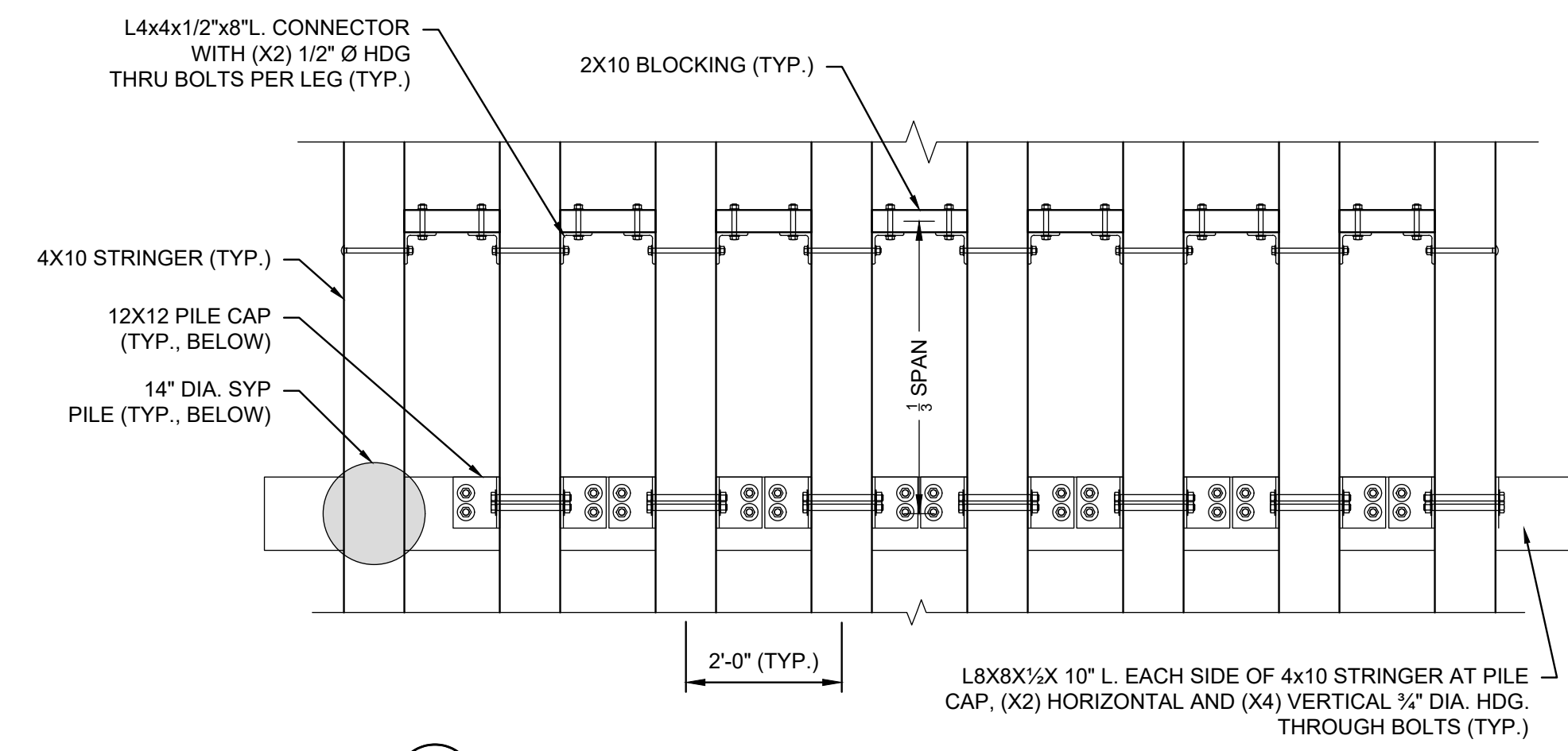
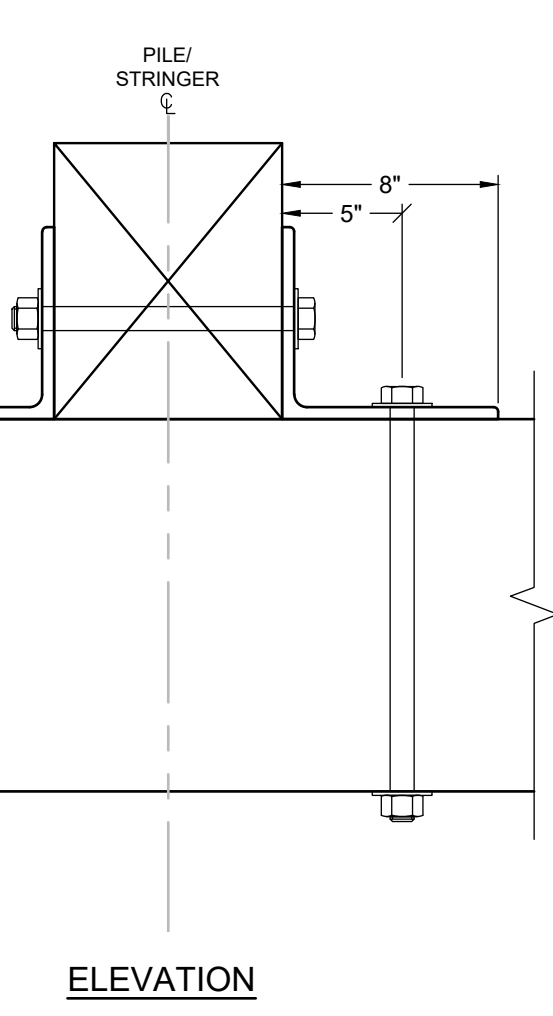


3 TIMBER PILE CAP TO SONOTUBE CONNECTION
SCALE: 1-1/2"=1'

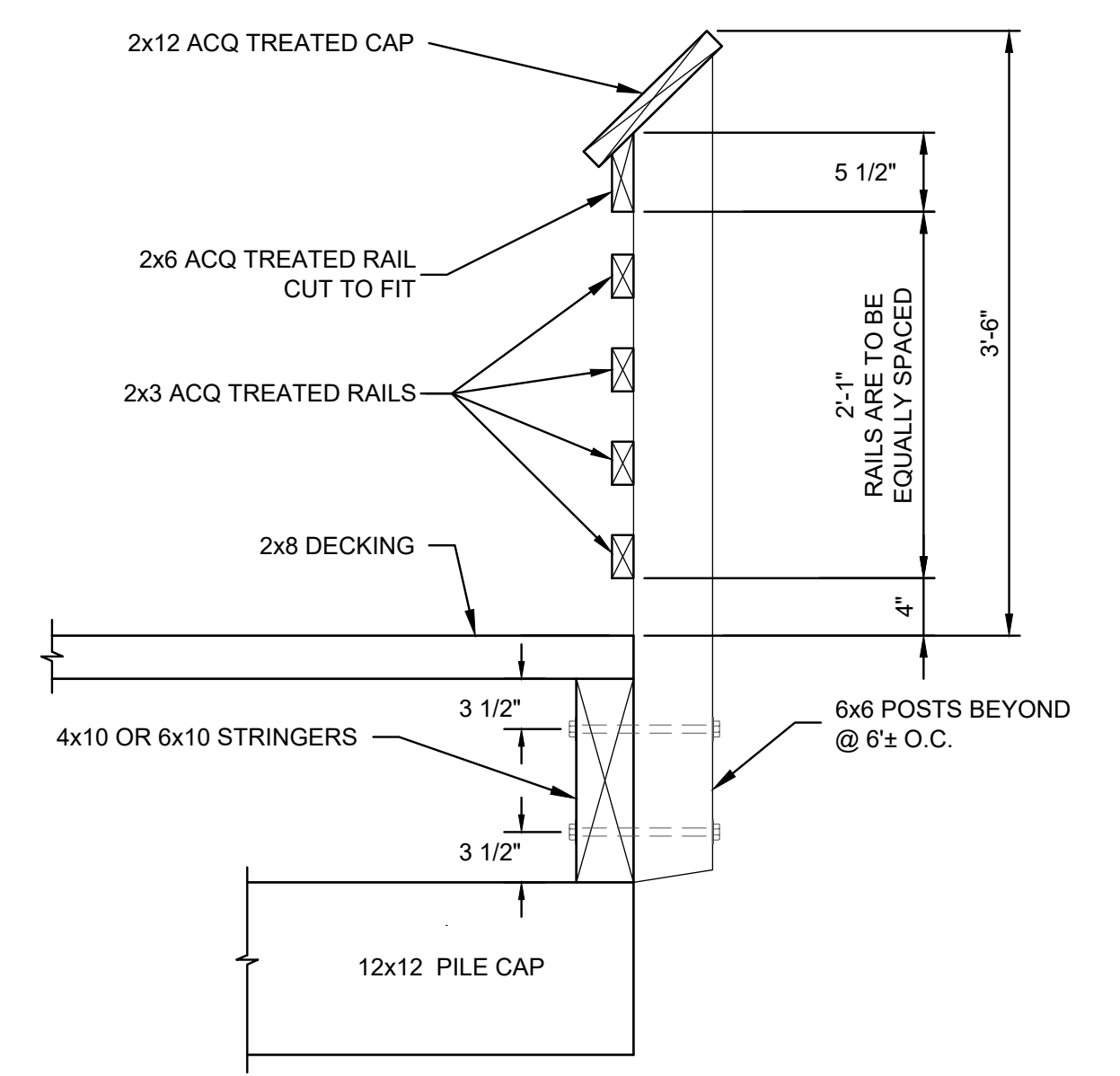
- NOTES:**
- TIMBER MEMBERS LOCATED BELOW THE DECKING SHALL BE SOUTHERN YELLOW PINE (SYP) GRADE NO. 2, TREATED WITH 2.5# CCA, UNLESS NOTED OTHERWISE.
 - TIMBER PILE CAPS IN THE ALIGNED IN THE EAST-WEST DIRECTION SHALL BE SYP GRADE NO. 1, TREATED WITH 2.5# CCA.
 - TIMBER MEMBERS LOCATED ON OR ABOVE THE PIER DECKING SHALL BE SYP GRADE NO. 2, TREATED WITH 0.6# ACQ, UNLESS OTHERWISE NOTED.
 - ALL BOLTS, PLATES, AND MISCELLANEOUS METALS IN CONTACT WITH TIMBER TREATED WITH ACQ SHALL BE TYPE 316 STAINLESS STEEL OR HAVE PVC SLEEVES & PVC FLAT STOCK TO PREVENT HDG STEEL CONTACT WITH ACQ-PRESERVED TIMBER.
 - TIMBER DECKING SHALL BE FASTENED TO THE STRINGERS WITH #8 - 2 1/2" LONG WOOD SCREWS. MINIMUM 2 SCREWS EACH LOCATION PER STRINGER PER PLANK.
 - ALL PILE SUPPORTED CROSS BRACING TO BE 3x8 AND ATTACHED TO PILES WITH 1" DIA. HDG. THRU BOLTS.
 - CONTRACTOR SHALL PLACE 15# ASPHALT ROOFING FELT (TAR PAPER) BETWEEN HORIZONTAL TIMBER TO TIMBER OR CONCRETE INTERFACES AND ALL TIMBER TO STEEL INTERFACES.
 - EPOXY ADHESIVE FOR ANCHORS DRILLED AND SET IN CONCRETE SHALL BE HILTI HIT-RE 500 V3, OR APPROVED EQUIVALENT.
 - INSTALL CONSTRUCTION JOINTS AT 40-FOOT MAXIMUM ON-CENTER FOR CONCRETE SLABS ON GRADE.
 - INSTALL CONTROL JOINTS AT 20-FOOT MAXIMUM ON-CENTER BETWEEN CONSTRUCTION JOINTS.



4 INTERIOR STRINGER TO PILE CAP CONNECTION DETAILS
SCALE: 1-1/2"=1'

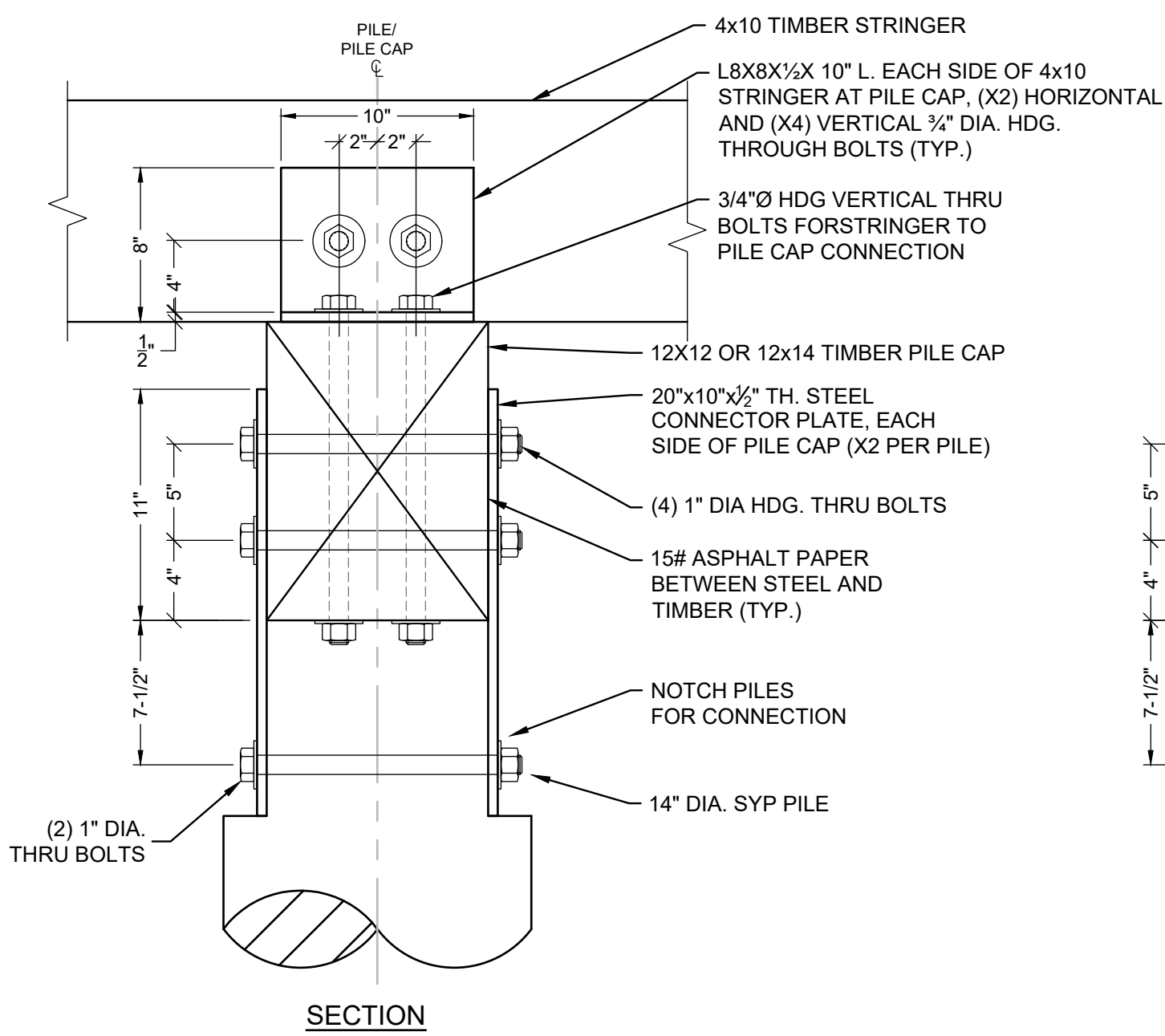


5 STRINGER BLOCKING CONNECTION DETAIL (AT GRANITE BLOCK WALL)
SCALE: 1"=2'

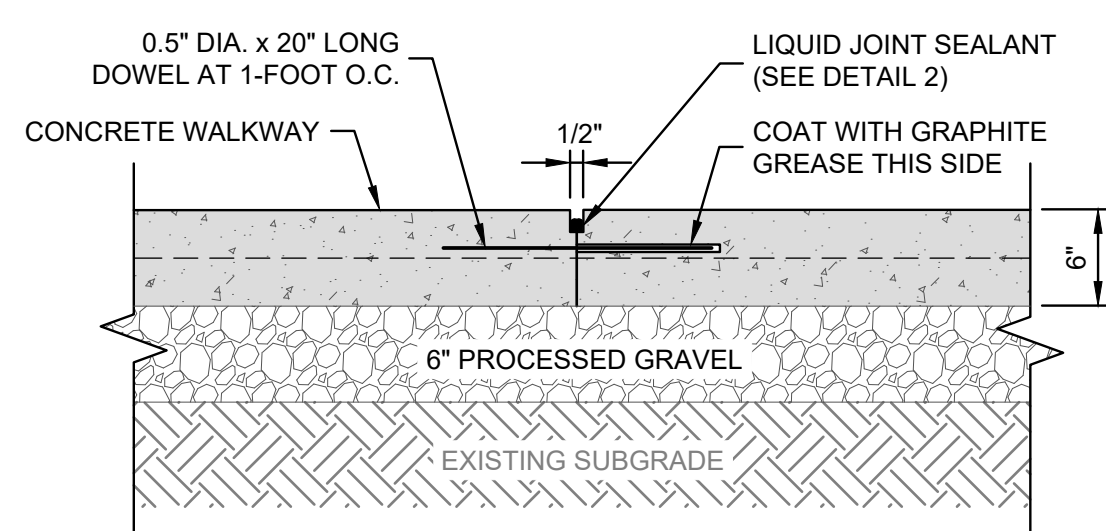
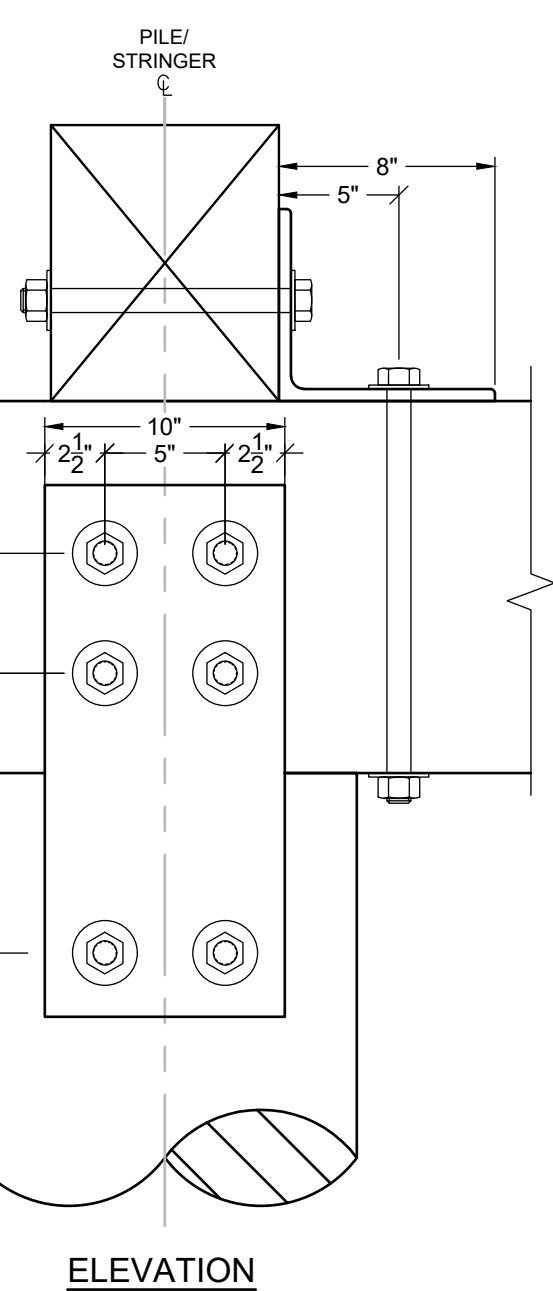


6 TIMBER RAIL - SECTION
SCALE: 1" = 1'

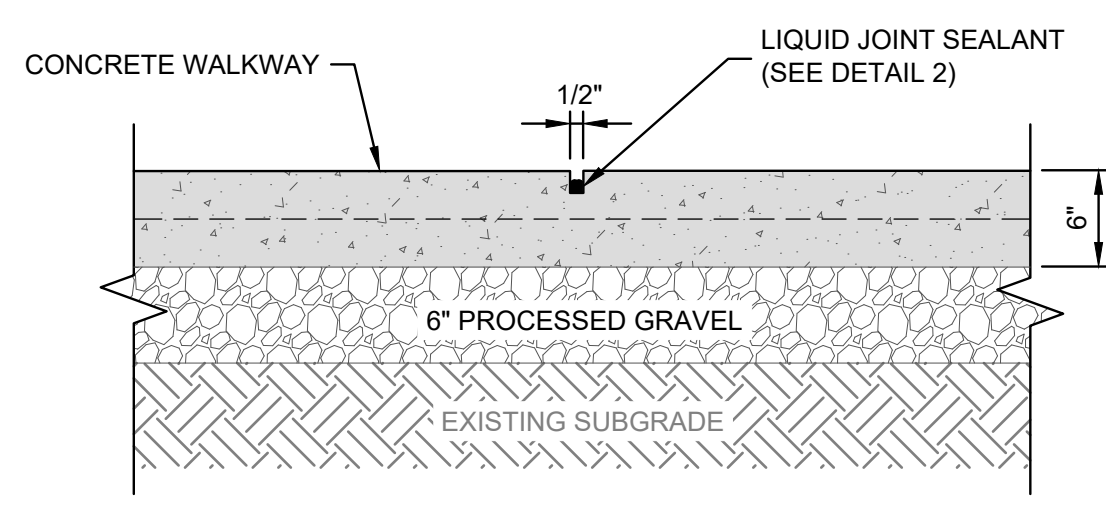
NOTES: SEE NOTE 4



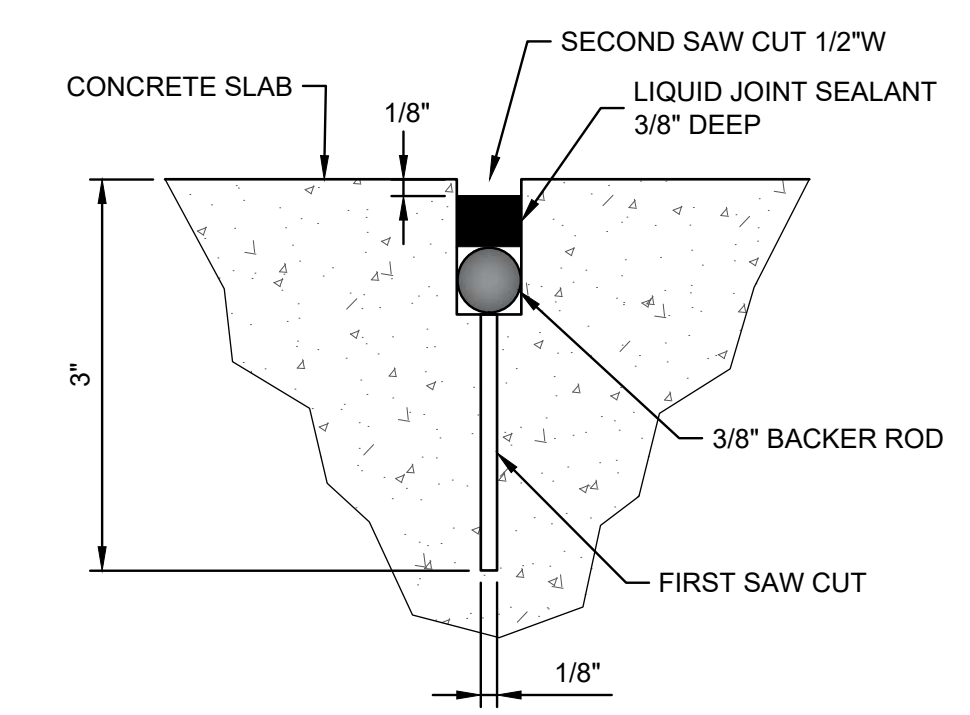
7 EXTERIOR STRINGER TO PILE CAP AND PILE CAP TO PILE CONNECTION - DETAILS
SCALE: 1-1/2"=1'



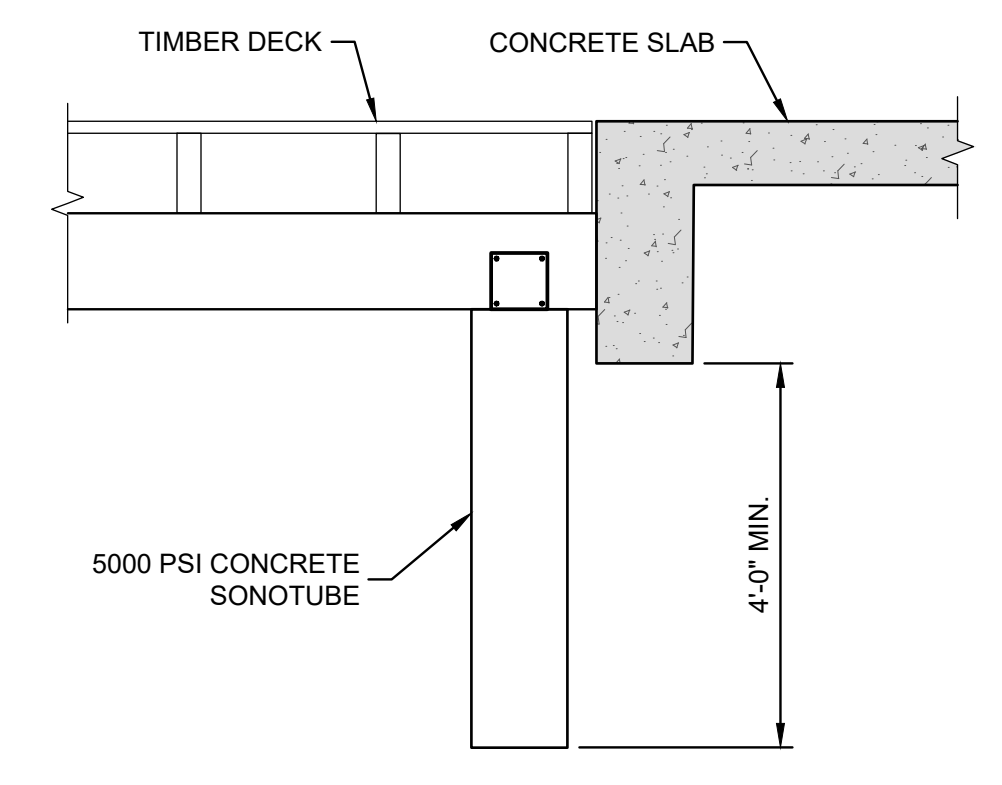
8 CONCRETE WALKWAY JOINT DETAILS
SCALE: 1"=1'



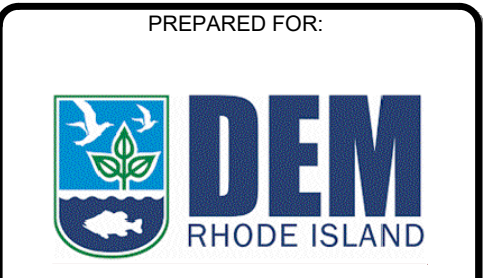
CONTROL JOINT DETAIL



9 SEALANT DETAIL
NOT TO SCALE



10 SONOTUBE - DETAIL
SCALE: 1" = 1'



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100% DESIGN DRAWINGS NOT FOR CONSTRUCTION

REV.	DESCRIPTION	DATE	BY

PROJECT NO: 35429.00
DATE: DECEMBER, 2025
PROJECT MGR: TWS
DESIGNED BY: JTF
DRAWN BY: GRB
CHECKED BY: TWS
REVIEWED BY: MJP
SCALE: AS NOTED
REVISION NO.

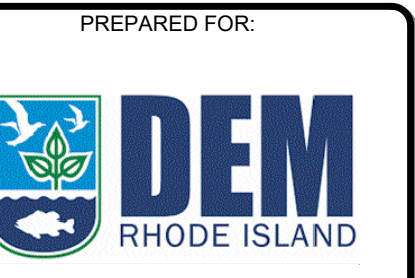
CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

**PROPOSED TIMBER DETAILS
PHASE 1**

DRAWING
11
SHEET NO. 11 OF 15



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100% DESIGN DRAWINGS
NOT FOR CONSTRUCTION

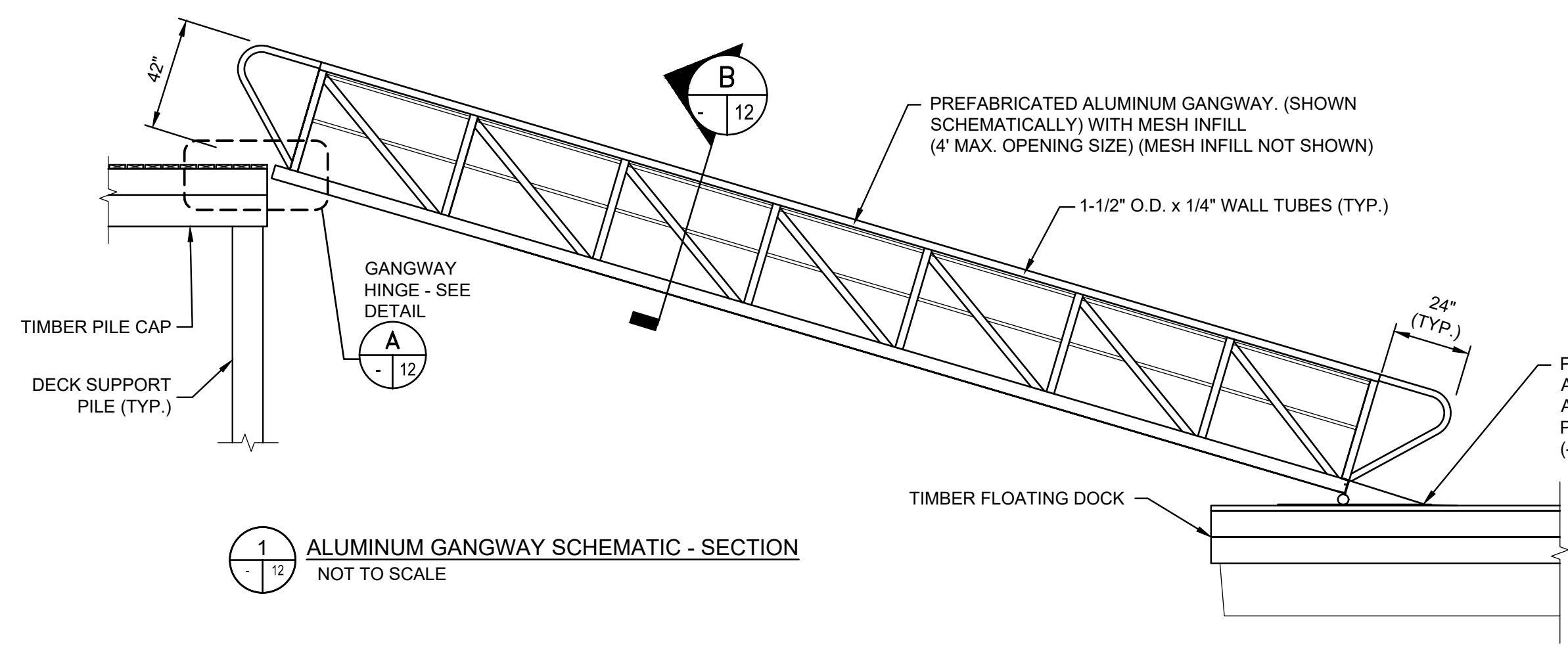
REV DESCRIPTION DATE BY

PROJECT NO:	35429.00
DATE:	DECEMBER, 2025
PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

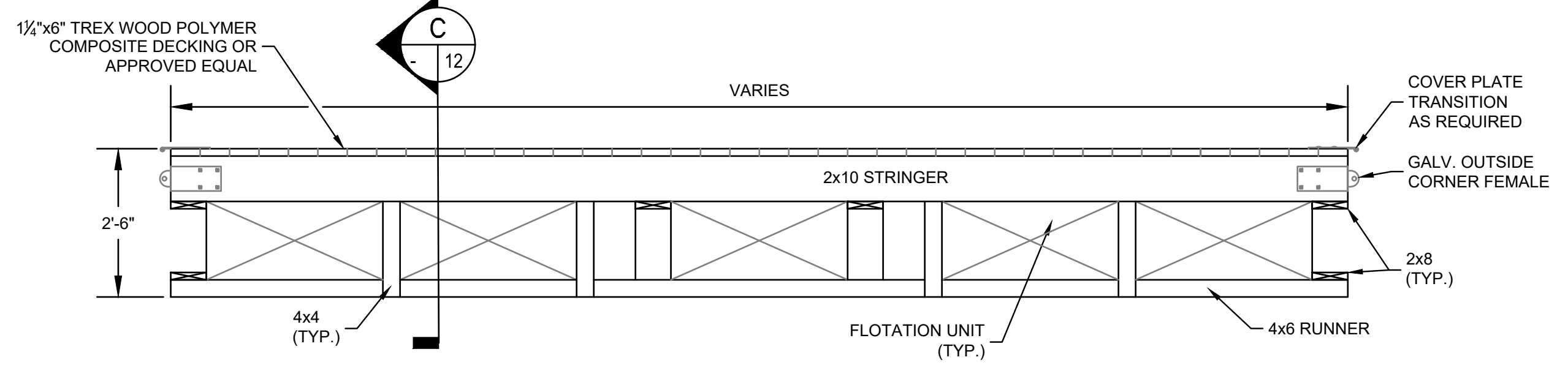
CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

PROPOSED GANGWAY - DOCK
DETAILS / PHASE 1

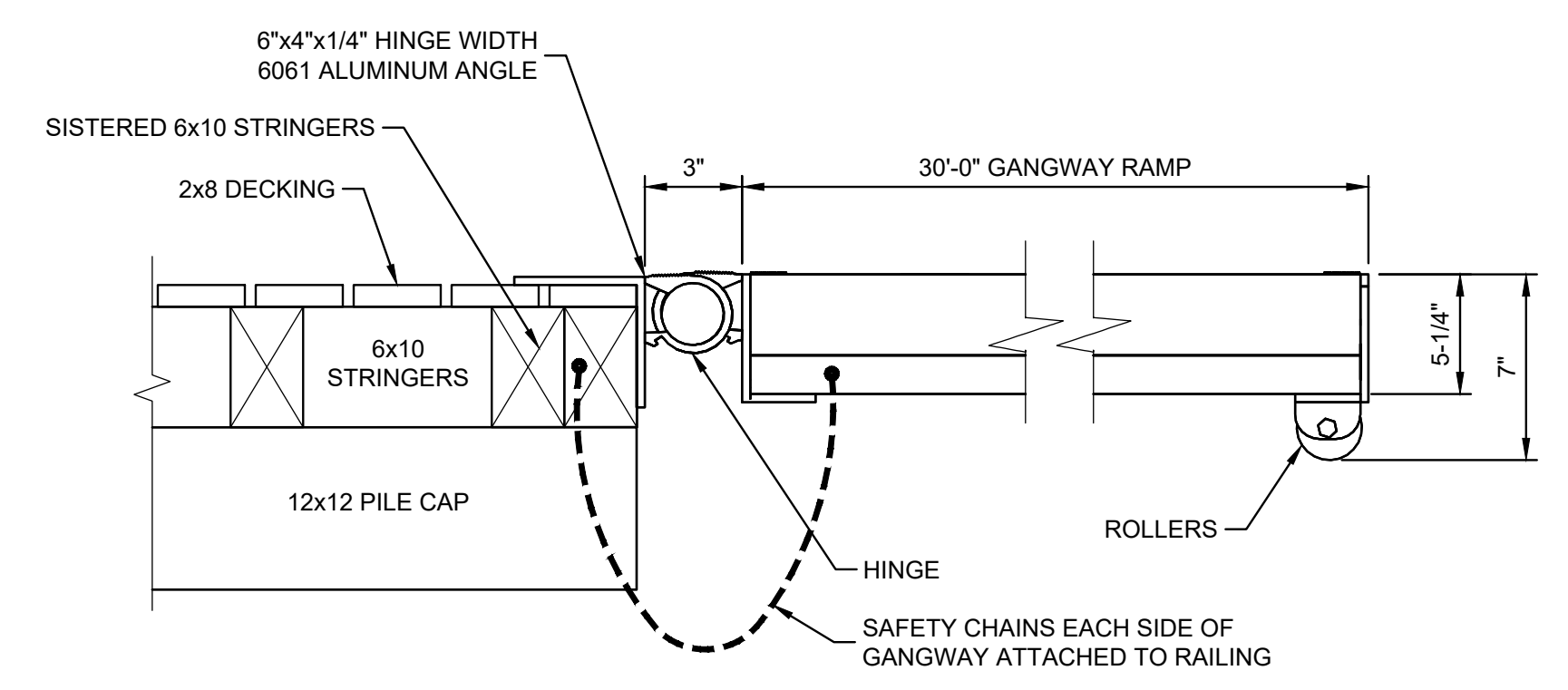
DRAWING
12
SHEET NO. 11 OF 15



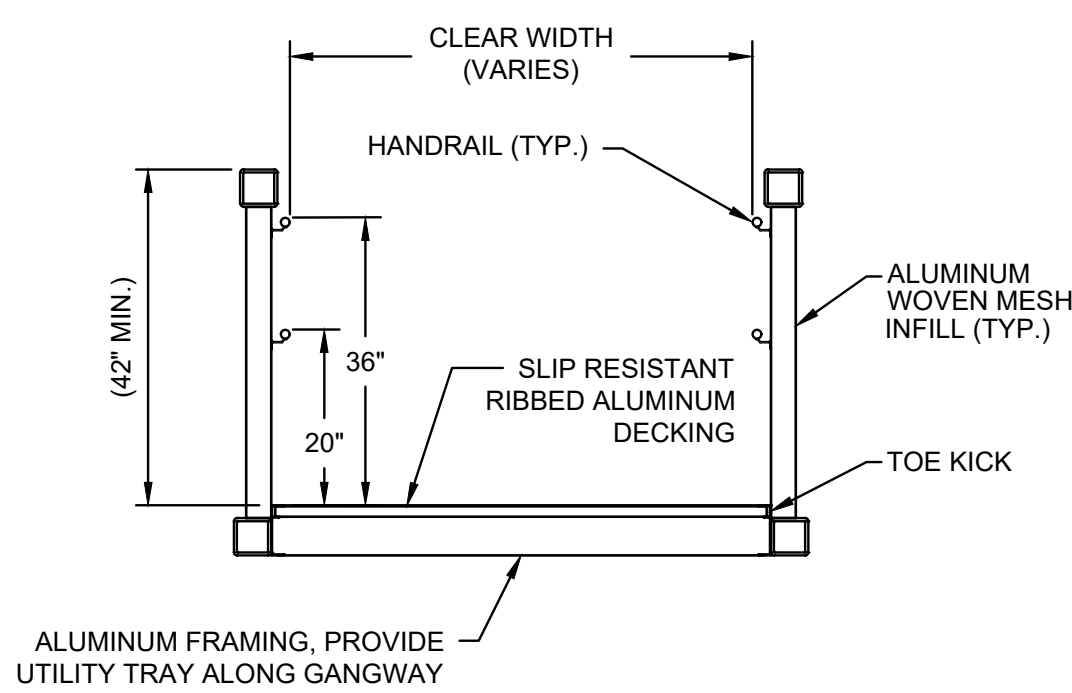
1 ALUMINUM GANGWAY SCHEMATIC - SECTION
NOT TO SCALE



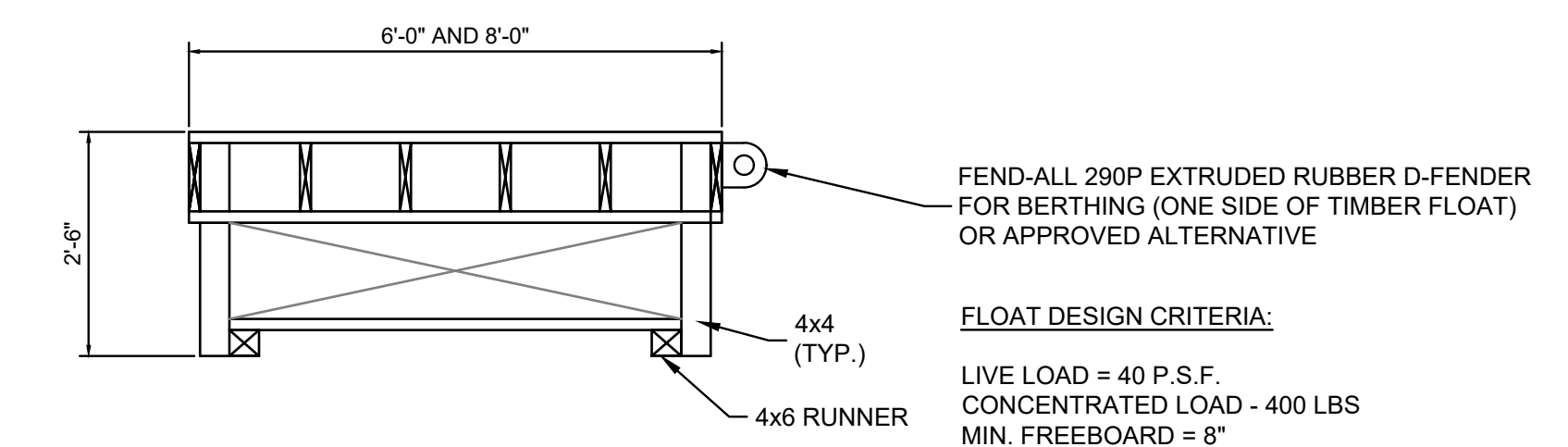
2 TIMBER FLOAT ELEVATION
SCALE 1"=2'



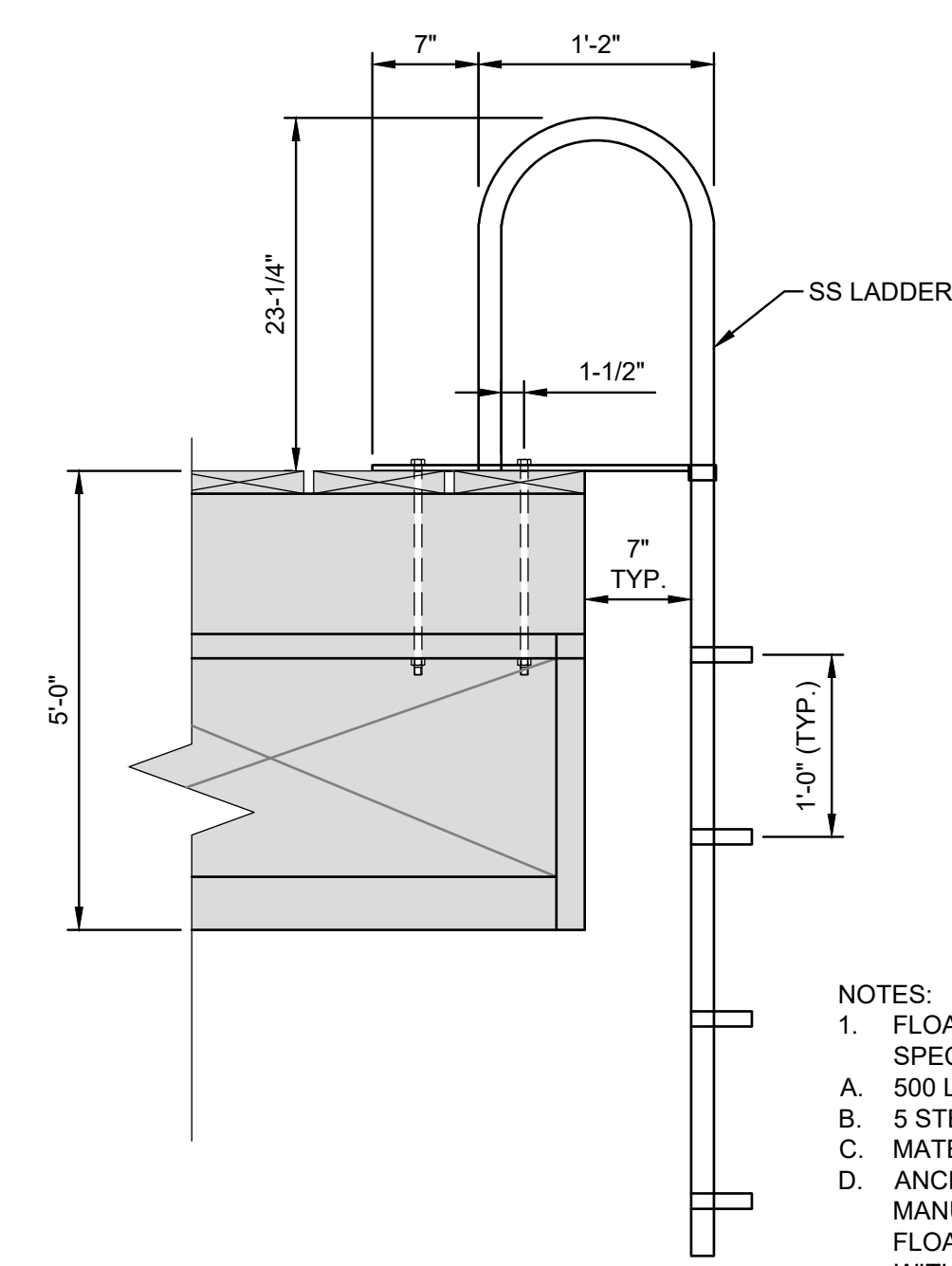
A TYPICAL GANGWAY - SECTION
NOT TO SCALE



B TYPICAL GANGWAY - SECTION
SCALE: 1"=2'

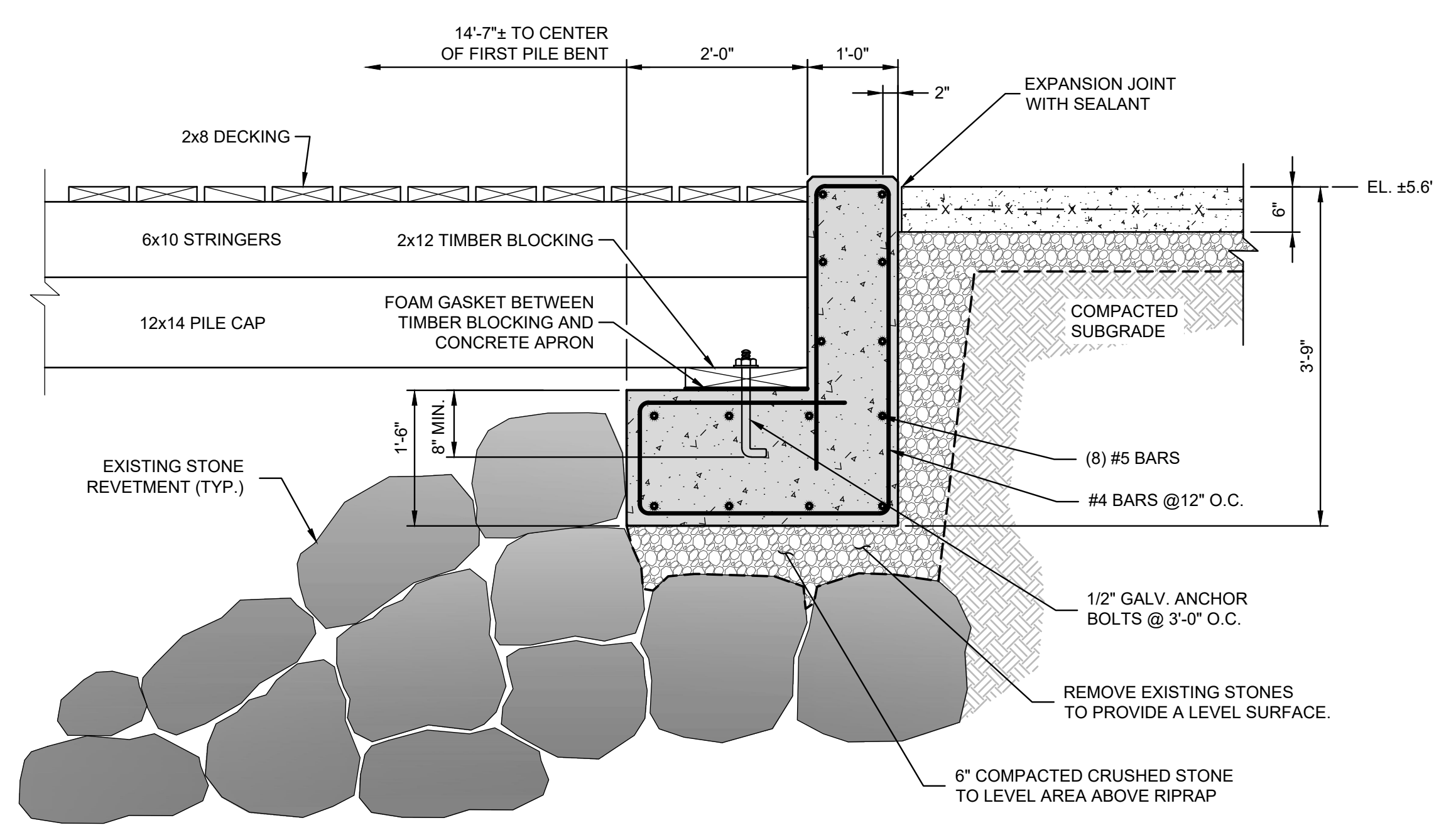


C TIMBER FLOAT SECTION
SCALE 1"=2'

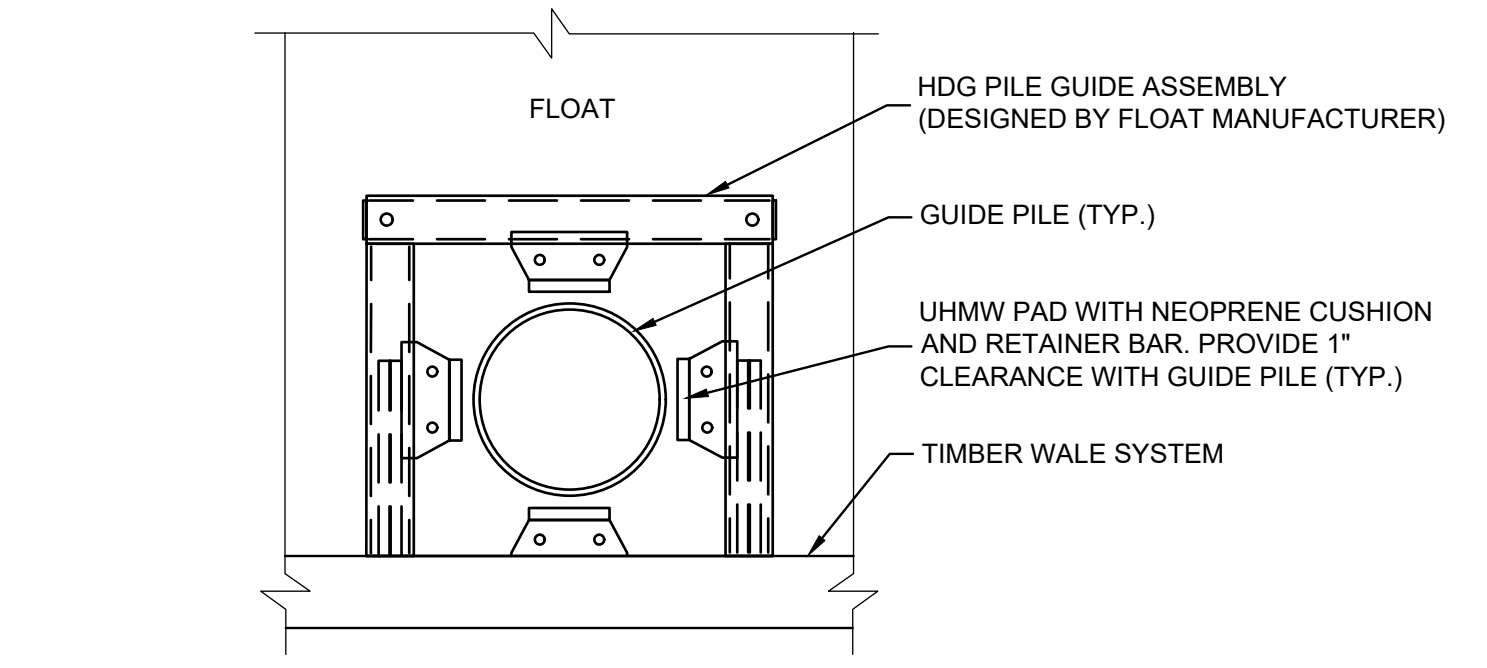


4 LADDER DETAIL
NOT TO SCALE

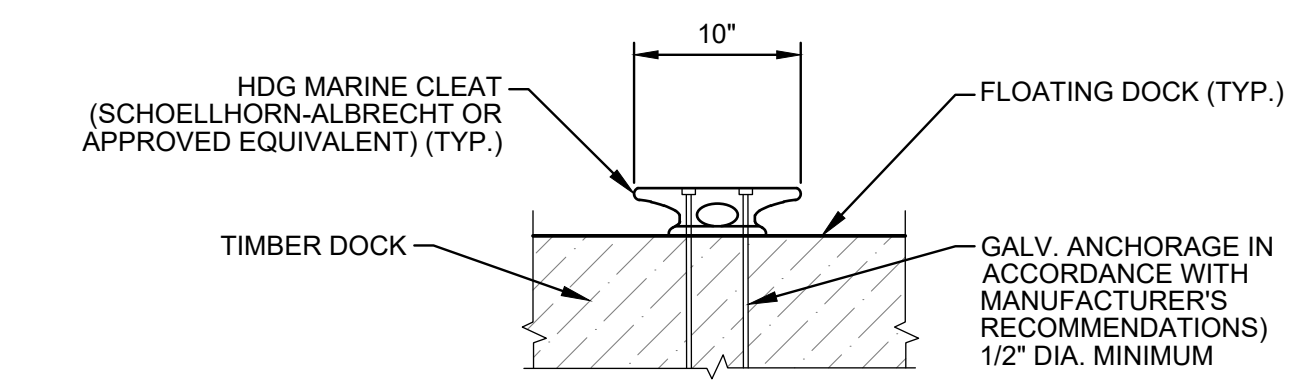
- NOTES:
- FLOATING DOCK LADDER SPECIFICATIONS:
 - A. 500 LBS CAPACITY
 - B. 5 STEPS, EACH STEP 4" WIDE
 - C. MATERIAL: STAINLESS STEEL
 - D. ANCHOR TO FLOAT PER MANUFACTURER'S RECOMMENDATIONS. FLOAT MANUFACTURER TO COORDINATE WITH LADDER MANUFACTURER FOR ANCHORAGE DETAILS.



5 DECKING / CONCRETE APRON - DETAIL
SCALE: 1"=1'



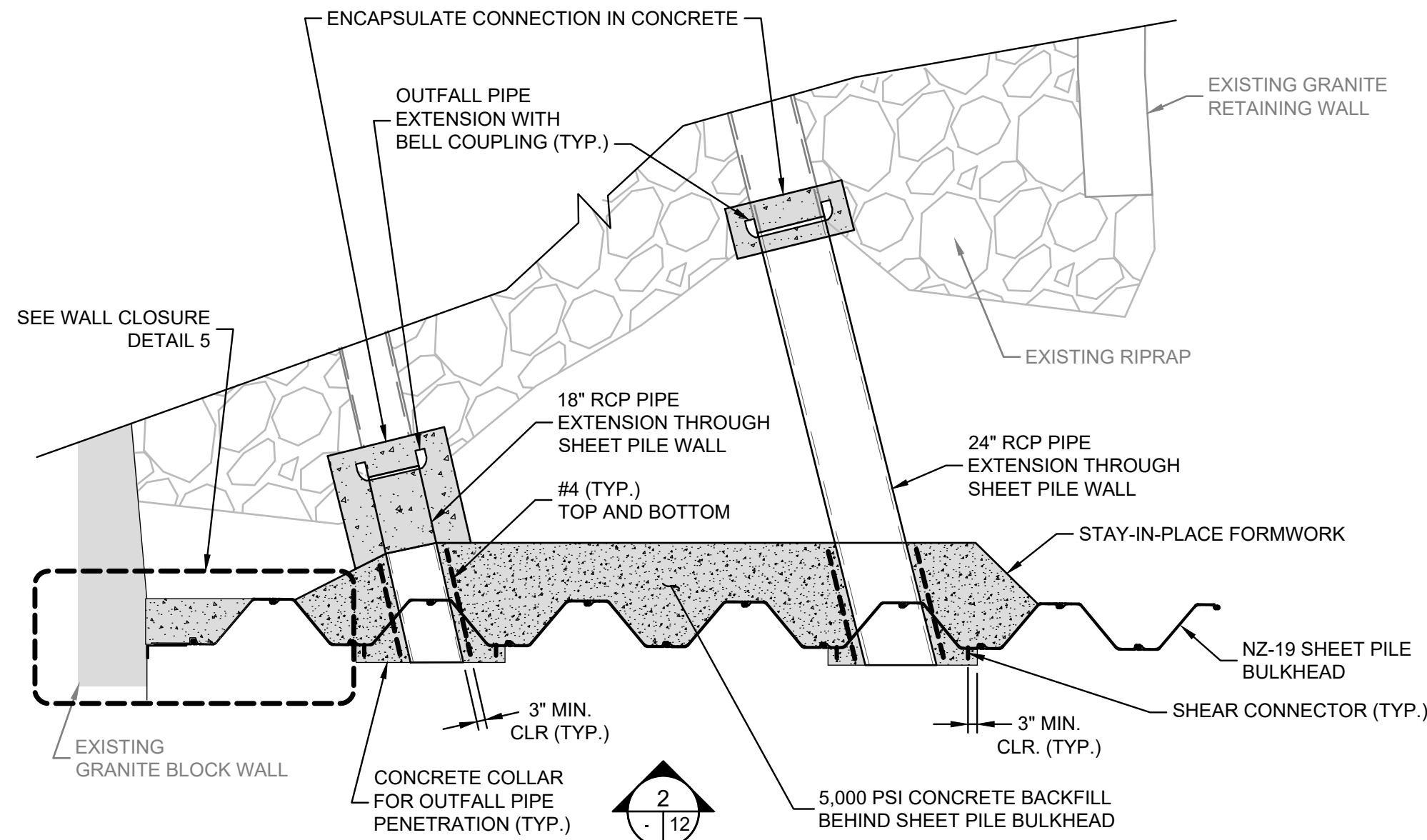
3 TYPICAL PILE GUIDE WITH UHMW PADS SCHEMATIC
SCALE: 1"=2'



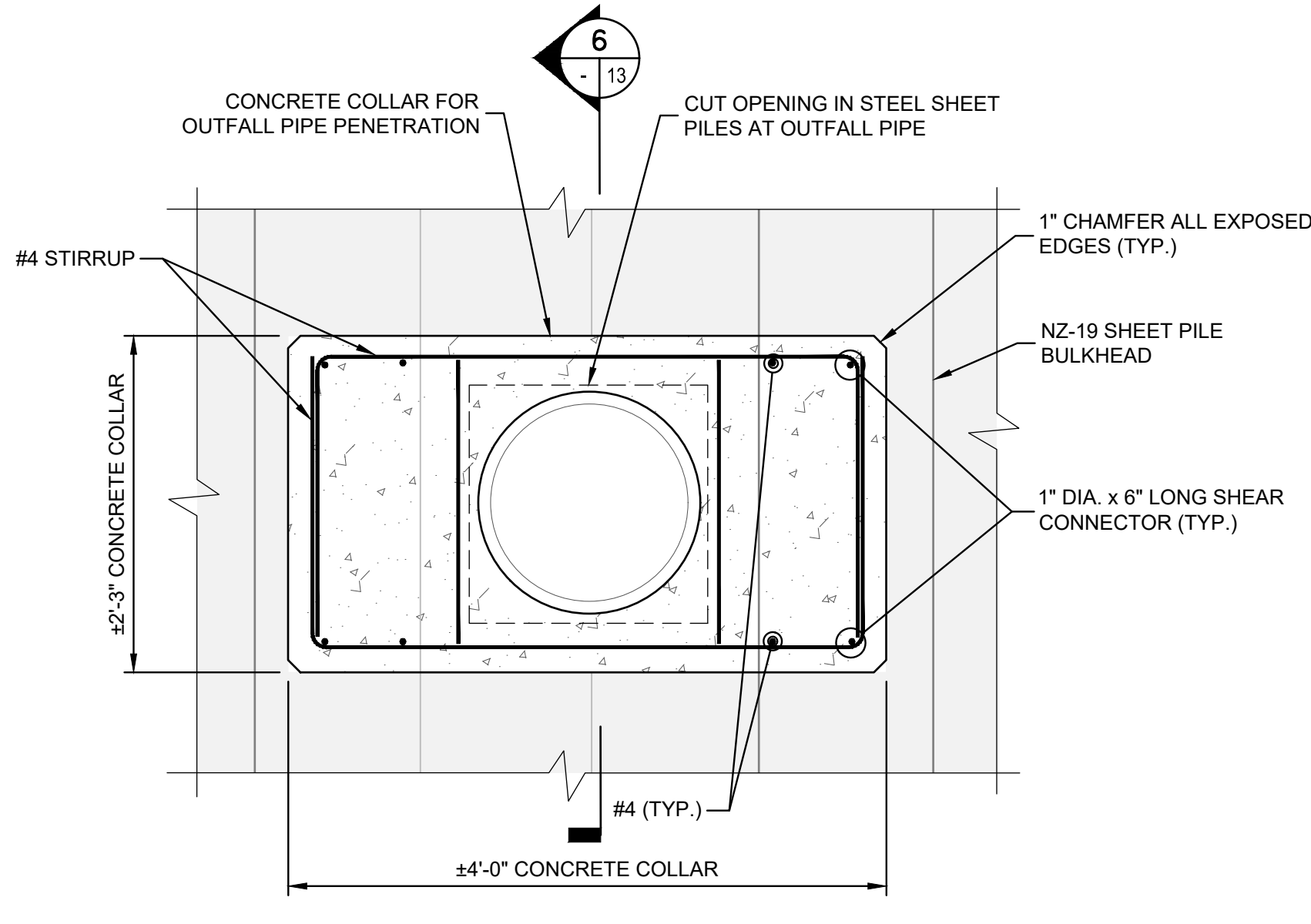
6 TYPICAL CLEAT DETAIL
NOT TO SCALE

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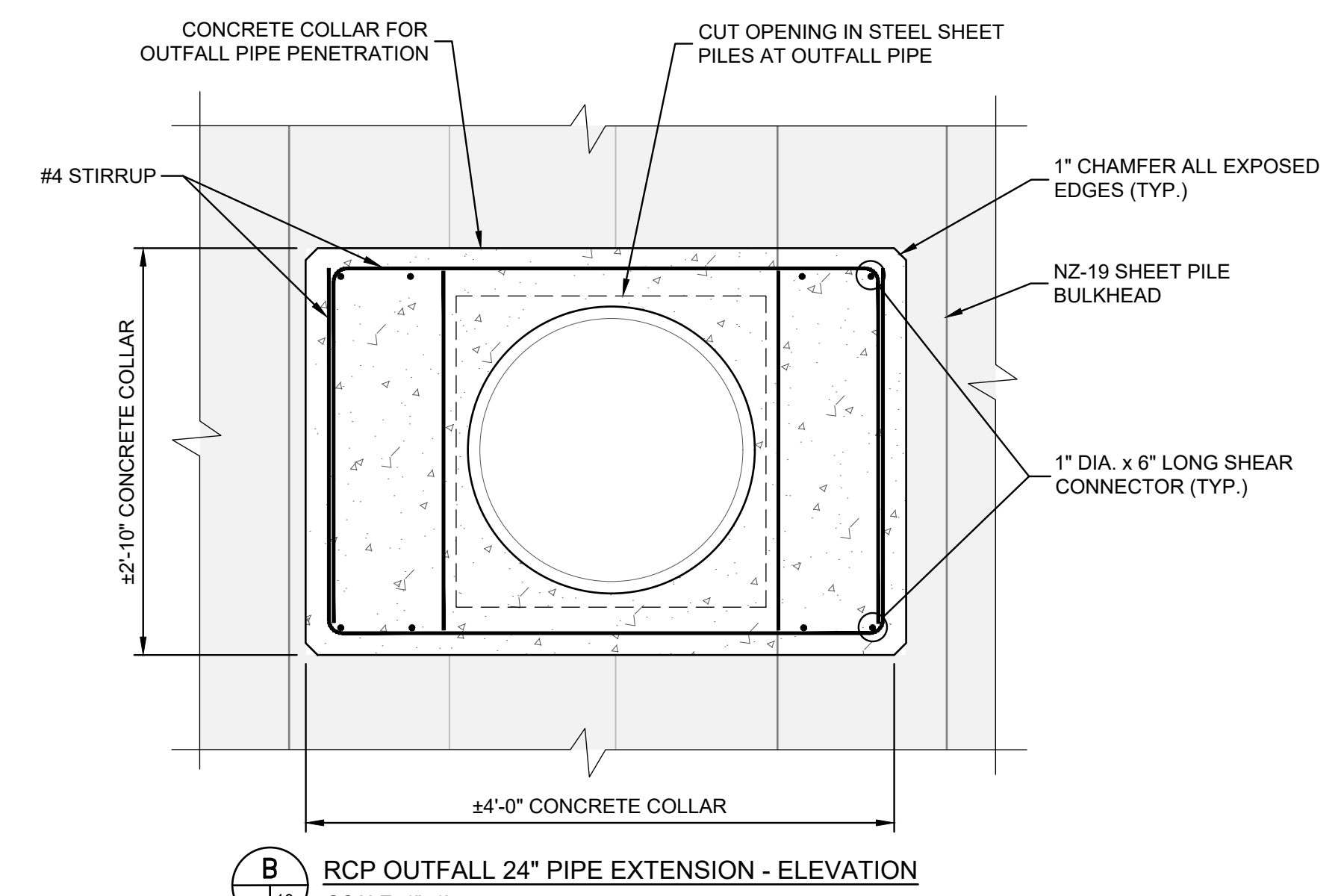
© 2025 - GZA GeoEnvironmental, Inc. GZA-J\GEO\35429-TWS\FIGURES\CAD\DWGS\CURRENT\PLANS\90% DESIGN SET\35429.00_13_PR BULKHEAD-OUTFALL DETAILS.DWG 13 December 3, 2025 GARY BASTIEN



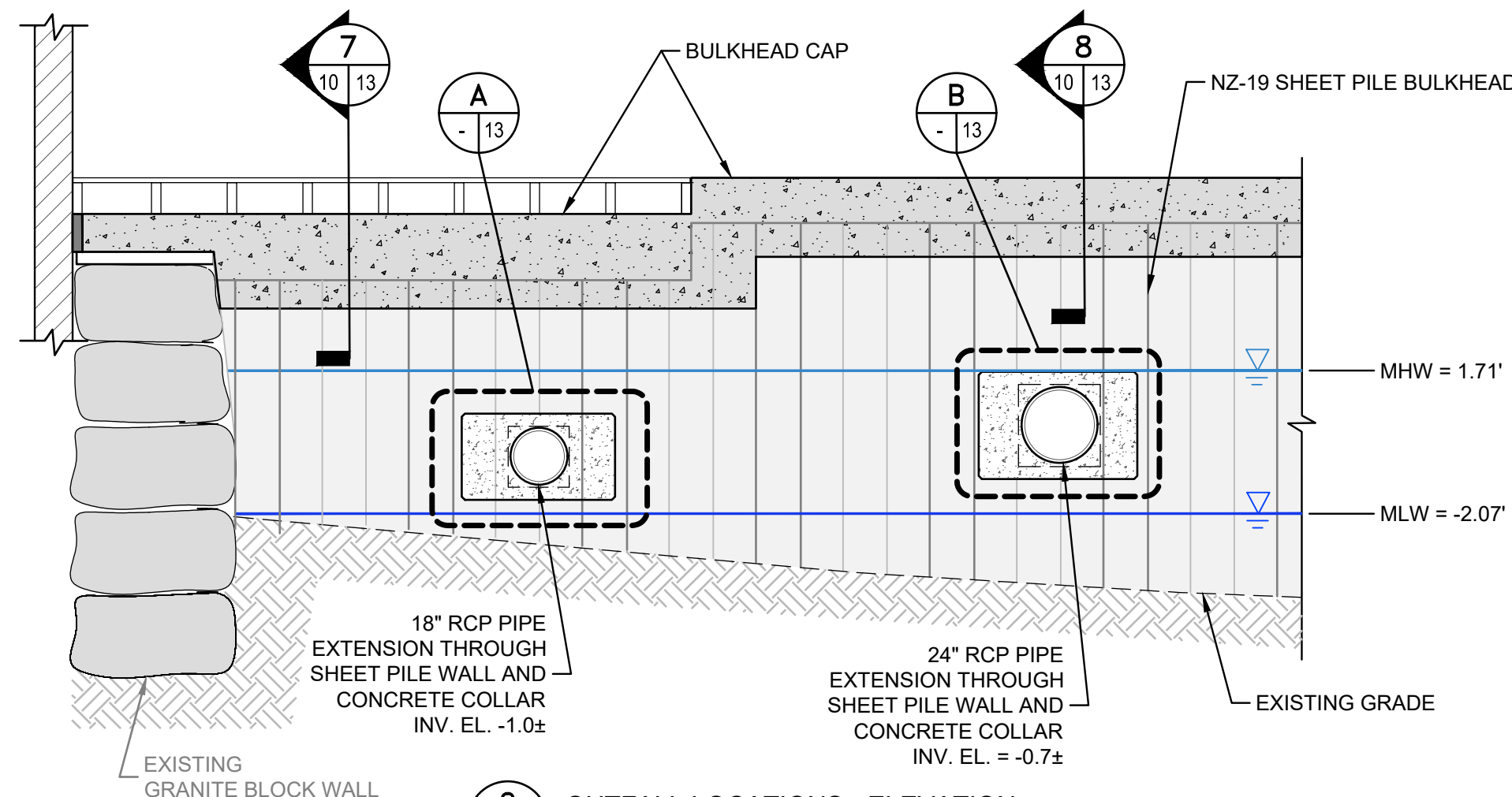
1 OUTFALL LOCATIONS - PARTIAL PLAN
SCALE: 1"=4'



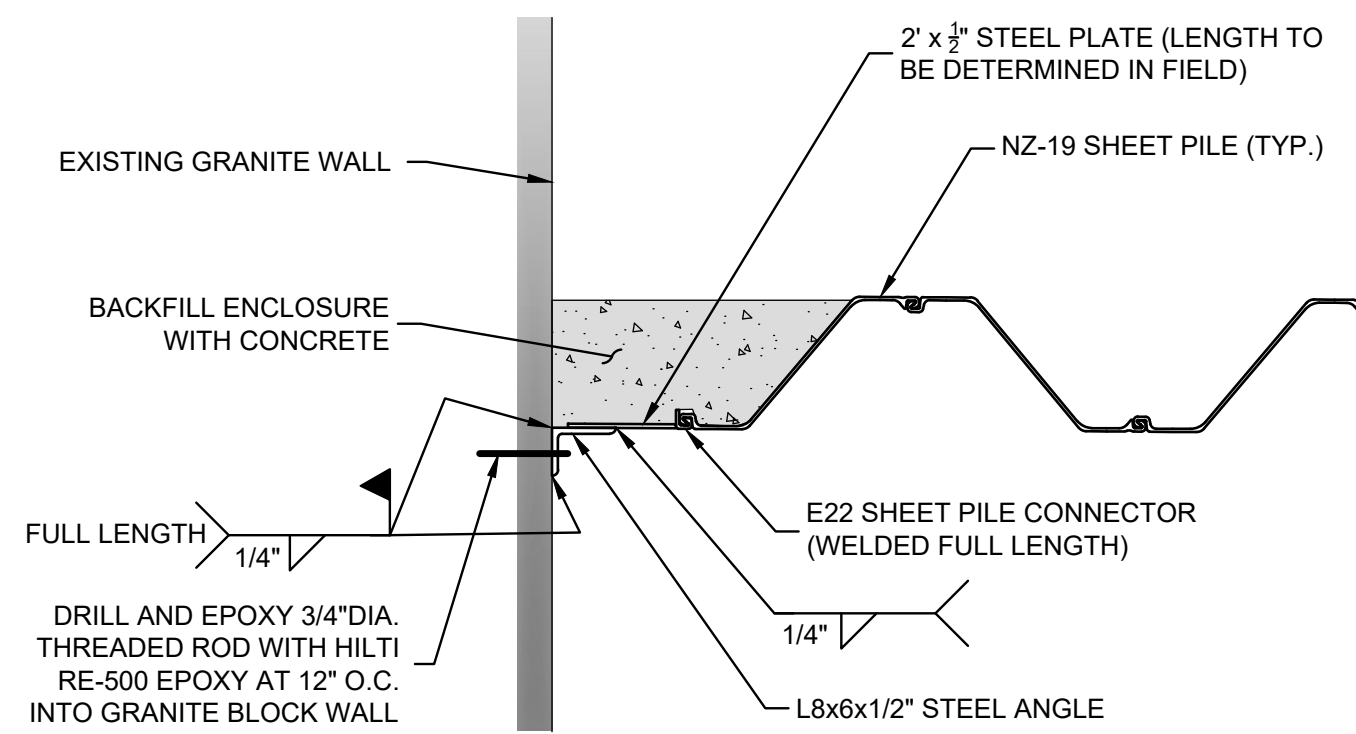
A RCP OUTFALL 18" PIPE EXTENSION - ELEVATION
SCALE: 1"=1'



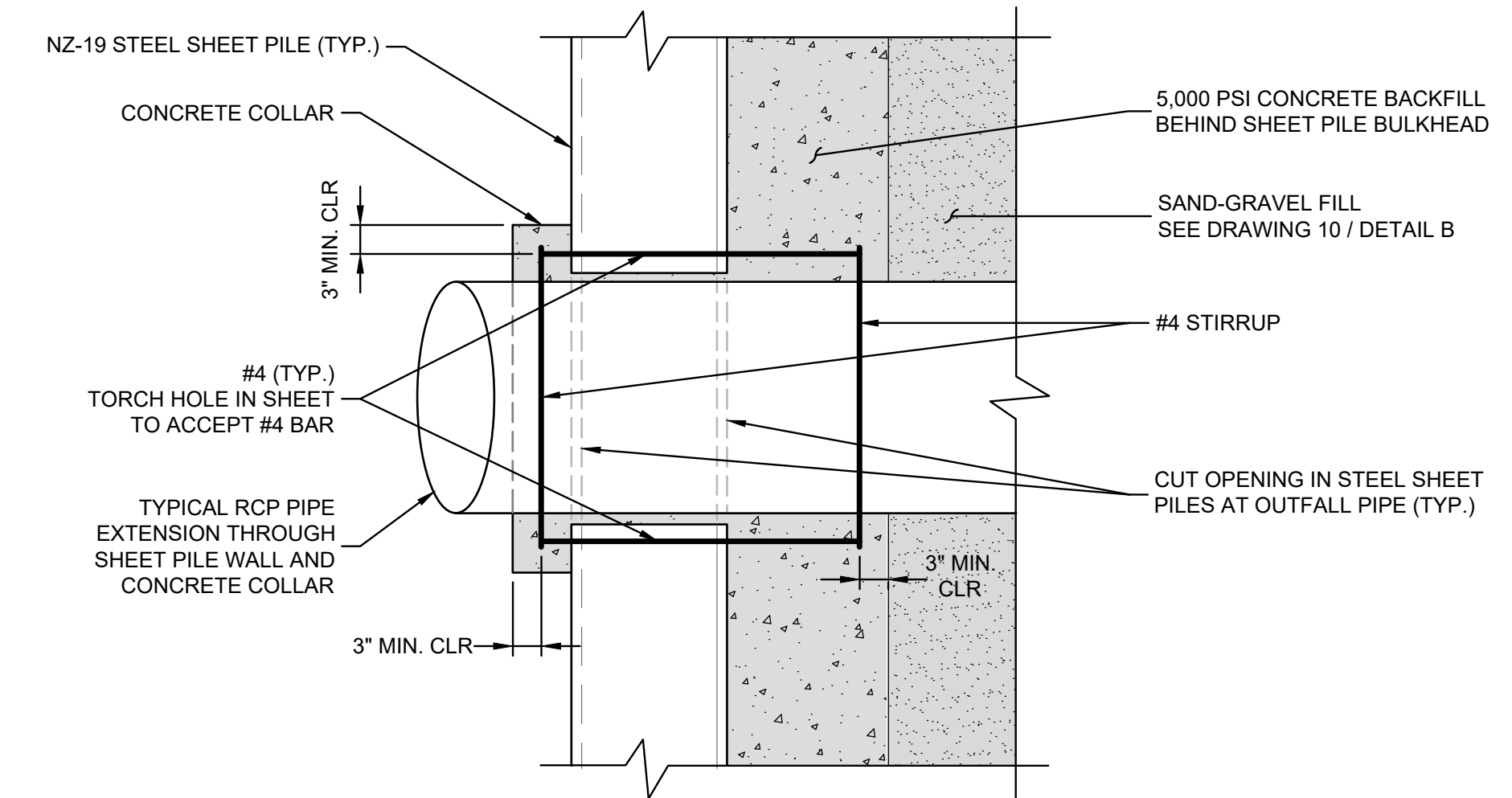
B RCP OUTFALL 24" PIPE EXTENSION - ELEVATION
SCALE: 1"=1'



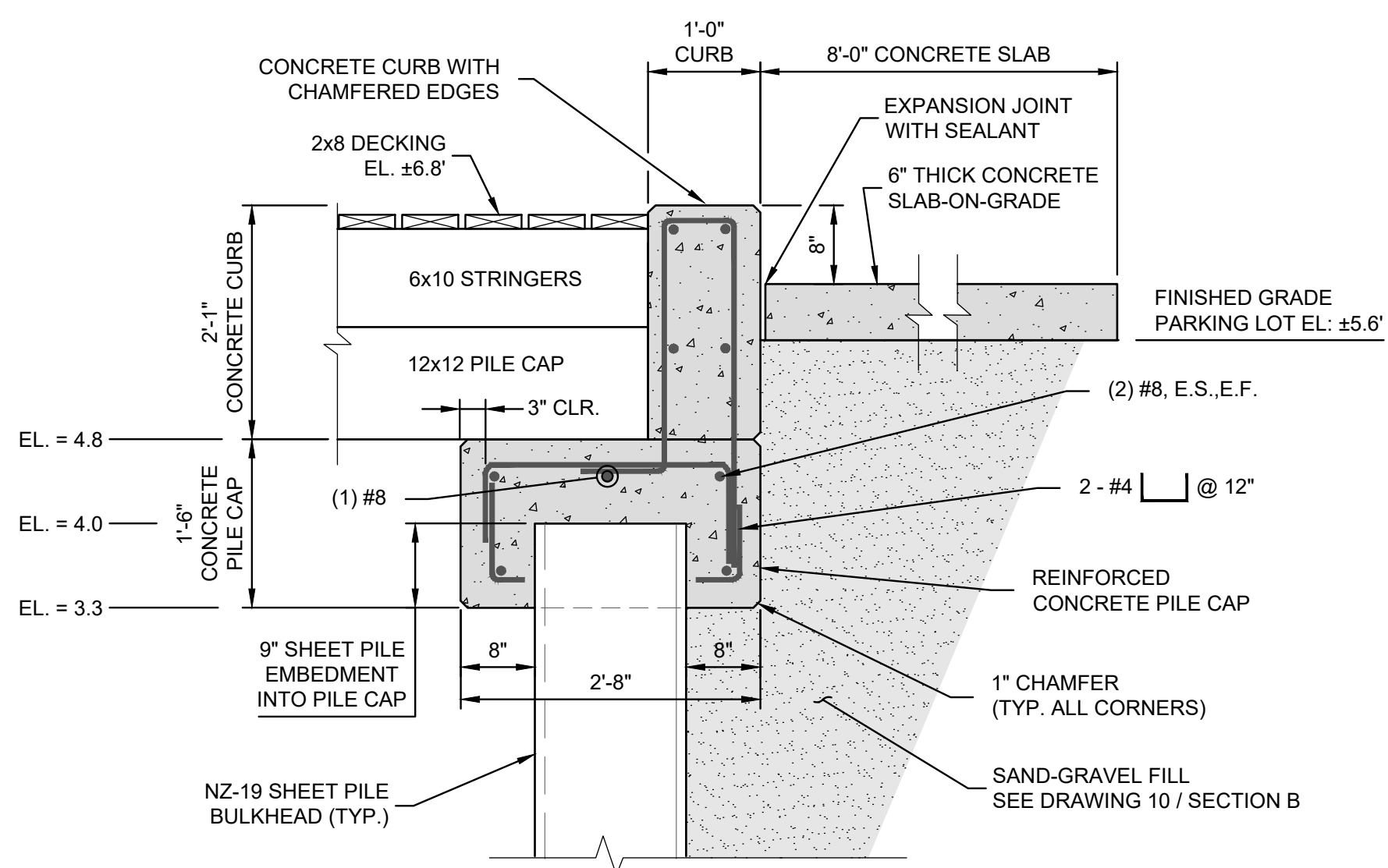
2 OUTFALL LOCATIONS - ELEVATION
SCALE: 1"=4'



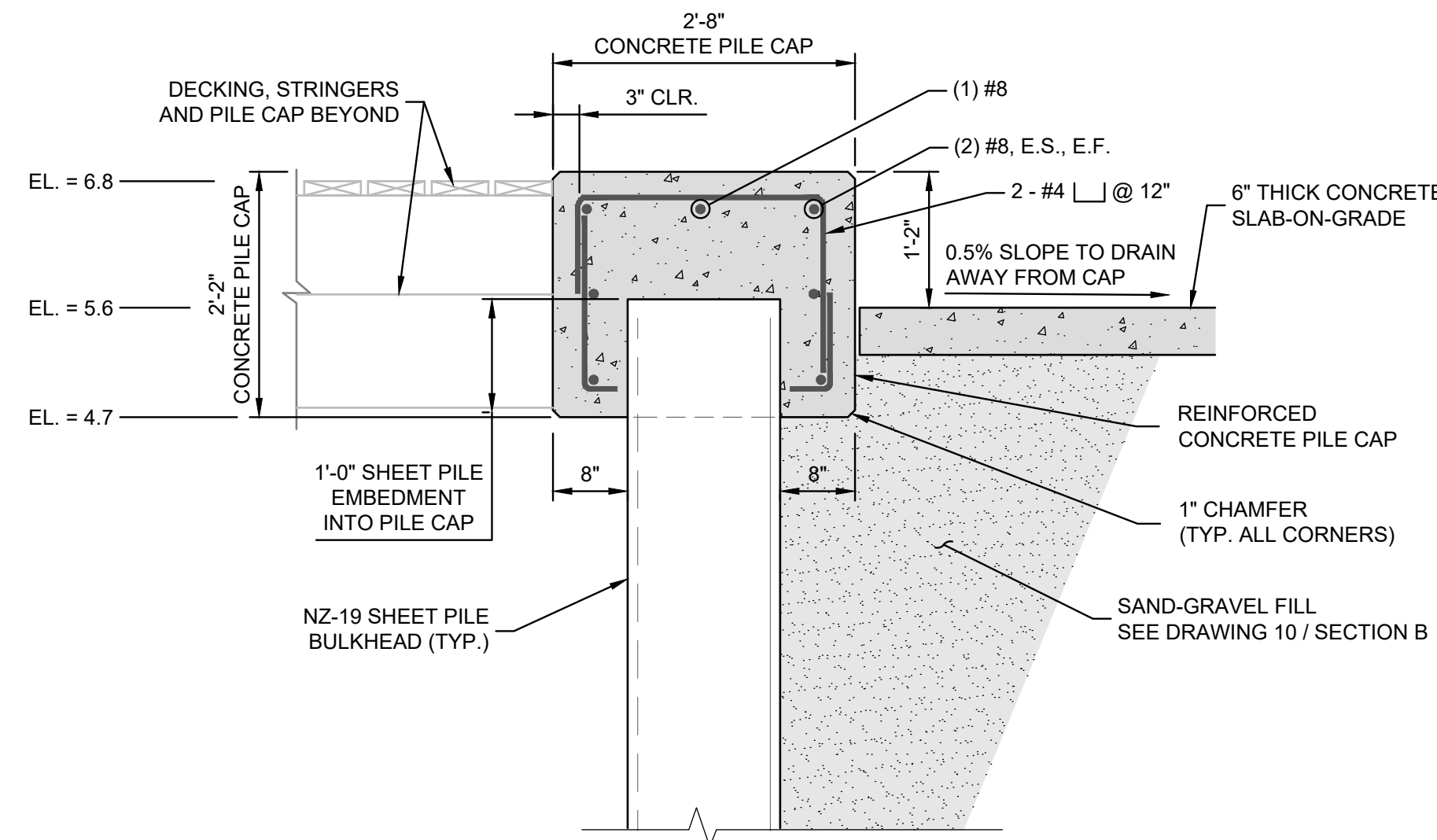
5 BULKHEAD CLOSURE AT GRANITE BLOCK WALL
SCALE: 1"=1'



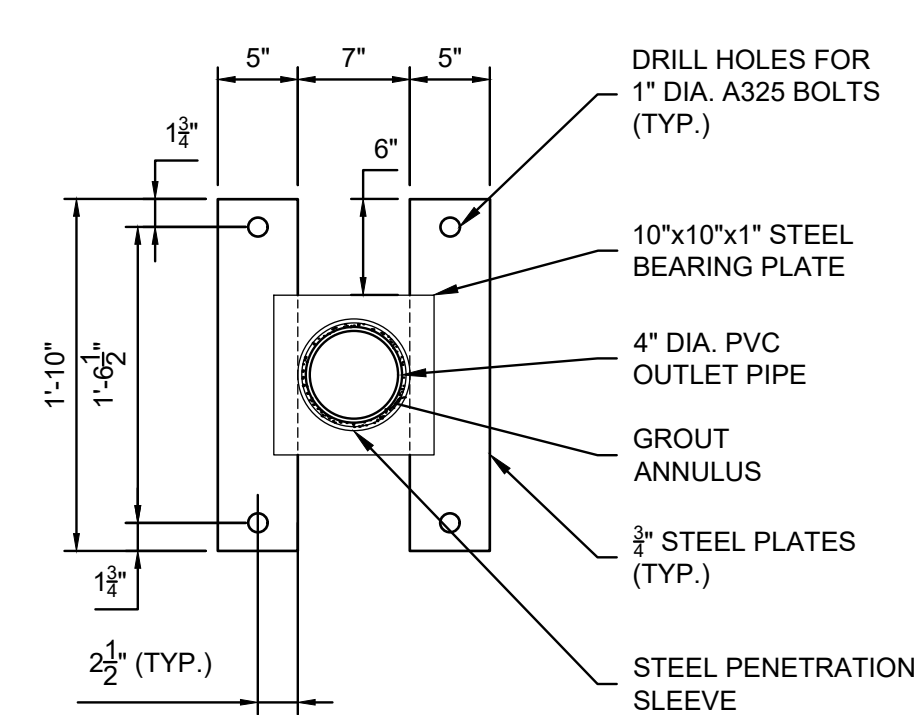
6 TYPICAL RCP EXTENSION THROUGH BULKHEAD - DETAIL
SCALE: .75"=1'



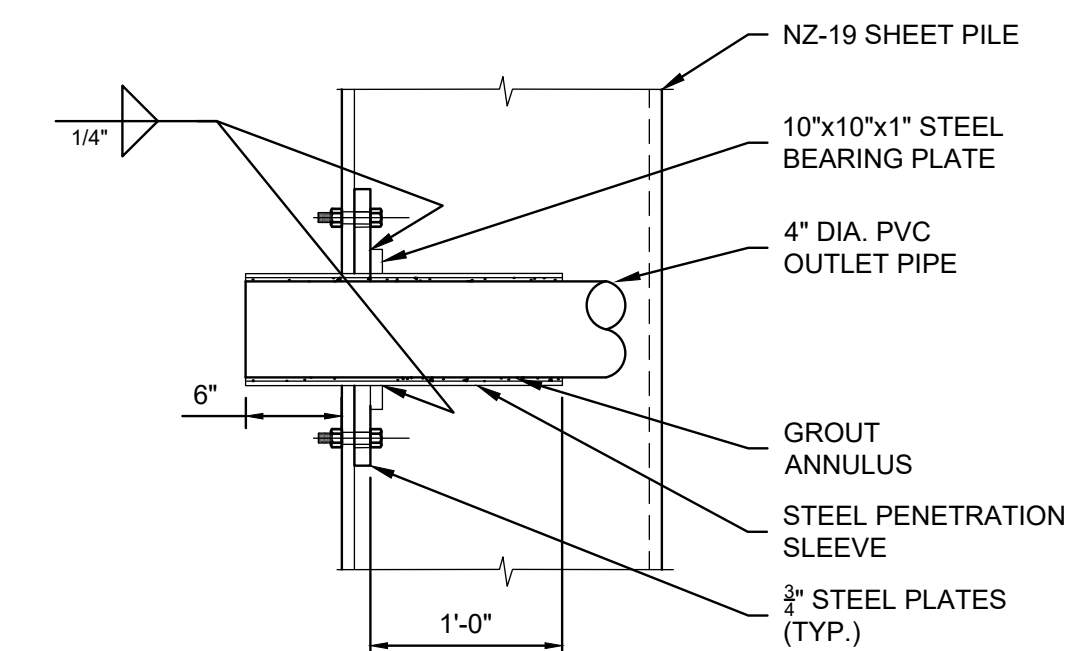
7 SECTION - CONCRETE BULKHEAD CAP WITH CURB
SCALE: .75"=1'



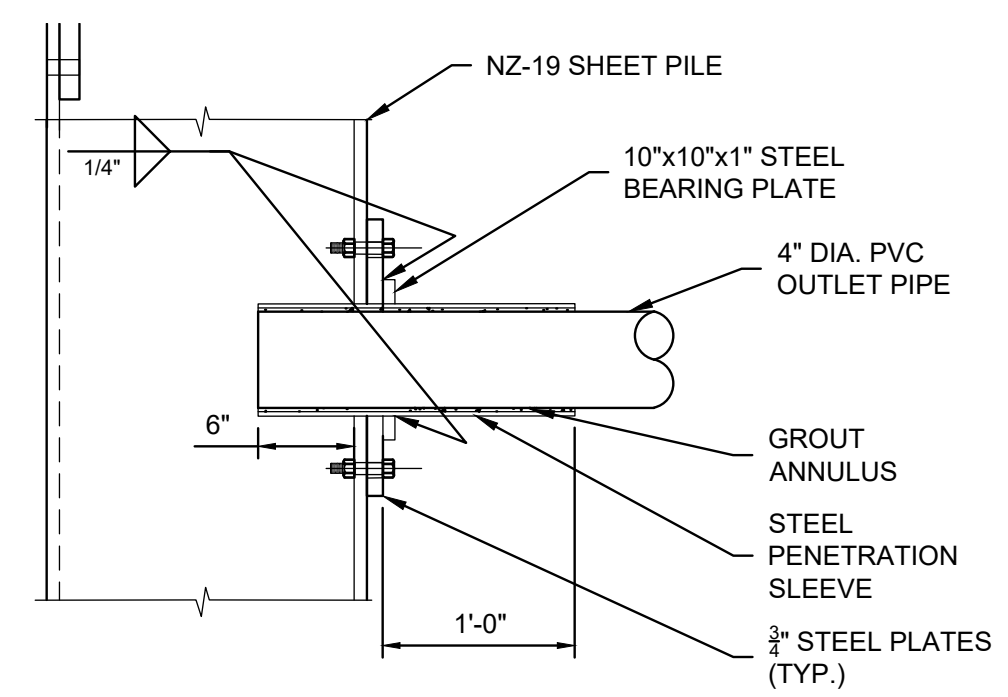
8 SECTION - CONCRETE BULKHEAD CAP
SCALE: .75"=1'



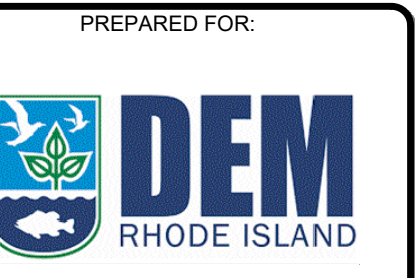
9 DETAIL - PVC OUTFALL PENETRATION
SCALE: 1"=1'



10 DETAIL - PVC OUTFALL PENETRATION
SCALE: 1"=1'



11 DETAIL - PVC OUTFALL PENETRATION
SCALE: 1"=1'



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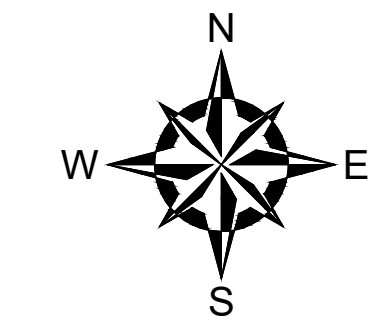
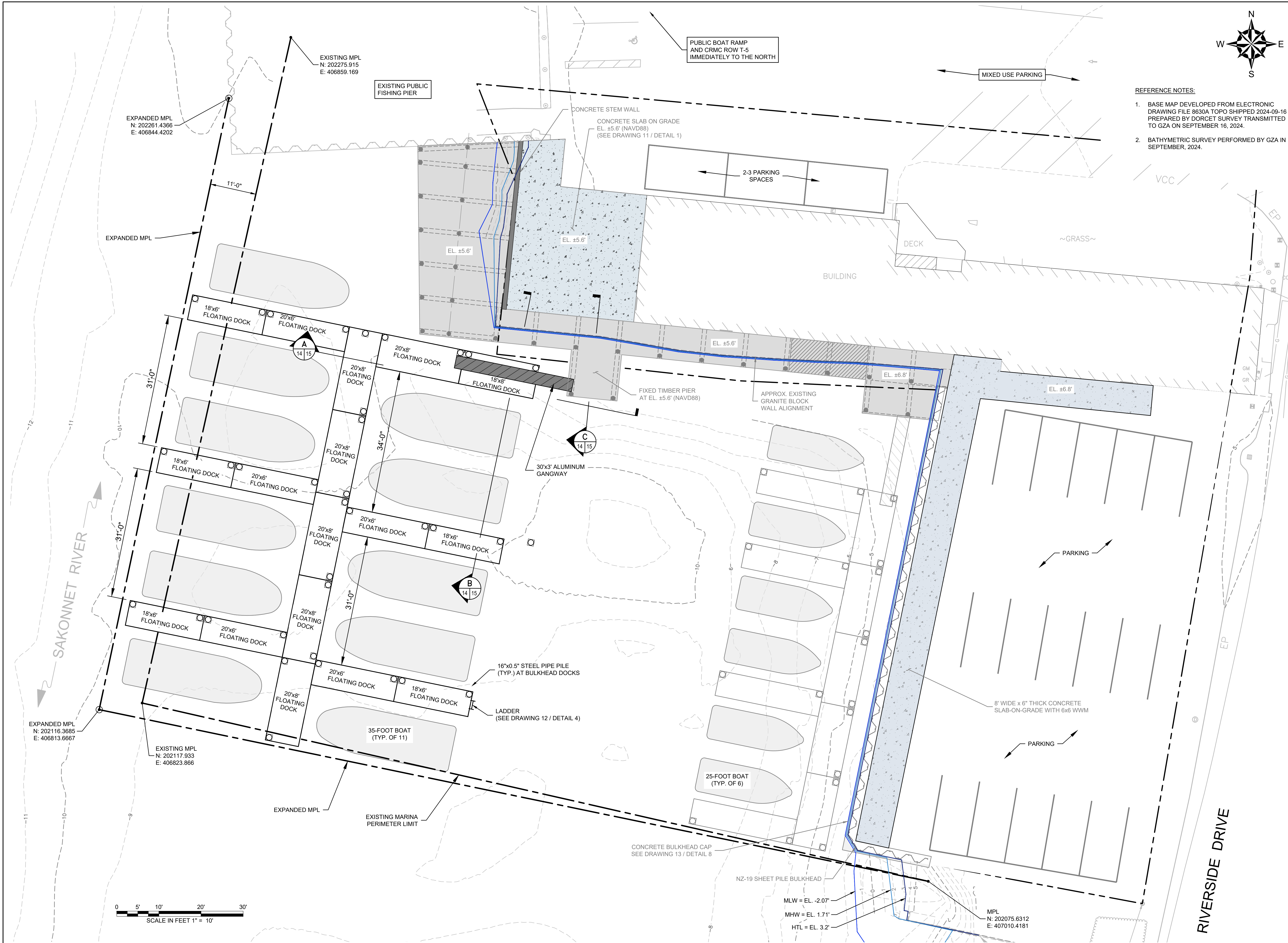
REV.	DESCRIPTION	DATE	BY

PROJECT NO:	35429.00
DATE:	DECEMBER, 2025
PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

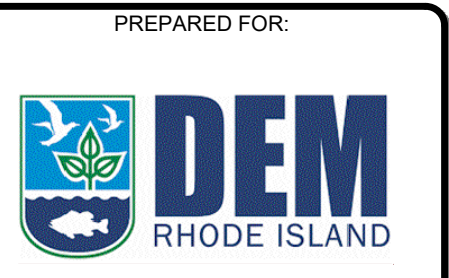
CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

**PROPOSED BULKHEAD - OUTFALL
DETAILS / PHASE 1**

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- REFERENCE NOTES:**
1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DORCET SURVEY TRANSMITTED TO GZA ON SEPTEMBER 16, 2024.
 2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024.



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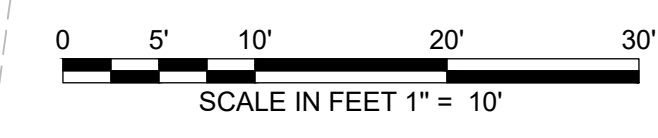
REV	DESCRIPTION	DATE	BY

PROJECT NO: 35429.00
 DATE: DECEMBER, 2025
 PROJECT MGR: TWS
 DESIGNED BY: JTF
 DRAWN BY: GRB
 CHECKED BY: TWS
 REVIEWED BY: MJP
 SCALE: AS NOTED
 REVISION NO.

CHASE MARINA
 169 RIVERSIDE DRIVE
 TIVERTON, RHODE ISLAND

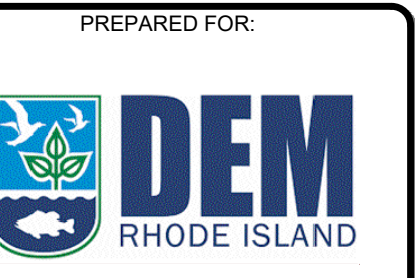
**PROPOSED CONDITIONS PLAN - PHASE 2
 AND FINAL CONDITIONS PLAN**

DRAWING
14
 SHEET NO. 14 OF 15

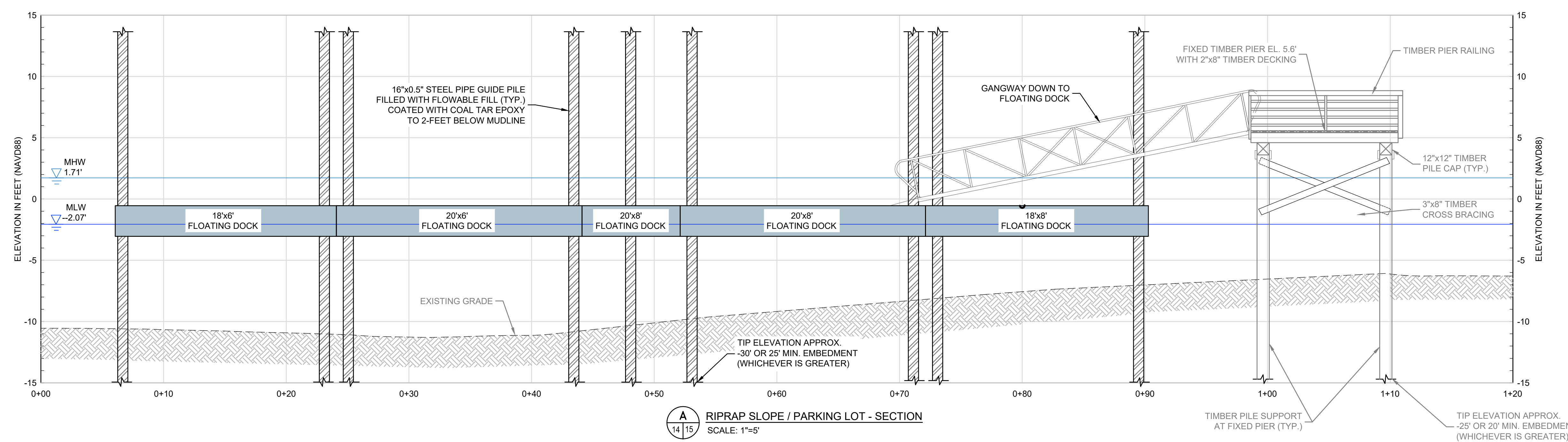




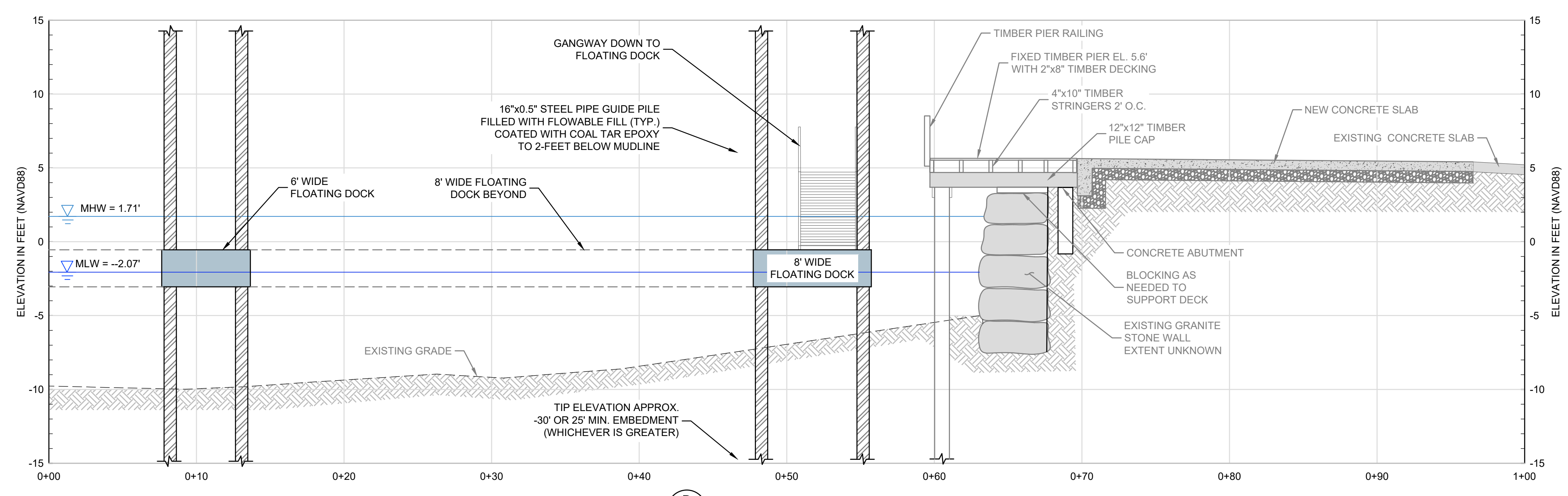
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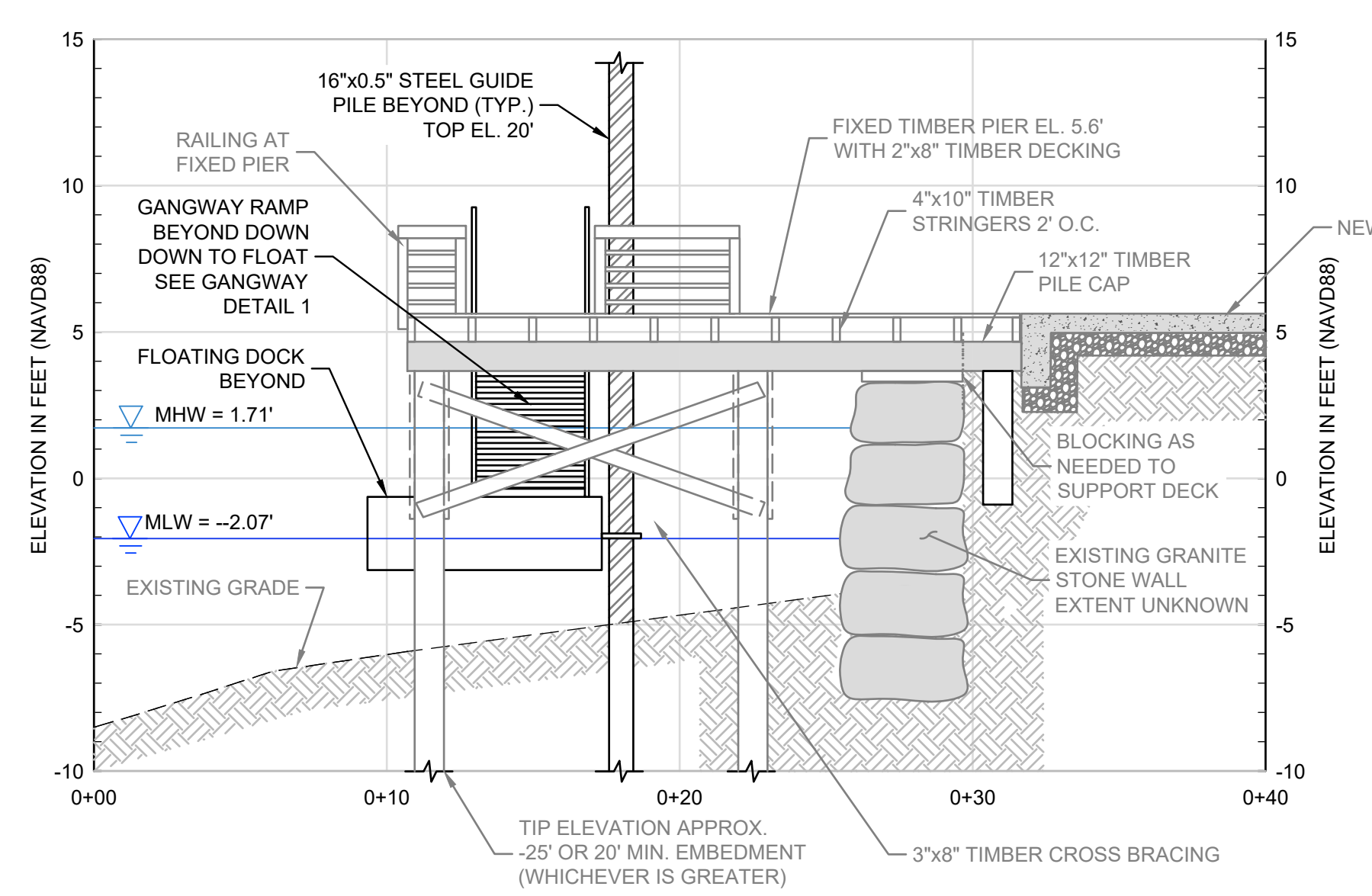
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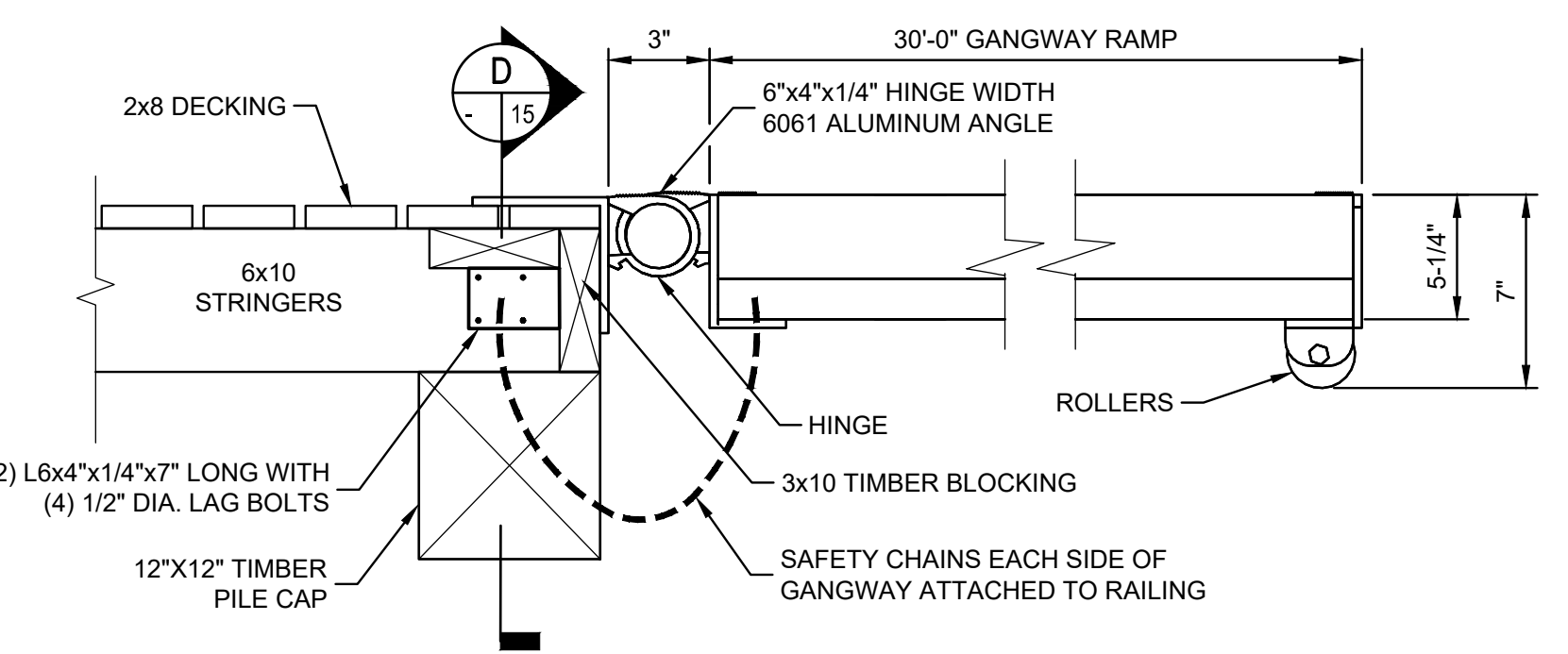
A RIPRAP SLOPE / PARKING LOT - SECTION
SCALE: 1"=5'



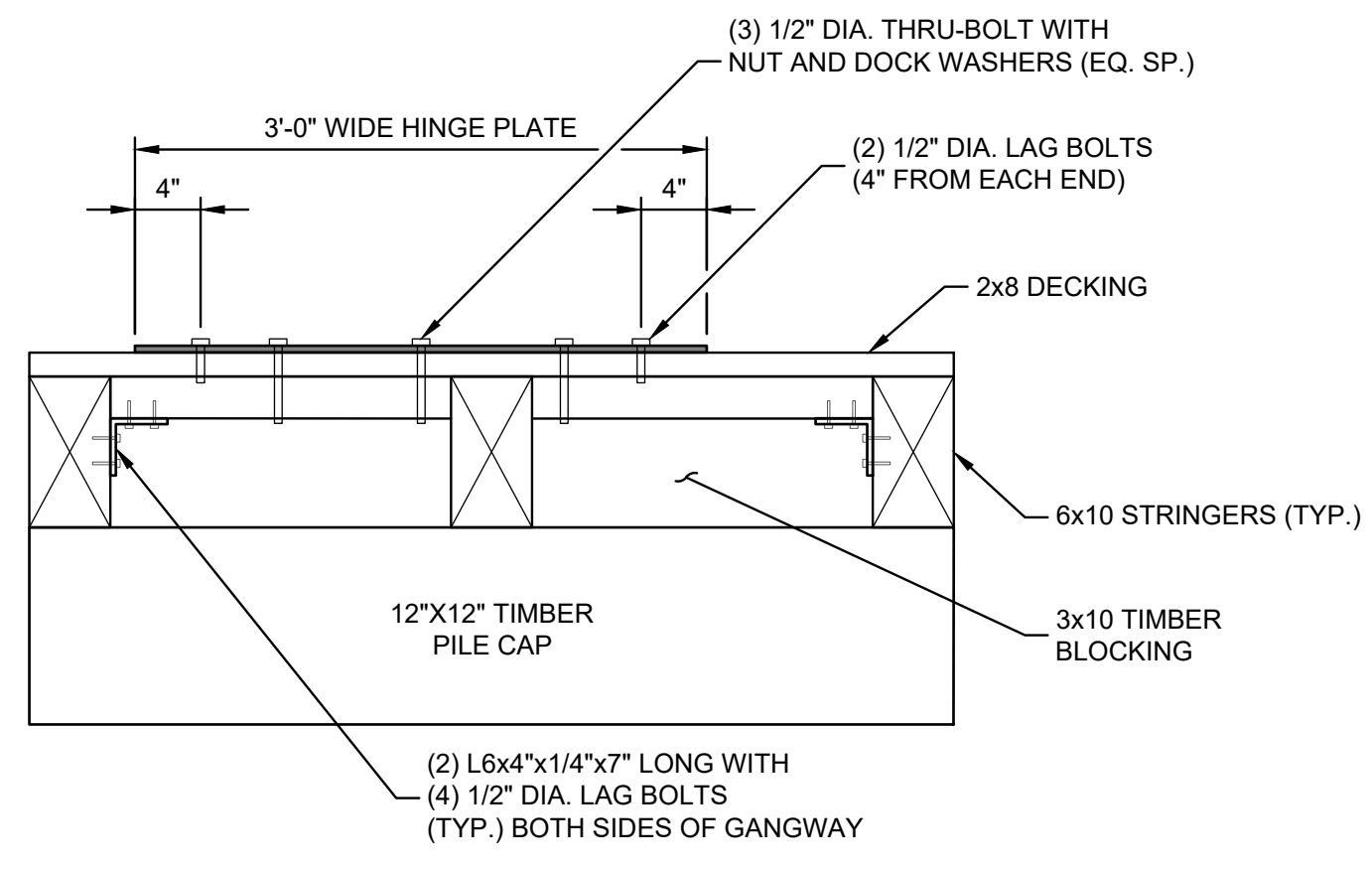
B DOCK PLATFORM - SECTION
SCALE: 1"=5'



C GRANITE BLOCK WALL - SECTION
SCALE: 1"=5'



1 TYPICAL GANGWAY - DETAIL
NOT TO SCALE



D TYPICAL GANGWAY CONNECTION - SECTION
NOT TO SCALE

- NOTE:
1. TOP OF PILE CUTOFF ELEVATION = 20- FEET
 2. ALL STEEL PIPE GUIDE PILES TO BE CAPPED WITH A PVC PILE CAP.



100% DESIGN DRAWINGS
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REV.	DESCRIPTION	DATE	BY

PROJECT NO: 35429.00
DATE: DECEMBER, 2025
PROJECT MGR: TWS
DESIGNED BY: JTF
DRAWN BY: GRB
CHECKED BY: TWS
REVIEWED BY: MJP
SCALE: AS NOTED
REVISION NO.

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

PROPOSED CONDITIONS SECTIONS
PHASE 2

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CRMC DECISION WORKSHEET

2025-06-026

Rhode Island Department of Environmental Management

Hearing Date:	
Approved as Recommended	
Approved w/additional Stipulations	
Approved but Modified	
Denied	Vote

APPLICATION INFORMATION						
File Number	Town	Project Location		Category	Special Exception	Variance
2025-06-026	Tiverton	169 Riverside Dr.		B	<input type="checkbox"/>	<input type="checkbox"/>
		Plat	303			
		Owner Name and Address				
Date Accepted	6/13/2025	Department of Environmental Management 235 Promenade St. Providence, RI 02908		Work at or Below MHW	<input checked="" type="checkbox"/>	
Date Completed	10/22/2025			Lease Required	<input type="checkbox"/>	

PROJECT DESCRIPTION

The applicant requests assent to repair existing infrastructure at the former Chase Marina and establish a commercial marina facility. As part of this application, the existing Marina Perimeter Limit (MPL) has a slight expansion to the south and west in an effort to maintain safe fairway distances and navigation within the facility. The proposed slip count increases by one vessel from sixteen (16) total vessels to seventeen (17) total vessels. New docks and piles are proposed to be installed within the revised MPL. Additionally the project involves filling in tidal waters at an existing derelict marine railway basin.

KEY PROGRAMMATIC ISSUES

Coastal Feature: Manmade Shoreline / Bulkhead

Water Type: Type 3, High Intensity Boating

Red Book: 1.2.1(D), 1.2.2(F), 1.3.1(A), 1.3.1(B), 1.3.1(J), 1.3.1(C), 1.3.1(D), 1.3.6

SAMP: N/A

Variations and/or Special Exception Details:

Not applicable

Additional Comments and/or Council Requirements:

Please see Staff Report for comments and Staff recommendations

Specific Staff Stipulations (beyond Standard stipulations):

If approved, additional stipulations shall be required as outlined in the Staff Report.

STAFF RECOMMENDATION(S)

Engineer EMS Recommendation: Approval

Biologist _____ Recommendation: _____

Engineering Supervisor Sign-Off _____ date

[Signature] 10/23/25
Executive Director Sign-Off _____ date

Supervising Biologist Sign-off _____ date

[Signature] 10/22/25
Staff Sign off on Hearing Packet (Eng/Bio) _____ date



STATE OF RHODE ISLAND
COASTAL RESOURCES MANAGEMENT COUNCIL
STAFF REPORT TO THE COUNCIL


DATE: October 22, 2025
TO: Jeffrey M. Willis, Executive Director
FROM: Mason Sherman, Marine Infrastructure & Dredging Coordinator

Applicant's Name:	Department of Environmental Mangement
CRMC File Number:	2025-06-026
Project:	Repair existing infrastructure, revise existing MPL, establish a commercial marina facility.
Location:	169 Riverside Drive; Tiverton: Plat(s): 303; Lot(s): 126, 127, 128
Water Type/Name:	3 – High Intensity Boating / Sakonnet River
Coastal Feature:	Manmade Shoreline/Bulkhead
Plans Reviewed:	“Chase Marina”, Sheets 1 to 15, by GZA Geoenvironmental Inc., stamped by Matthew J. Page, P.E. (No. 9448), and dated August 2025.

INTRODUCTION:

The applicant requests assent to repair existing infrastructure at the former Chase Marina and establish a commercial marina facility. As part of this application, the existing Marina Perimeter Limit (MPL) has a slight expansion to the south and west in an effort to maintain safe fairway distances and navigation within the facility. The proposed slip count increases by one vessel from sixteen (16) total vessels to seventeen (17) total vessels. New docks and piles are proposed to be installed within the revised MPL.

Another scope of the project involves filling in tidal waters at an existing derelict marine railway basin to add additional parking spaces for marina patrons and reinforce the shoreline. Additional work to be included: repairs/filling of rip rap slopes, installation of a sheet pile bulkhead, and construction of timber walkways and docks, and reinforcement of existing granite block walls.

Signed: _____ Staff Biologist
Signed:  _____ Staff Engineer

CRMC HISTORY:

The applicant claims the original Chase Marina Facility was constructed prior to 1939. Aerial photos dating back to 1939 are blurred, but it is clear that the building structure is in place and the old marine railway may also be in use. The 1997 aerials clearly show the facility being utilized as a marina with docks laid out in a similar manner to what the applicant is proposing. In addition, based on historic CRMC assents, this facility was utilized as a marina dating back to at least 1988. This location was purchased by the State and utilized as a laydown yard for construction of the new Sakonnet River Bridge.

File Number	Description	Owner	Category - Status
2025-06-026	Establish Commercial Facility	RIDEM	Current Application
2025-01-001	Borings and Pile Removal	RIDEM	M – Approved
2021-02-096	Install Cap on Property	RIDOT	M – Approved
2005-02-086	Reconfigure Marina	Francis Chase	A – Approved
1999-04-031	Repair Existing Loading Dock	Francis Chase	F – Approved
1988-03-049	Renovations and New SDS	Francis Chase	A – Approved
1988-03-029	Maintenance, Dredging, Filling	Francis Chase	B – Approved
1985-03-071	Replace Pile In Kind	T. Brian Jandrigan	A – Approved
1985-03-070	Addition to Building	William Higginbottom	A – Approved
1977-08-019	One-Story Addition to Building	Tiverton Machine/Welding	A – Approved

COMMENTS ON APPLICATION/APPLICABLE POLICIES, STANDARDS & ETC:

SECTION	TITLE	SUMMARY
1.2.1(D)	Type 3 Waters - High Intensity Boating – Sakonnet River	The priority use for this water type is to maintain and expand recreational boating and enhance water dependent businesses. With new areas for marinas being limited, the Council supports recycling of already altered sites. While the applicant is proposing a commercial marina/facility, they are utilizing an already altered site to enhance water dependent businesses. The proposed commercial marina does not directly add recreational slips but does enhance water dependent businesses in the area by providing slips for the State’s commercial fishing fleet. They will also allow slips for charter fishing vessels if commercial slips become vacant. In addition the applicant is providing a pump out location for a Tiverton operated pump out vessel which would service recreational moorings and transient vessels.
1.2.2(F)	Manmade Shorelines	As previously mentioned, historic aerials indicate this location has been developed with shoreline protection structures and other alterations dating back to 1939 or earlier. The applicant’s proposal supports the Council’s goals of maintaining existing structures and preventing the accumulation

Signed: _____ Staff Biologist

Signed: *EMS* _____ Staff Engineer

		of debris along shore by cleaning up the currently inactive and deteriorating facility.
1.3.1(A)	Category B Requirements	
(1)(a)	Demonstrate Need	This facility has historically been utilized as a marina but is not fit for this use, or any other use for that matter, in it's current condition. This is evident by GZA's 2021 Property Condition Assessment as well as site inspections by CRMC. Restoring this facility would provide berthing and job opportunities for the State's fishing industry.
(1)(b)	Zoning and Building Codes	Any necessary codes and local permits shall be followed and/or applied for. CRMC Building Official Sign-Off Form was provided and indicates the applicant has been working to obtain the required RISBC permits. USACE and RIDEM permits have been applied for concurrently.
(1)(c)	Coastal Water Boundaries	The proposed work is adjacent to the Sakonnet River. Length of work along the shoreline is approximately 700 linear feet which wraps around the existing structures. With the exception of filling the derelict marine railway basin and improvements to shoreline protection, impacts on the landside portion of the facility are minimal and remain close to existing.
(1)(d)	Erosion	The proposed repairs will improve and stabilize the existing shoreline protection thus minimizing future risk of erosion or deposition into the adjacent waterway post construction. During construction there is anticipated to be some level of deposition on land as a result of the filling and grading as well as turbidity within the water column. The applicant has provided plans which include sufficient controls (turbidity curtains and silt/sediment filter socks) around the work area extents to minimize impacts throughout construction.
(1)(e)	Plant and Animal Diversity	With this facility most recently being utilized as a laydown area for the Sakonnet River Bridge project and prior to that being a marina with close to the same MPL as currently proposed, no significant negative impacts to plant or animal life are anticipated. Based on the RIGIS SAV Mapping tool, no SAV is present in the area.
(1)(f)	Public Access	The facility's current state prohibits any kind of public access to this location. The proposed repairs and improvements will correct this and allow use of this section of the Sakonnet River. There is an existing CRMC ROW and public boat launch to the

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		north. These public access areas will be unimpeded by the applicant's proposal. In addition, more parking will be created at the facility for marina patrons. This will free up space at the boat ramp and boat ramp parking with marina tenants storing their vessels in marina slips rather than launching at the boat ramp.
(1)(g)	Water Circulation, Flushing, Turbidity, & Sedimentation	With the exception of a small amount of turbidity expected during shoreline protection improvements and driving new piles, there is no expected long term impacts to Water Circulation, Flushing, Turbidity, or Sedimentation. As previously mentioned, turbidity curtains will be installed throughout construction to minimize impacts.
(1)(h)	Water Quality	As mentioned above, no anticipated long term impacts to water quality. An RIDEM Water Quality Cert is being applied for concurrently and will further address additional water quality concerns.
(1)(i)	Historic and Archaeological Significance	HPHC letter of no effect received on 7/11/2025.
(1)(j)	Water Dependent Uses	The proposed commercial marina does not directly add recreational slips but does enhance water dependent businesses in the area by providing slips for the State's commercial fishing fleet. They will also allow slips for charter fishing vessels if commercial slips become vacant. In addition the applicant is providing a pump out location for a Tiverton operated pump out vessel which would service recreational moorings and transient vessels. Staff believes the applicant is demonstrating this facility will improve recreational boating while also expanding opportunities for commercial fishermen.
(1)(k)	Scenic Impact	The proposed project should enhance the waterfront value of this location by transforming a currently derelict facility into a useable and fully functioning facility.
1.3.1(B)	Filling, Removing, or Grading of Shoreline Features	Proposed fill is an estimated total of 2025 square feet which is less than the 5000 sqft threshold requiring an approved erosion and sediment control plan. This being said, the applicant has included plans for erosion and sediment control in their drawings and narrative. The fill area is comprised of a derelict, unused, marine railway basin and filling of existing rip rap slope behind the proposed bulkhead. Proposed fill within tidal waters is a total of 260 CY. The quantity of fill is less than

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		<p>10,000 CY, the area is less than two acres, and the affected area is not a historic area of concern per HPHC's letter of no effect. This being said, the fill activities do not need to be reviewed at the CAT B level.</p> <p>The fill area within the basin shall be utilized to square off the parking area and provide additional parking spaces for the facility to help meet CRMC's marina parking requirements.</p>
1.3.1(J)	Filling In Tidal Waters	<p>It is Council policy that filling may be permitted where necessary for an approved bulkheading project. The applicant is proposing a bulkhead to be installed at the base of existing riprap revetment. With the bulkhead being at the base of this revetment the proposed filling is considered by Staff to be the minimum necessary. The bulkhead will be utilized to contain and support filling of the old railway basin and the proposed parking lot for the marina.</p>
1.3.1(C)	Residential, Commercial, Industrial, and Recreational Structures	
(1)	Policies	<p>A Public Access Plan drawing was provided by the applicant which highlights the adjacent public boat ramp, CRMC ROW T-5, Public Fishing Pier, and designated parking for the marina.</p> <p>There is an existing approved MPL for this facility per CRMC Assent 2005-02-086. Although this facility will be utilized as a commercial marina, the applicant is proposing to keep this MPL, rather than an SPL with a modest expansion to the south and west in an effort to maintain safe fairways within the marina floating dock arrangement.</p>
(2)	Prerequisites	<p>CRMC building official sign-off was provided indicating a building permit will be required and the applicant is working with the building official to obtain said permits.</p>
(3)	Prohibitions	<p>The proposal is not prohibited by any parts of this section.</p>
(4)	Standards	<p>The design of this facility was performed and stamped by a registered professional engineer. An MPL is already established and the applicant wishes to keep this general MPL with modest expansion to the south and west. A condition assessment was conducted by GZA in November of 2021 to assess existing conditions and develop their proposed plans for repairing and maintaining the facility.</p>

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(6)	Flood Zone Construction	As required, the CRMC State Building Code Sign-Off sheet was completed and submitted with the application.
(7)	Construction in Flood Hazard Zones	As specified, ASTM standards will be followed as applicable.
1.3.1(D)	Recreational Boating Facilities	
(2)	Marina Policies	
(a)	Use of Space	There have been buildings and shoreline protections in place at this location dating back to 1939 based on historic aerial photos and the primary use of a marina dating back to at least 1988 based on historic aerials and permits, with marina use up until around 2008. The applicant intends to bring this marina back to a similar configuration and fill an old unused railway basin to increase parking in the area.
(b)(1,2,&3)	Appropriateness of Facility, Structure, and Potential Impacts	This is not technically a new marina due to the existing permitted MPL, and not a significant expansion by definition, so a PD was not required by the CRMC regs, however, the application was reviewed preliminarily with comments and additional information requirements sent to the applicant. A pre application meeting/site visit was performed in early 2025. This facility is appropriate for the area given its prior use in a similar capacity to what the applicant is proposing as well as other marina facilities being located in this general area of the Sakonnet River. Additionally, negative impacts of the proposed facility to public trust resources are anticipated to be minimal based on historic use of this location.
(b)(4)	Navigation Impacts	There will be a small expansion of the MPL (approx. 11 feet) to the west but the overall configuration of the facility remains close to existing and what the facility has been historically used for. Adequate distance from the nearest mooring field and channel is maintained. No negative navigation impacts are anticipated.
(b)(5)	Scenic Impacts	The current facility has remained fairly derelict since completion of the Sakonnet River bridge. The applicant's proposal should improve scenic value by maintaining and existing facility and business.
(b)(6&7)	Vessel Density Impacts	While it has been close to 20 years, this facility did at one time operate as a marina. The applicant's goal is to restore the facility to serve a similar purpose. The applicant has demonstrated a need for this type of facility, serving mainly commercial

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		vessels, in this location. They have also arranged the layout to accommodate the average vessel size currently utilizing the Sakonnet River. As such, no negative vessel density impacts are anticipated.
(b)(8&9)	Public Impacts	As previously mentioned, a public access plan drawing was provided which highlights an existing CRMC ROW, public boat ramp, and fishing pier to the north which will remain unimpeded due to this project. As noted by the applicant, some of their expected tenants currently utilize the public boat ramp and associated parking. With those tenants now utilizing the marina, it will free up space for the general public to utilize these resources and ultimately improve public access.
(b)(10)	Extension over Submerged Land	The floating docks are required to extend over submerged land to accomplish their purpose. The proposed MPL and dock expansion to what was historically approved is modest and helps to maintain adequate fairways and safe navigation.
(c)	Marina Expansion within MPL	The proposed expansion is very modest, 11' to the west and less than half of that to the south. With a slip expansion of just 1 vessel (16 slips to 17 slips) and the trend of vessels getting larger over the years, Staff believes the proposed expansion is the minimum necessary.
(d)	Water Quality	RIDEM water quality cert and USACE permits have been applied for concurrently with this CRMC application.
(e)	Public Trust	Staff feels that there is no negative impact to the public trust associated with applicant's proposal. It is Council policy to recycle already altered sites to enhance water dependent business and the applicant's proposal accomplishes this.
(f)	New or Significant Expansion	The proposed expansion is very modest, 11' to the west and less than half of that to the south. With a slip expansion of just 1 vessel (16 slips to 17 slips) and the trend of vessels getting larger over the years, Staff believes the proposed expansion is the minimum necessary.
(g)	Vessel Size Variety	Applicant's proposal accommodates vessels ranging in size from 25' to 35' which Staff feels is adequate for the intended purpose of the marina being to tend to more commercial type vessels.
(h)	Public Access	A public access plan drawing was provided.
(4)(a)	Prerequisites	Where the southern limit of the MPL makes landfall it is outside of the property line extension, however, the MPL falls in line with what was

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		historically already approved in CRMC Assent A2005-02-086.
(5)	Marina Prerequisites	
(a)	Preliminary Determination (PD)	This is not technically a new marina due to the existing permitted MPL, and not a significant expansion by definition, so a PD was not required by the CRMC regs. However, a preliminary review of the file and information request was submitted to the applicant.
(b)	Public Trust	Staff feels that there is no negative impact to the public trust associated with applicant's proposal. It is Council policy to recycle already altered sites to enhance water dependent business and the applicant's proposal accomplishes this.
(c)(1)	Storage Alternatives	The proposed wet vessel storage arrangement falls closely in line with what has been utilized in the past. There is no available space upland for dry stack storage.
(c)(2)	Additional Permits	Army Corps of Engineers and RIDEM permits have been applied for concurrently with this application.
(c)(3)	Commercial Mooring Area	No Commercial Mooring Area proposed with this application.
(c)(4)	Plan Drawing Requirements	A plan drawing outlining the proposed MPL is included. NAD83 is used and MLW and MHW are identified.
(7)	Prohibitions	
(a)	Type 1 and 2 Waters	The proposed marina is located in Type 3 waters thus, not prohibited by water type.
(b)	Type 1 Waters	The proposed marina is located in Type 3 waters thus, not prohibited by water type.
(c)	Unloading Catches	This is not a residential or limited recreational boating facility, thus unloading catches is not prohibited.
(d)	Additional Structures	Residential or limited recreational boating facilities are not applicable to this application.
(e)	Discharge	Discharge shall not be allowed at this facility.
(f)	Private Launching Ramps	Not applicable to this application.
(g)	T and L-Dock Sections	Residential or limited recreational boating facilities are not applicable to this application.
(h)	Terminal Floats	Residential or limited recreational boating facilities are not applicable to this application.
(i)	Terminal Float Area	Residential or limited recreational boating facilities are not applicable to this application.
(j)	Marine Railway	The existing marine railway basin shall be filled as part of this application, thus this section is not applicable.

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(k)	Facilities/Lot	Residential or limited recreational boating facilities are not applicable to this application.
(l)	Cribs	Residential or limited recreational boating facilities are not applicable to this application.
(8)	Standards	
(a)	Site Plans	Site plans are provided which include MLW and MHW contours.
(b)	PE Stamp	Plan drawings are stamped by an RI licensed Professional Engineer (Matthew J. Page P.E. No. 9448).
(c)	Structural Design	Structures were designed by a licensed Professional Engineer and shall be constructed in accordance with the ASCE and RI State Building Code.
(d)	Submerged Aquatic Vegetation	Per the RI SAV mapping tool there are no known SAV in this area. Additionally, with the exception of small expansions, the work is all being completed within an existing MPL.
(9)	Marina Standards	
(a)	Design for Storm Frequency	Plans were designed by a Professional Engineer and incorporate or exceed the FEMA 100-Year BFE.
(b)	Preliminary Determination (PD)	This is not technically a new marina due to the existing permitted MPL, and not a significant expansion by definition, so a PD was not required by the CRMC regs. However, a preliminary review of the file and information request was submitted to the applicant.
(c)	Impacts of Project	This facility was in operation as a marina up until the early 2000's. The applicant intends to restore the facility to what it was once used for. Any new impacts not associated with the existing facility are minimal. Proposed uses of the marina are consistent with other water dependent uses in the area.
(d)	Vessel Density	The proposed vessel density is greater than 30/acre.
(e)	Dry Stacked Vessels	Not applicable to this application.
(f)	Dock Layout	The proposed layout is similar to what has been utilized at this facility in the past with the expansion of one vessel (16 slips to 17 slips) and slight expansion of the MPL which aids in accomplishing adequate fairway widths.
(g)	Maximum Length Dock	Maximum dock lengths do not exceed 1000'.
(h&i)	Sanitary Facilities	Adequate sanitary facilities for the proposed number of vessels are present at the existing building for marina patron use. A pump out has not historically been required at this facility due to existing pump-out availability in the area.

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(j)	Fire Code	Applicant shall be responsible for following all applicable state and local codes and the building official sign-off form was included which indicates the Building Official has been notified and a permit from them will be issued/required.
(k)	Electrical Installations	Applicant shall be responsible for following all applicable state and local codes and the building official sign-off form was included which indicates the Building Official has been notified and a permit from them will be issued/required.
(l)	Parking	Policy is 1 space for every 1.5 vessels which would require 12 parking spaces for this facility. A total of 18 spaces are available and highlighted in applicant's Public Access Plan.
(m&n)	Minor Repairs and Heavy Machinery/Marina Maintenance Program	Applicant understands and will adhere to CRMC requirements for minor repairs and proper notifications for the use of heavy machinery. Standards have been implemented in the applicant's Operations and Maintenance Plan (OMP).
(o)	Marina Perimeter Limit (MPL)	The MPL is designated on the plan drawings and encompasses all in water structures. Plan drawings meet the maximum 10' off structures requirement.
(p)	Vessel Berthing	All vessels are proposed to be berthed within the MPL.
(q)	MPL and Capacity Alterations	This proposal increases the previously assented slip count by 1 vessel (16 slips to 17 slips) as well as a modest expansion of the MPL. As such, this application is being treated as a CAT B.
(r & s)	Operations & Maintenance Plan (OMP)	An OMP was submitted and follows CRMC's guidance document.
(t)	Clean Marina Cert	This marina has not gone through the Clean Marina Program and thus has submitted the full OMP.
(u)	Mooring Area Alterations	Not applicable to this application.
(v, w, & x)	Pumpout Facility	The historic marina did not previously facilitate pump-out capabilities. The applicant has plans in place to provide a pump out stub for use by a Tiverton operated pump out boat. This will help service local recreational boaters on moorings or recreational vessels transiting the area.
(y)	Setback Policy	There are no mooring fields within 50' of the proposed MPL and there is no Federal Navigation Project/Federal Nav. Channel in the vicinity. This policy is satisfied.
(z)	Floating Dock Materials and Replacement	Applicant shall replace all floats with new which are properly encapsulated thus satisfying this policy.

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(aa)	ADA Compliance	The proposed capacity of this marina would fall under the regs of a Limited Marina (less than 25 vessels) which are not required to be ADA compliant.
(10)	Launching Ramp Standards	There is an existing marine railway basin at the facility. The railway tracks were removed during a prior project/assent. The railway basin is proposed to be filled and converted to parking. There is an existing public launch ramp immediately adjacent to the north to mitigate any negative impacts to removal of this old railway and basin. There is no railway or launching ramp on the applicant's property which would be applicable to this regulation.
1.3.6	Protection and Enhancement of Public Access to the Shore	The facility's current state prohibits any kind of public access to this location. The proposed repairs and improvements will correct this and allow use of this section of the Sakonnet River. There is an existing CRMC ROW and public boat launch to the north. These public access areas will be unimpeded by the applicant's proposal. In addition, more parking will be created at the facility for marina patrons. This will free up space at the boat ramp and boat ramp parking with marina tenants storing their vessels in marina slips rather than launching at the boat ramp.

COMMENTS ON VARIANCE REQUEST:

- No variance requests were required for this application.

COMMENTS ON OBJECTIONS:

- No objections were received during the public comment period. The Tiverton Harbor Commission did submit a letter in support of the project.

CONCLUSION AND RECOMMENDATION:

The applicant is proposing to repair and maintain an existing facility for the use of a commercial marina. This facility has historically been utilized as a marina up until it was purchased by the state for use as a lay down area for construction of the Sakonnet River Bridge. The facility was historically a 16-slip marina. This location and the associated structure have been fairly derelict since demobilization of the bridge project and the applicant's proposal seeks to restore this facility to improve and expand on water dependent businesses along our coastline. This falls in line with a priority use and the Council's goals of this water type.

This facility is located in Type 3 waters which has a Council policy to maintain and expand recreational boating and enhance water dependent businesses. The primary purpose of this marina will be to berth commercial fishing vessels. While this project does not directly provide recreational boating slips, the applicant is proposing charter vessels be allowed at the facility should there be vacant slips unoccupied by commercial vessels. In addition, the applicant is proposing a pump out hookup be included for use by a

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Tiverton operated pump out vessel. This vessel would be utilized to service local recreational boaters on moorings as well as transient vessels coming through the area. It is Staff's opinion that the applicant is working to incorporate Council goals and policies of this water type into their commercial marina.

In association with this project the applicant has proposed fill in an approximately 2,000 sqft area and a volume of 260 cubic yards in tidal waters. This proposed filling aids in stabilization of the existing rip rap slope in conjunction with installation of sheet pile and fills the old unused railway basin to help provide additional parking in the area. It is Staff's opinion that this filling of tidal waters behind a proposed bulkhead is the minimum necessary to accomplish this shoreline stabilization work and is also below the thresholds that would typically require a CAT B review.

The proposal involves an increase in slip count (16 total slips currently assented with 17 total slips proposed), and a modest expansion of the MPL of approximately eleven (11') feet to the west and approx. five (5') feet to the south at the southwestern MPL coordinate. Note that while the southern edge of the proposed MPL falls outside of the applicant's southern property line, it does tie in with the previously approved MPL (2005-02-086) where it meets landfall. Staff believe these proposed increases are modest and follow the applicant's intent to rebuild and maintain this facility in similar fashion to its historical uses.

This facility will provide slip capacity in the eastern bay for commercial fishing vessels and help to maintain and expand Rhode Island's blue economy. Staff recommends approval of this application given the following additional stipulations derived from correspondence and meetings between the applicant and CRMC Staff. Note that these additional stipulations aim to increase the recreational aspects of this facility.

ADDITIONAL STIPULATIONS:

1. The Port's management procedures shall include berthing of charter vessels as a second-tier priority to commercial fishing vessels. Should there be any open slips unfilled by commercial vessels, the slip waiting list shall be opened up for charter vessel use.
2. A pump-out hookup shall be provided and maintained at the facility for use by a pump-out vessel.

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US Army Corps
of Engineers®

PUBLIC NOTICE

Applicant:
David DeCost
RI Department of Environmental
Management

Published: November 5, 2025
Expires: December 5, 2025

New England District
Permit Application No. NAE-2025-00669

TO WHOM IT MAY CONCERN: The New England District of the U.S. Army Corps of Engineers (Corps) has received an application for a Department of the Army permit pursuant to Section 404 of the Clean Water Act (33 U.S.C. §1344) and Section 10 of the Rivers and Harbors Act of 1899 (33 U.S.C. §403). The purpose of this public notice is to solicit comments from the public regarding the work described below:

APPLICANT: David DeCost
RI Department of Environmental Management
235 Promenade Street
Providence, RI 02908

AGENTS: Tim Smith
Megan Elwell
GZA GeoEnvironmental
188 Valley Street, Suite 300
Providence, RI 02909

WATERWAY AND LOCATION: The project would affect waters of the United States associated with the Sakonnet River in Narragansett Bay's east passage at 169 Riverside Drive in Tiverton, RI at 41.637769°N, -71.211462°W.

EXISTING CONDITIONS: Chase Marina is a former medium sized, state-operated commercial fishing facility located on the Sakonnet River in Tiverton, RI. Chase Marina has the capacity to berth 17 commercial fishing vessels ranging in size from 25 to 40-feet. The dock system consists of 26 floating docks of various dimensions, a timber access walkway adjacent to the building, and two aluminum gangways for a total of 1,184 SF of dock space over tidal waters of the U.S.

The facility is comprised of approximately 17,000 square foot (SF) upland area and a 20,400 SF Marina Perimeter Limit (MPL) for a combined total area of approximately 0.85 acres. The upland area contains a two-story commercial building and gravel parking lot. The facility building is supported by a 105 linear feet (LF) granite block seawall and rip-rap revetment. The marina is adjacent to a public access right-of-way with a boat ramp maintained by the Rhode Island Turnpike and Bridge Authority (RITBA) and abutted to the south by a residential property.

PROJECT PURPOSE:

Basic: To repair existing infrastructure and redevelop the marina in support of the State's commercial fishing industry.

Overall: The marina requires structural and operational improvements to provide safe operational conditions as a commercial marina. The restored facility would provide berthing for commercial fishermen and job opportunities to individuals interested in the fishing industry, which is vital to the RI economy.

PROPOSED WORK: The applicant requests authorization to repair existing infrastructure and redevelop the Chase Marina facility site as a commercial marina. The Chase Marina facility, which was constructed prior to 1939, now consists of a two-story building, onsite parking, and remnants of vessel berthing infrastructure. The facility was previously used for seafood processing, sales, and as a recreational marina. RIDEM proposes to redevelop the facility in support of the State's commercial fishing industry and bring back the potential for the processing and distribution of seafood.

Approximately 2,025 SF (260 cubic yards) of permanent impacts (fill) will be discharged below the high tide line for the installation of a new 130 LF sheet pile bulkhead, repairing voids in rip rap slopes behind the existing bulkhead, and raising grades to match the existing parking area. The project also includes vibratory installation of 52, 16-inch diameter steel pipe piles and 29, 14-inch diameter timber piles, which will impact a total of 104 SF of bottom habitat below the mean high water (MHW) line. Proposed maintenance activities will also occur along approximately 350 LF of shoreline.

AVOIDANCE AND MINIMIZATION: The applicant has provided the following information in support of efforts to avoid and/or minimize impacts to the aquatic environment: Minimal soil disturbance is anticipated at the landward connection area, where sand-gravel fill is proposed. During the construction phases, composite silt filter socks will be utilized as perimeter controls at the limit of work to minimize soil erosion and deposition. A turbidity curtain will be deployed around the perimeter of all in-water work to minimize disturbance of soils within the tidal zone. Construction entrances will be established to prevent sediment tracking. Sediment bag inlet protection will be used within Site catch basins. Temporary controls are illustrated in Drawing 5 with details included in Drawing 6, attached.

COMPENSATORY MITIGATION: The applicant offered the following compensatory mitigation plan to offset unavoidable functional loss to the aquatic environment: As the Chase Marina project involves unavoidable permanent fill and the functional loss of aquatic resources in an intertidal area exceeding 1,000 SF, compensatory mitigation is required in accordance with the NAE Mitigation SOP, Appendix O, at a 1:1 replacement ratio. To offset the loss, the applicant, Rhode Island Department of Environmental Management (RIDEM) is considering the construction of a compensatory mitigation

project at a site within the same watershed as the proposed project or utilization of the Massachusetts In-Lieu Fee Program as the sole compensatory mitigation vehicle.

CULTURAL RESOURCES:

The Corps evaluated the undertaking pursuant to Section 106 of the National Historic Preservation Act (NHPA) utilizing its existing program-specific regulations and procedures along with 36 CFR Part 800. The Corps’ program-specific procedures include 33 CFR 325, Appendix C, and revised interim guidance issued in 2005 and 2007, respectively. The District Engineer consulted district files and records and the latest published version of the National Register of Historic Places and initially determines that should historic properties (i.e., properties listed in or eligible for inclusion in the National Register of Historic Places) be present within the Corps’ permit area, the proposed activity requiring the DA permit (the undertaking) is a type of activity that has no potential to cause an effect to an historic property.

The District Engineer’s final eligibility and effect determination will be based upon coordination with the SHPO and THPO, as appropriate and required, and with full consideration given to the proposed undertaking’s potential direct and indirect effects on cultural resources and historic properties within the Corps-identified permit area.

ENDANGERED SPECIES: The Corps has performed an initial review of the application, the U.S. Fish and Wildlife Service (USFWS) Information for Planning and Consultation (IPaC), National Marine Fisheries Service (NMFS) Greater Atlantic Regional Fisheries Office (GARFO) Section 7 Mapper, and the NMFS Critical Habitat Mapper to determine if any threatened, endangered, proposed, or candidate species, as well as the proposed and final designated critical habitat may occur in the vicinity of the proposed project. Based on this initial review, the Corps has made a preliminary determination that the proposed project may affect species and critical habitat listed below. No other ESA-listed species or critical habitat will be affected by the proposed action.

GZA reached a “no effect” determination under Section 7 of the ESA for the proposed endangered tricolored bat (*Perimyotis subflavus*) and endangered roseate tern (*Sterna dougallii dougallii*) on September 9, 2025 utilizing the IPaC online determination keys (IPaC Record Locator 2025-0144889). Thus, Corps coordination for these two species is complete.

Species Common Name and/or Critical Habitat Name	Scientific Name	Federal Status
Atlantic sturgeon	<i>Acipenser oxyrinchus oxyrinchus</i>	Threatened/Endangered
Shortnose sturgeon	<i>Acipenser brevirostrum</i>	Endangered
Green sea turtle	<i>Chelonia mydas</i>	Threatened
Kemp’s Ridley sea turtle	<i>Lepidochelys kempii</i>	Endangered
Leatherback sea turtle	<i>Dermochelys coriacea</i>	Endangered
Loggerhead sea turtle	<i>Caretta caretta</i>	Endangered
North Atlantic right whale	<i>Eubalaena glacialis</i>	Endangered
Fin whale	<i>Balaenoptera physalus</i>	Endangered

Pursuant to Section 7 ESA, any required consultation with the Service(s) will be conducted in accordance with 50 CFR part 402.

This notice serves as request to the U.S. Fish and Wildlife Service and National Marine Fisheries Service for any additional information on whether any listed or proposed to be listed endangered or threatened species or critical habitat may be present in the area which would be affected by the proposed activity.

ESSENTIAL FISH HABITAT: Pursuant to the Magnuson-Stevens Fishery Conservation and Management Act 1996, the Corps reviewed the project area, examined information provided by the applicant, and consulted available species information.

The Corps intends to initiate Essential Fish Habitat (EFH) consultation separately from this public notice. A separate EFH consultation package will be sent to the National Marine Fisheries Service (NMFS). The Corps will not make a permit decision until the consultation process is complete.

Our final determination relative to project impacts and the need for mitigation measures is subject to review by and coordination with the National Marine Fisheries Service.

NAVIGATION: The proposed structure or activity is located within the Sakonnet River and between the federal navigation channel reaches of CENAE_RI_01_SAR_1 and CENAE_MA_62_FLR_9, however the project will not impact the channel reaches.

SECTION 408: The applicant will not require permission under Section 14 of the Rivers and Harbors Act of 1899 (33 USC 408) because the activity, in whole or in part, would not alter, occupy, or use a Corps Civil Works project.

WATER QUALITY CERTIFICATION: Water Quality Certification under Section 401 of the Federal Clean Water Act (33 USC 1341) is required from the RI Department of Environmental Management (RIDEM).

COASTAL ZONE MANAGEMENT ACT CONSISTENCY: A Consistency Determination is required under the Federal Coastal Zone Management Act (16 USC 33) from the RI Coastal Resources Management Council (CRMC).

NOTE: This public notice is being issued based on information furnished by the applicant. This information has not been verified or evaluated to ensure compliance with laws and regulation governing the regulatory program. The geographic extent of aquatic resources within the proposed project area that either are, or are presumed to be, within the Corps jurisdiction has not been verified by Corps personnel.

EVALUATION: The decision whether to issue a permit will be based on an evaluation of the probable impact including cumulative impacts of the proposed activity on the public interest. That decision will reflect the national concern for both protection and

utilization of important resources. The benefits, which reasonably may be expected to accrue from the proposal, must be balanced against its reasonably foreseeable detriments. All factors which may be relevant to the proposal will be considered including cumulative impacts thereof; among these are conservation, economics, esthetics, general environmental concerns, wetlands, historical properties, fish and wildlife values, flood hazards, floodplain values, land use, navigation, shoreline erosion and accretion, recreation, water supply and conservation, water quality, energy needs, safety, food, and fiber production, mineral needs, considerations of property ownership, and in general, the needs and welfare of the people. Evaluation of the impact of the activity on the public interest will also include application of the guidelines promulgated by the Administrator, EPA, under authority of Section 404(b) of the Clean Water Act or the criteria established under authority of Section 102(a) of the Marine Protection Research and Sanctuaries Act of 1972. A permit will be granted unless its issuance is found to be contrary to the public interest.

COMMENTS: The Corps is soliciting comments from the public; Federal, State, and local agencies and officials; Indian Tribes; and other Interested parties in order to consider and evaluate the impacts of this proposed activity. Any comments received will be considered by the Corps to determine whether to issue, modify, condition, or deny a permit for this proposal. To make this determination, comments are used to assess impacts to endangered species, historic properties, water quality, general environmental effects, and the other public interest factors listed above. Comments are used in the preparation of an Environmental Assessment (EA) and/or an Environmental Impact Statement pursuant to the National Environmental Policy Act (NEPA). Comments are also used to determine the need for a public hearing and to determine the overall public interest of the proposed activity.

The New England District will receive written comments on the proposed work, as outlined above, until December 5, 2025. Comments should be submitted electronically via the Regulatory Request System (RRS) at <https://rrs.usace.army.mil/rrs> or to cenae-r-ri@usace.army.mil. Alternatively, you may submit comments in writing to the Commander, U.S. Army Corps of Engineers, New England District, Attention: Elizabeth Waterhouse, 696 Virginia Road, Concord, Massachusetts 01742. Please refer to the permit application number in your comments.

Any person may request, in writing, within the comment period specified in this notice, that a public hearing be held to consider the application. Requests for public hearings shall state, with particularity, the reasons for holding a public hearing. Requests for a public hearing will be granted, unless the District Engineer determines that the issues raised are insubstantial or there is otherwise no valid interest to be served by a hearing.

CHASE MARINA

169 RIVERSIDE DRIVE TIVERTON, RHODE ISLAND

90% DESIGN

AUGUST, 2025

PREPARED FOR:



RHODE ISLAND DEPARTMENT
OF ENVIRONMENTAL MANAGEMENT
301 GREAT ISLAND ROAD
NARRAGANSETT, RHODE ISLAND

DESIGNED BY:



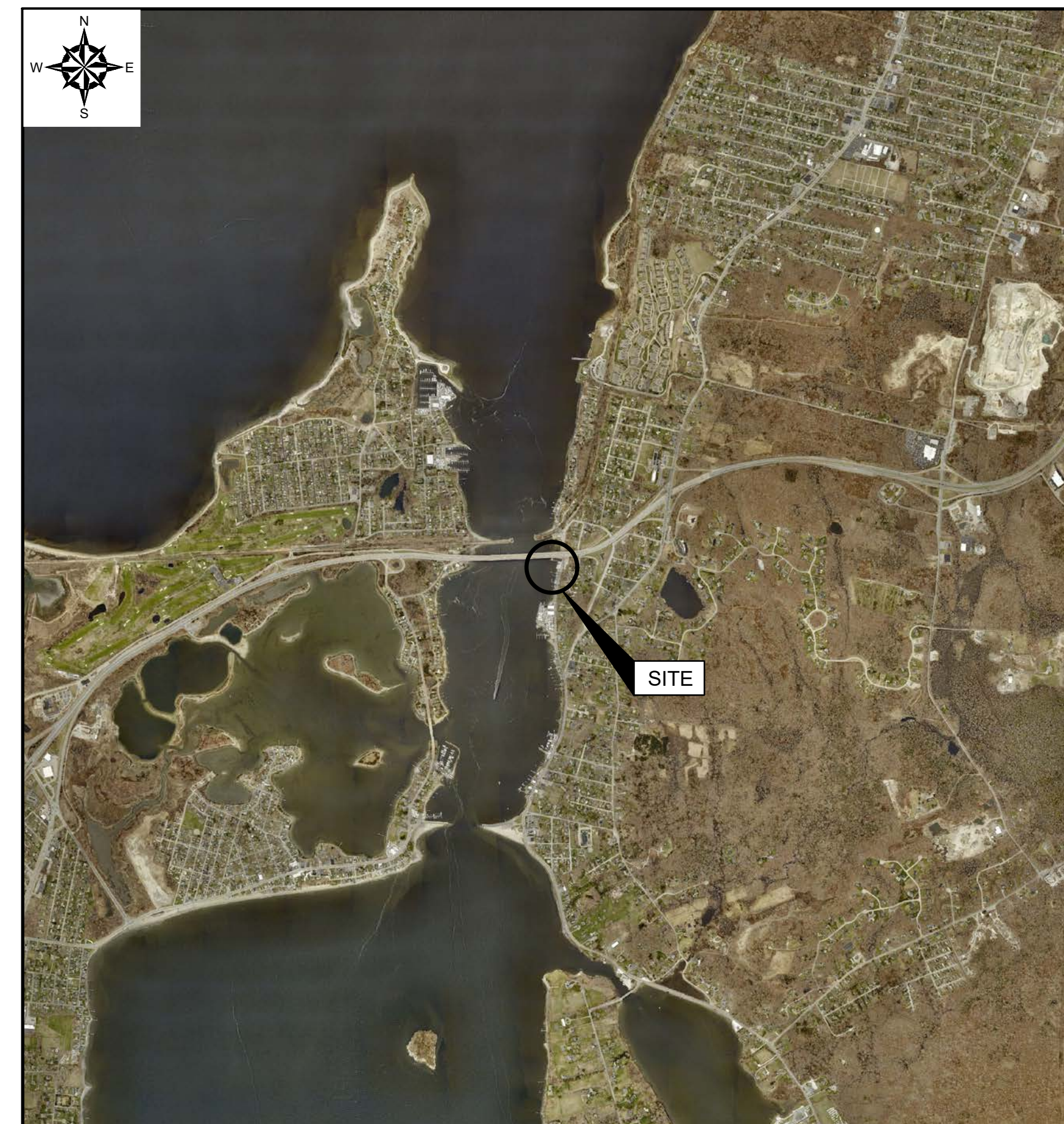
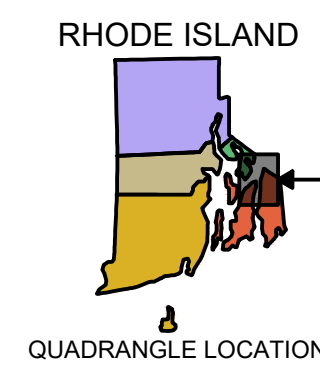
GZA GEOENVIRONMENTAL, INC.
188 VALLEY ST. SUITE 300
PROVIDENCE RHODE ISLAND, 02909



PROJECT LOCUS MAP



SOURCE: USGSSTORE.GOV
BASE MAP FROM THE FOLLOWING USGS QUADRANGLE MAP:
TIVERTON, RHODE ISLAND (2021), FALL RIVER, MASSACHUSETTS (2021)
DIGITAL TOPOGRAPHIC MAPS PROVIDED BY USGSSTORE.GOV.
CONTOUR ELEVATIONS REFERENCE NAVD 88.
CONTOURS ARE SHOWN IN FEET AT 10 FOOT INTERVALS.



PROJECT VICINITY MAP



BASE MAP DEVELOPED FROM RIGIS AERIAL IMAGERY
PUBLISHED IN APRIL 2019.

INDEX OF DRAWINGS	
SHEET No.	SHEET TITLE
1	COVER SHEET AND INDEX OF DRAWINGS
2	GENERAL NOTES
3	EXISTING CONDITIONS PLAN
4	EXISTING CONDITIONS SECTIONS
5	TEMPORARY CONTROLS PLAN - PHASE I
6	TEMPORARY CONTROLS DETAILS
7	DEMOLITION PLAN
8	DEMOLITION SECTIONS
9	PROPOSED CONDITIONS PLAN - PHASE I
10	PROPOSED CONDITIONS SECTIONS - PHASE I
11	PROPOSED TIMBER DETAILS - PHASE I
12	PROPOSED GANGWAY-DOCK DETAILS - PHASE I
13	PROPOSED BULKHEAD-OUTFALL DETAILS - PHASE I
14	PROPOSED CONDITIONS AND FINAL CONDITIONS PLAN - PHASE II
15	PROPOSED CONDITIONS SECTIONS - PHASE II

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REFERENCE NOTES

EXISTING CONDITIONS PLAN BASE MAP DEVELOPED FROM THE FOLLOWING:

- 1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16, PREPARED BY DOUGET SURVEY AND TRANSMITTED TO GZA ON SEPTEMBER 16, 2024..
2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024..
3. LOCATION OF OVERHEAD BRIDGE APPROXIMATED USING GOOGLE EARTH IMAGERY.

GENERAL NOTES

- 1. LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY, AND ARE NOT WARRANTED TO BE CORRECT. ADDITIONAL UTILITIES MAY EXIST WHICH ARE NOT INDICATED ON THESE PLANS. ALL EXISTING UTILITIES SHALL BE VERIFIED FOR SERVICE, SIZE, INVERT ELEVATION, LOCATION, ETC. PRIOR TO ANY CONSTRUCTION WORK IN THE VICINITY THEREOF.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINDING, VERIFYING, AND RE-SETTING (IF NECESSARY) CONTROL POINTS AND BENCHMARKS FOR THE WORK OF THE CONTRACT OR OTHERWISE ESTABLISHING THEIR OWN.
3. THE RESPONSIBILITY FOR SAFETY IN, ON, OR ABOUT THE JOBSITE SHALL BE THAT OF THE CONSTRUCTION CONTRACTOR. THESE DRAWINGS DO NOT INCLUDE COMPONENTS WHICH MAY BE NECESSARY FOR CONSTRUCTION SAFETY.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION, EXCEPT WHERE SPECIFICALLY DETAILED IN THE PLANS. LIKEWISE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SEQUENCE OF THE WORK, EXCEPT WHERE SPECIFICALLY DETAILED IN THE PLANS.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SURFACE AND GROUNDWATER CONTROL DURING THE WORK OF THE CONTRACT. TEMPORARY WATER CONTROL MEASURES SHALL BE, AT MINIMUM, AS REQUIRED BY THE PROJECT PLANS, SPECIFICATIONS, AND PERMIT CONDITIONS.
6. ACCESS TO THE SITE SHALL BE FROM RIVERSIDE DRIVE. THE CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED BY CONSTRUCTION AS PER THE PLANS AND SPECIFICATIONS.
7. CONTRACTOR STAGING AREAS SHALL BE WITHIN LIMITS OF WORK AS DELINEATED ON THE PLANS.
8. PRIOR TO THE START OF WORK, THE CONTRACTOR SHALL DEVELOP, SUBMIT, AND MAINTAIN AN EMERGENCY CONTACT LIST WITH NAMES AND PHONE NUMBERS (DAY AND NIGHT) OF ALL KEY PERSONNEL INVOLVED WITH THE PROJECT.
9. THE CONTRACTOR IS RESPONSIBLE FOR ALL PENALTIES AND DELAYS DUE TO NON-COMPLIANCE WITH PERMIT CONDITIONS.
10. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS IN THE FIELD.

EROSION AND SEDIMENTATION CONTROL

- 1. TEMPORARY CONTROL OF EROSION AND SEDIMENT DISCHARGE IS REQUIRED THROUGHOUT THE DURATION OF THE PROJECT AND UNTIL PROPOSED STABILIZATION IS ACHIEVED.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING ALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES NECESSARY TO EXECUTE AND COMPLETE THE WORK OF THE CONTRACT.
3. ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO PREVENT MIGRATION INTO WATER BY SILT, SEDIMENT, FUELS, SOLVENTS, LUBRICANTS, CONCRETE, OR ANY OTHER POLLUTANTS ASSOCIATED WITH CONSTRUCTION PROCEDURES.
4. ACTUAL LOCATION OF EROSION CONTROLS AND BEST MANAGEMENT PRACTICES (BMP) MAY VARY DUE TO FIELD CHANGES, ONGOING CONSTRUCTION, ACCESS NEEDS, WEATHER, ETC.
5. ALL EROSION CONTROLS AND BMPs SHALL REMAIN IN PLACE, EXCEPT AS OTHERWISE NECESSARY.
6. ADDITIONAL EROSION CONTROL BARRIERS SHALL BE INSTALLED AT THE DIRECTION OF THE ENGINEER TO MINIMIZE THE THREAT OF ADVERSE IMPACT DURING THE CONSTRUCTION PROCESS.
7. PERIODIC INSPECTION, MAINTENANCE, AND CLEANING OF TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES AND BMPs ARE REQUIRED.
8. CONTRACTOR SHALL PROVIDE AND MAINTAIN TURBIDITY CURTAINS AND IS RESPONSIBLE FOR DETERMINING LAYOUT AND INSTALLATION PROCEDURES IN ACCORDANCE WITH CONTRACT DOCUMENTS.
9. TURBIDITY CURTAINS SHALL BE INSTALLED TO SURROUND THE LIMIT OF WORK, AS NEEDED, INCLUDING BUT NOT LIMITED TO DEMOLITION AND WALL CONSTRUCTION.

WATER CONTROL NOTES

- 1. TEMPORARY WATER CONTROL BY THE CONTRACTOR SHALL BE PERFORMED AS SPECIFIED IN THE CONTRACT DOCUMENTS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TEMPORARY WATER CONTROL, SURFACE WATER AND GROUNDWATER CONTROL, NECESSARY TO EXECUTE AND COMPLETE THE WORK OF THE CONTRACT.
3. ALL TEMPORARY WATER CONTROL MEASURES SHALL BE IMPLEMENTED IN CONJUNCTION WITH APPROPRIATE SEDIMENT AND EROSION CONTROL MEASURES.
4. ANY TEMPORARY PUMPS USED AT THE SITE MUST BE PROPERLY BAFFLED AGAINST EXCESSIVE NOISE.
5. WATER PUMPED FROM THE EXCAVATIONS MUST BE PASSED THROUGH A PUMPED WATER FILTER BAG OR OTHER SUCH BEST MANAGEMENT PRACTICE FEATURE PRIOR TO BEING DISCHARGED BACK TO A WATER BODY.
6. PUMPED WATER DISCHARGE AREAS MUST BE PROPERLY PROTECTED TO PREVENT EROSION BY HIGH VELOCITY FLOW.

CONSTRUCTION NOTES:

- 1. THE CONTRACTOR AND/OR SUBCONTRACTORS MUST SHOW EVIDENCE OF PRIOR EXPERIENCE AND SUCCESSFUL COMPLETION OF THE TYPE OF REPAIR WORK INDICATED.

LAYOUT NOTES:

- 1. ALL LINES ARE PERPENDICULAR OR PARALLEL TO THE LINES FROM WHICH THEY ARE MEASURED UNLESS OTHERWISE INDICATED.
2. WALL LAYOUTS SHALL BE FIELD ADJUSTED TO PROVIDE THE MINIMUM WALKWAY OR GROUND FEATURE DIMENSIONS AS SHOWN ON THE PLANS.

LOAM AND SEED NOTES:

- 1. CONTRACTOR SHALL APPLY FERTILIZER AND LOAM TO PREPARE TOPSOIL SURFACE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
2. CONTRACTOR SHALL SEED ENTIRE DISTURBED AREA AS INDICATED IN THE PROJECT DOCUMENTS.
3. CONTRACTOR SHALL MULCH ENTIRE SEEDED AREA WITH STRAW MULCH TO HELP DEVELOP AN ACCEPTABLE STAND OF GRASS.
4. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE SEEDED AREA THROUGH ONE GROWING SEASON, MAINTENANCE INCLUDES MOWING, WATERING, AND RE-SEEDING AS REQUIRED TO DEVELOP A SATISFACTORY STAND OF HEALTHY GRASS.

DEMOLITION NOTES:

- 1. EXCAVATED MATERIALS SUSPECTED OF CONTAMINATION SHALL BE SEPARATED, AND WRAPPED IN POLYTHENE SHEETING STOCKPILED ON-SITE FOR EVALUATION BY THE ENGINEER.
2. CONTRACTOR SHALL EXERCISE EXTREME CAUTION TO AVOID DAMAGE TO ANY EXISTING UTILITIES AND STRUCTURES TO REMAIN IN PLACE DURING CONSTRUCTION AND/OR AFTER CONSTRUCTION IS COMPLETE.
3. ALL MATERIALS REMOVED AND NOT SPECIFIED TO BE SALVAGED OR REUSED SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND LEGALLY DISPOSED OF OFF-SITE.

MATERIAL NOTES:

- 1. MATERIAL AND PRODUCT REQUIREMENTS, WHERE REFERENCED AS RIDOT, THE REQUIREMENT REFERS TO THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE DESIGN "BLUE BOOK", LATEST EDITION.

GRAVEL BORROW: M.01.02.1

CRUSHED STONE: M.01.06

COMMON FILL: M.01.01

LOAM: L.01.01.1 / M.18.01

FILTER FABRIC: NON-WOVEN FABRIC, INERT TO UV LIGHT, EQUAL IN PROPERTIES TO MIRAFI 180N OR APPROVED EQUAL

2. REINFORCING SHALL HAVE A MINIMUM OF 2 INCHES OF COVER WHEN CONCRETE IS CAST AGAINST FORMWORK. FOR CONCRETE CAST AGAINST EARTH, THE COVER SHALL BE 3 INCHES MINIMUM.

CONCRETE NOTES:

CONCRETE - 28-DAY COMPRESSIVE STRENGTH OF 5,000 PSI MINIMUM
3/4 INCH MAXIMUM AGGREGATE SIZE
AIR CONTENT OF 5.0% - 7.0%
COLD WEATHER OR HOT WEATHER REQUIREMENTS AS APPLICABLE AND AS DETERMINED BY THE ENGINEER

CEMENT - PORTLAND CEMENT, ASTM C 150, TYPE II

REINFORCING STEEL NOTES:

REINFORCING BAR: ASTM A 615, GRADE 60, GALVANIZED ASTM A 123, ASTM A 153

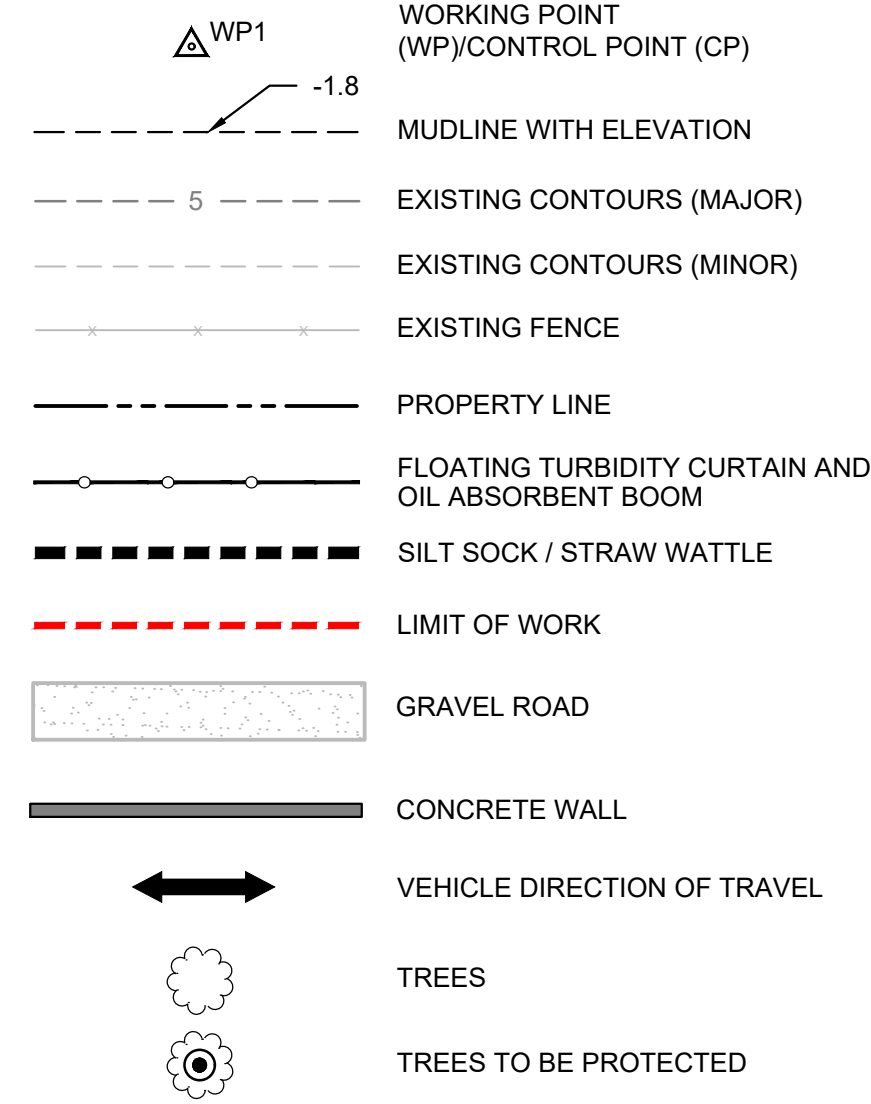
DOWELS: ASTM A 615, GRADE 60, GALVANIZED ASTM A 123, ASTM A 153

GROUT: TWO-COMPONENT TYPE EPOXY ADHESIVE SIKADUR 32, HI MOD OR APPROVED EQUAL

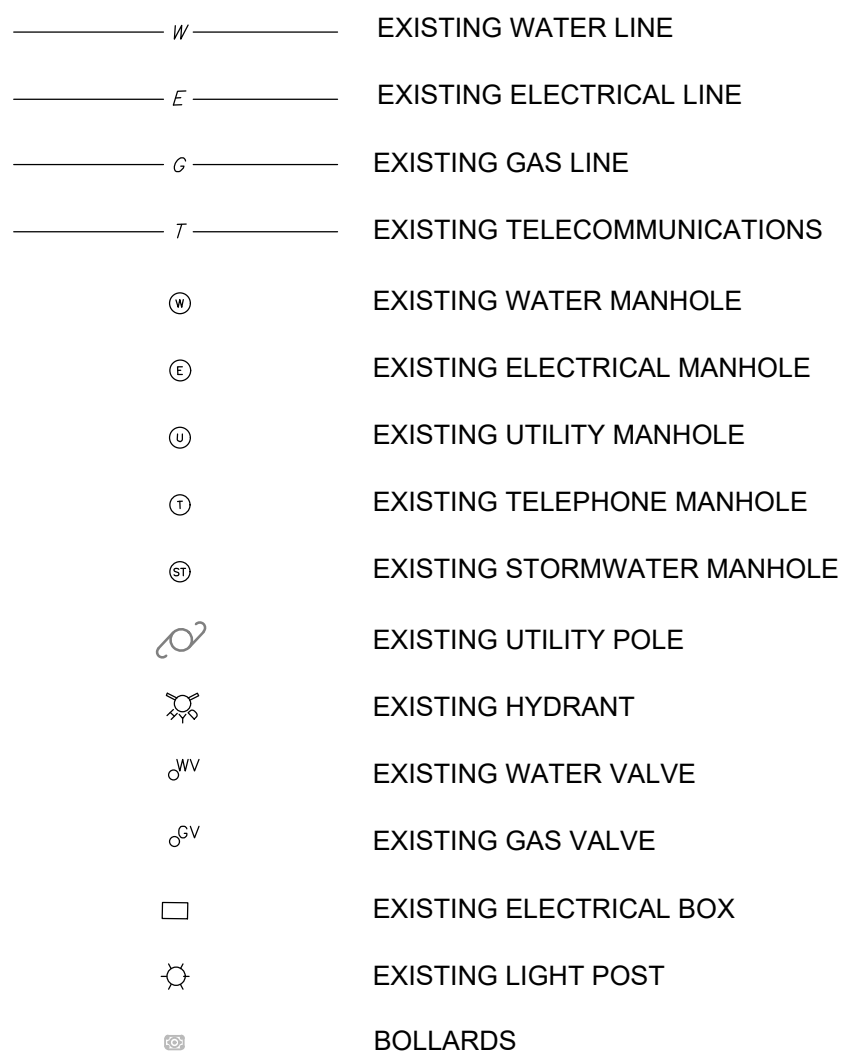
DESIGN CRITERIA:

- A. AMERICAN CONCRETE INSTITUTE, ACI 318, (LATEST EDITION)
B. STEEL CONSTRUCTION MANUAL, AMERICAN INSTITUTE OF STEEL CONSTRUCTION (LATEST EDITION)
C. WELDING OF STRUCTURAL AND MISCELLANEOUS STEEL - STRUCTURAL WELDING CODE (LATEST EDITION)
D. RHODE ISLAND BUILDING CODE

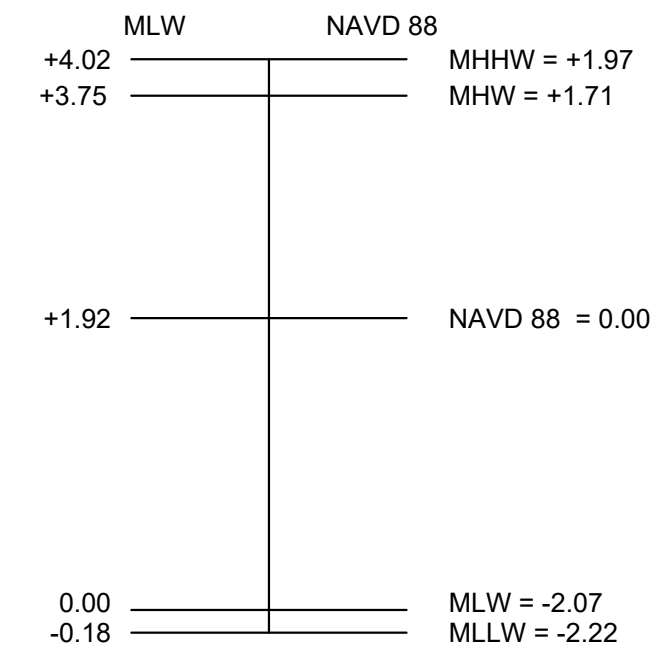
LEGEND



UTILITIES LEGEND:



TIDAL ELEVATIONS



NOTE: TIDAL ELEVATIONS ARE REFERENCED TO NOAA STATION 8450948, ANTHONY POINT, TIVERTON, RI.

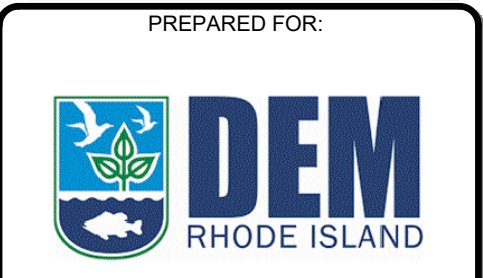
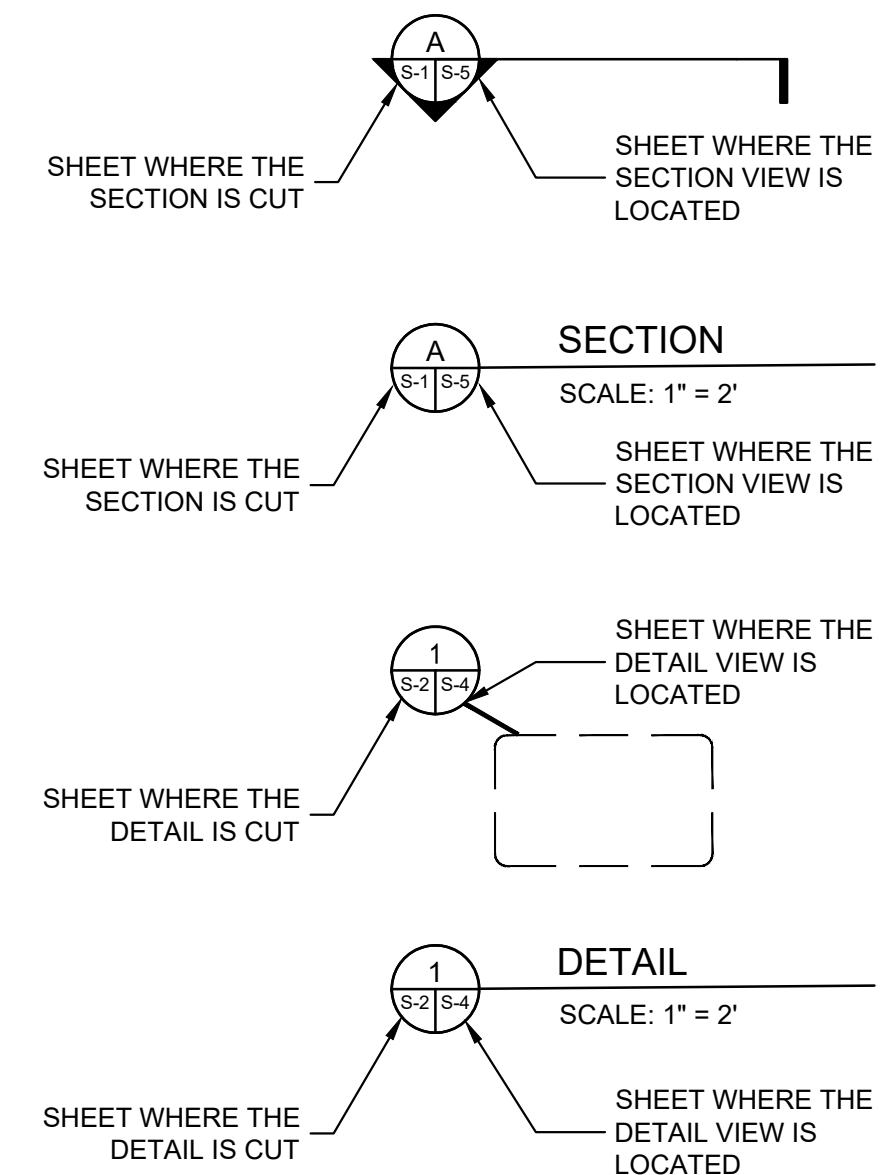
SURVEY AND PROJECT DATUM:

- 1. ALL ELEVATIONS SHOWN ON PLANS ARE IN U.S. FEET AND REFERENCE THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
2. COORDINATES ARE BASED ON NORTH AMERICAN DATUM OF 1983 (NAD83), RHODE ISLAND STATE PLANE, EASTERN ZONE, U.S. FEET (RI83-EF).
3. CONTRACTOR SHALL MAINTAIN ADEQUATE SURVEY CONTROL AT ALL TIMES TO ESTABLISH AND MAINTAIN ALL LINES AND ELEVATIONS.

ABBREVIATIONS AND ACRONYMS

Table with 2 columns: Abbreviation and Full Name. Includes entries like CON (City of Newport), DIA (Diameter), D.I. (Ductile Iron), EL (Elevation), HASP (Health and Safety Plan), HDPE (High-Density Polyethylene), INV (Invert), MHHW (Mean High, High Water), etc.

ANNOTATIONS AND LABELS



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90% DESIGN DRAWINGS NOT FOR CONSTRUCTION

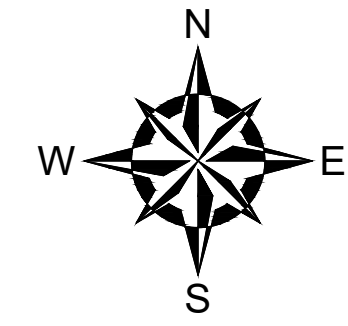
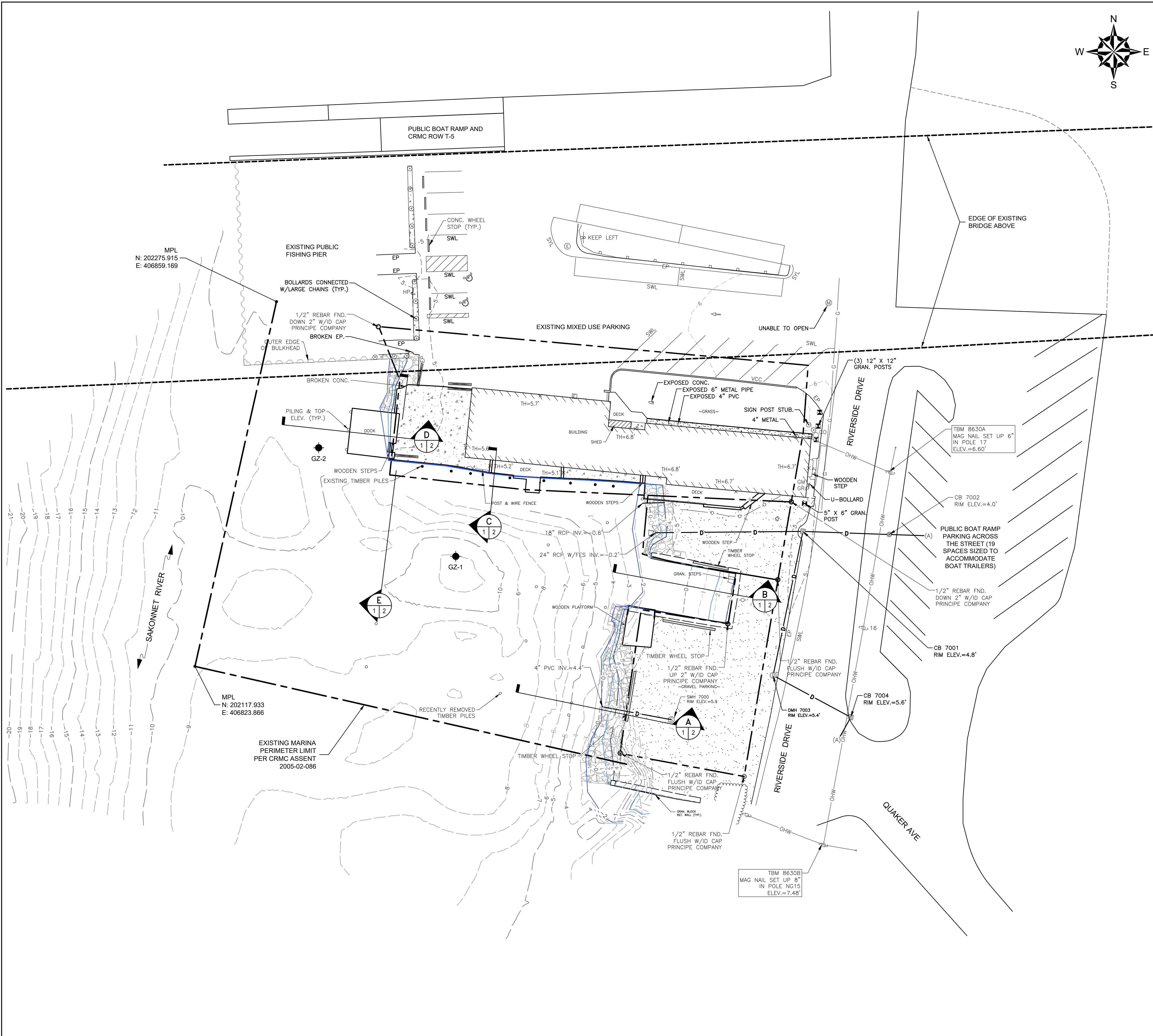
Table with columns: REV, DESCRIPTION, DATE, BY

Table with project details: PROJECT NO: 35429.00, DATE: AUGUST, 2025, PROJECT MGR: TWS, DESIGNED BY: JTF, DRAWN BY: GRB, CHECKED BY: TWS, REVIEWED BY: MJP, SCALE: AS NOTED, REVISION NO.

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND
GENERAL NOTES

DRAWING 2 SHEET NO. 2 OF 15

APPROVED:

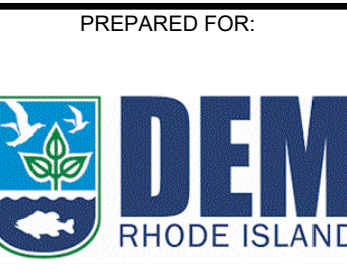


REFERENCE NOTES:

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2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024.

LEGEND

- APPROXIMATE LOT LINE PER REF. PLAN
- - - MAJOR CONTOUR LINE
- - - MINOR CONTOUR LINE
- RETAINING WALL
- CHAIN LINK FENCE
- HANDRAIL
- GUARDRAIL
- OVERHEAD WIRE
- DRAIN LINE
- GAS LINE
- PROPERTY BOUNDARY
- HIGH TIDE LINE (HTL) = EL. 3.2'
- MEAN HIGH WATER (MHW) = EL. 1.71'
- MEAN LOW WATER (MLW) = EL. -2.07'
- SHRUB LINE
- CONCRETE
- RIP RAP
- CRUSHED STONE
- X 5.2 SPOT GRADE
- PIPE/ROD FOUND
- UTILITY POLE
- UTILITY POLE & GUY WIRE
- UTILITY POLE STUB
- DRAIN MANHOLE
- CATCH BASIN
- FLARED END SECTION
- SEWER MANHOLE
- CLEANOUT
- GAS METER
- GAS REGULATOR VALVE
- ELECTRIC MANHOLE
- MANHOLE
- SIGN
- SQUARE POST
- POST
- WOODEN PILING & TOP ELEVATION
- ACCESSIBLE PARKING SPACE
- FLOW DIRECTION
- JERSEY BARRIER
- CONC. CONCRETE
- EP EDGE OF PAVEMENT
- GRAN. GRANITE
- PVC POLYVINYL CHLORIDE PIPE
- RCP REINFORCED CONCRETE PIPE
- RET. WALL RETAINING WALL
- SWL SINGLE WHITE LINE
- SYL SINGLE YELLOW LINE
- TH THRESHOLD ELEVATION
- TYP. TYPICAL
- VCC VERTICAL GRANITE CURB
- (X) INVERT I.D. CONNECTION UNKNOWN
- GZ-1 / GZ-2 TEST BORINGS PERFORMED BY REAGAN MARINE CONSTRUCTION ON FEBRUARY 25 AND 26, 2025 AND OBSERVED BY GZA.



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REV.	DESCRIPTION	DATE	BY

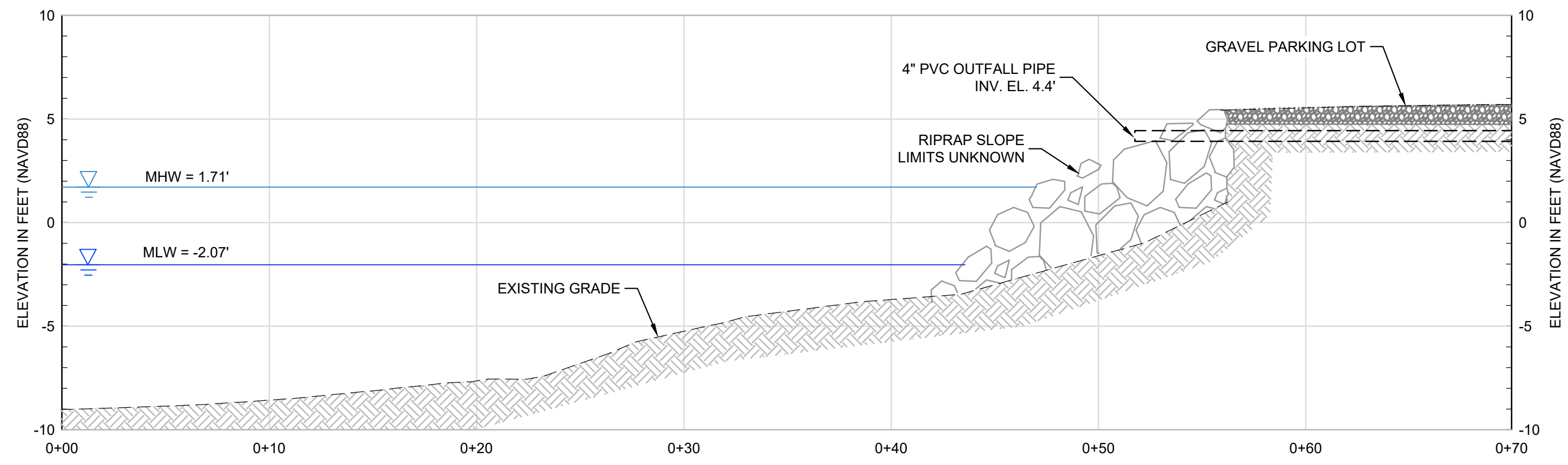
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DATE:	AUGUST, 2025
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DESIGNED BY:	JTF
DRAWN BY:	GRB
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CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

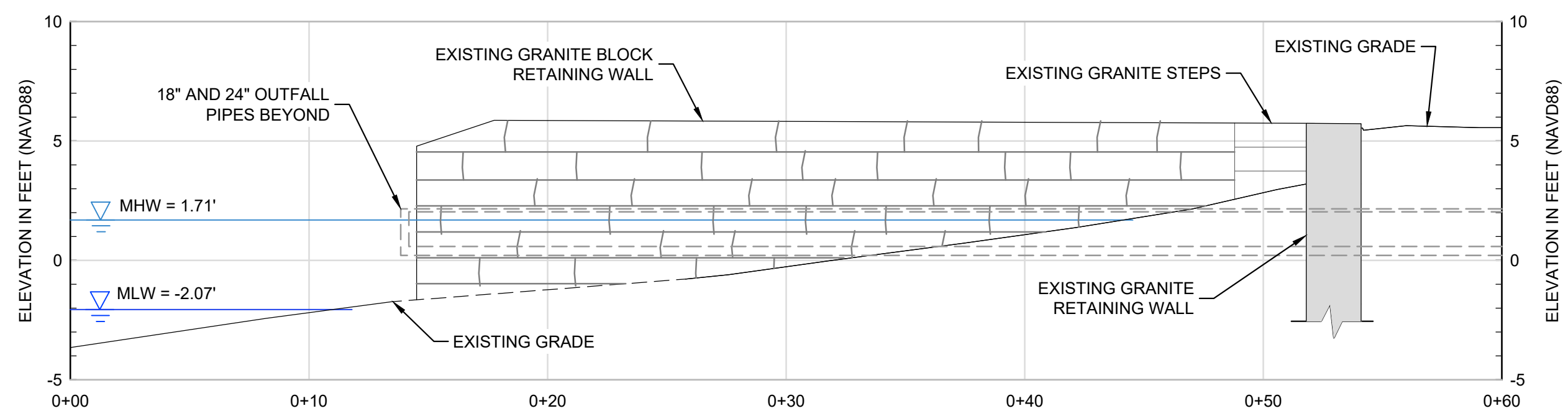
EXISTING CONDITIONS PLAN



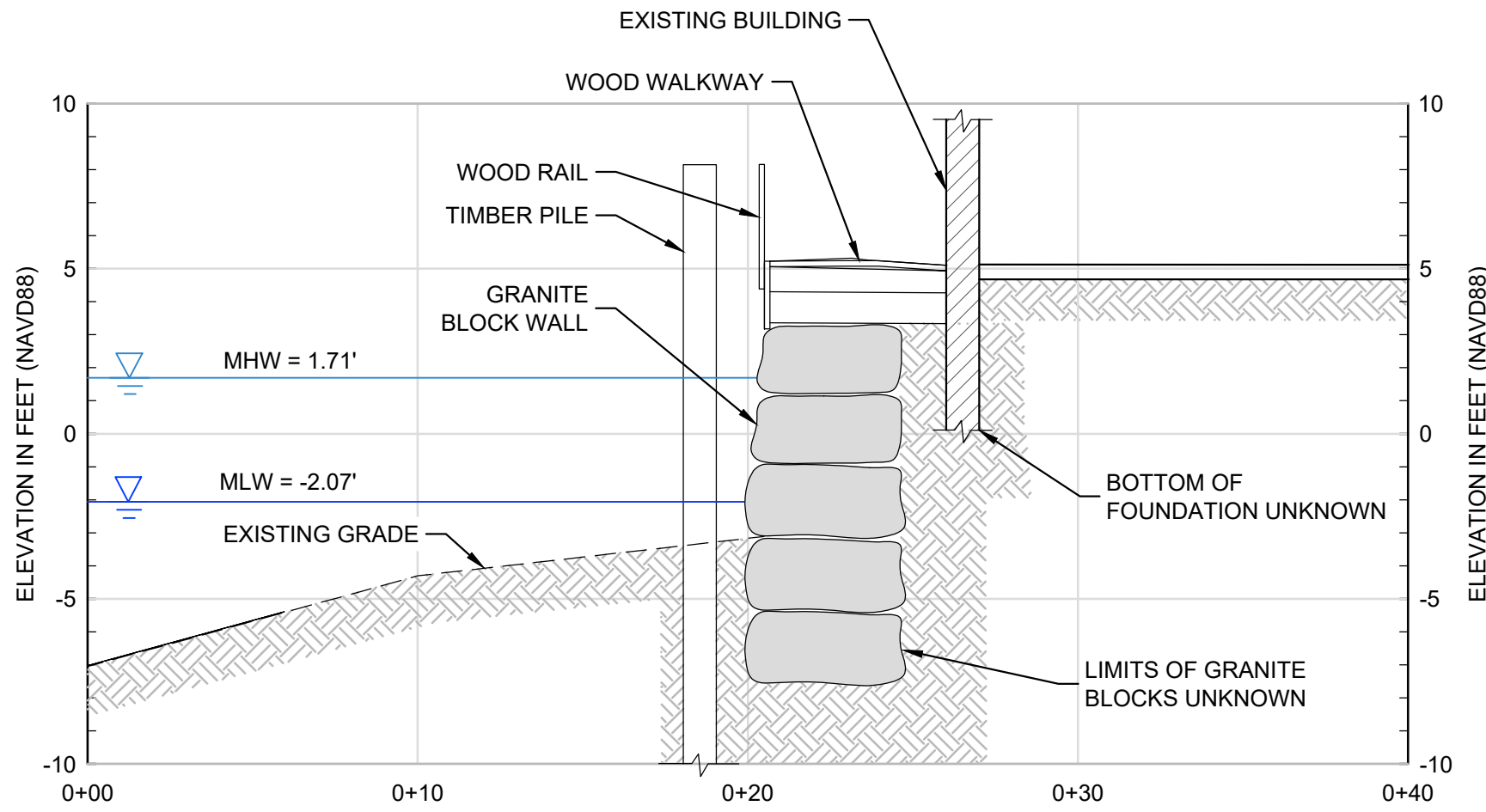
© 2025 - GZA GeoEnvironmental, Inc. GZA-J:\GEO\35429-TWS\FIGURES\CAD\DWGS\CURRENT\PLANS\90% DESIGN_SET\35429.00_4_EXISTING COND SECTIONS.DWG 4 July 7, 2025 GARY BASTIEN



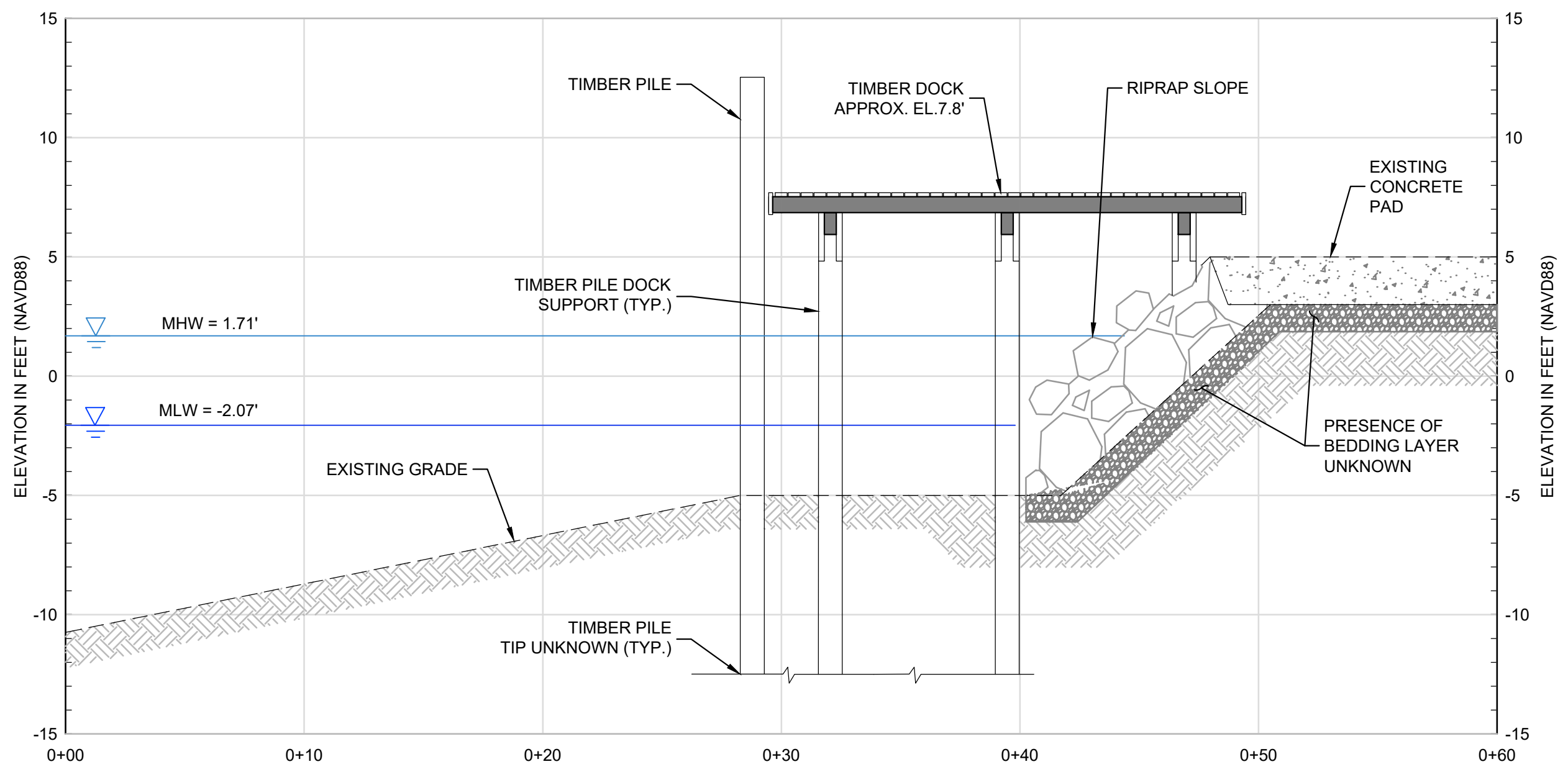
A RIPRAP SLOPE / PARKING LOT - SECTION
SCALE: 1"=5'



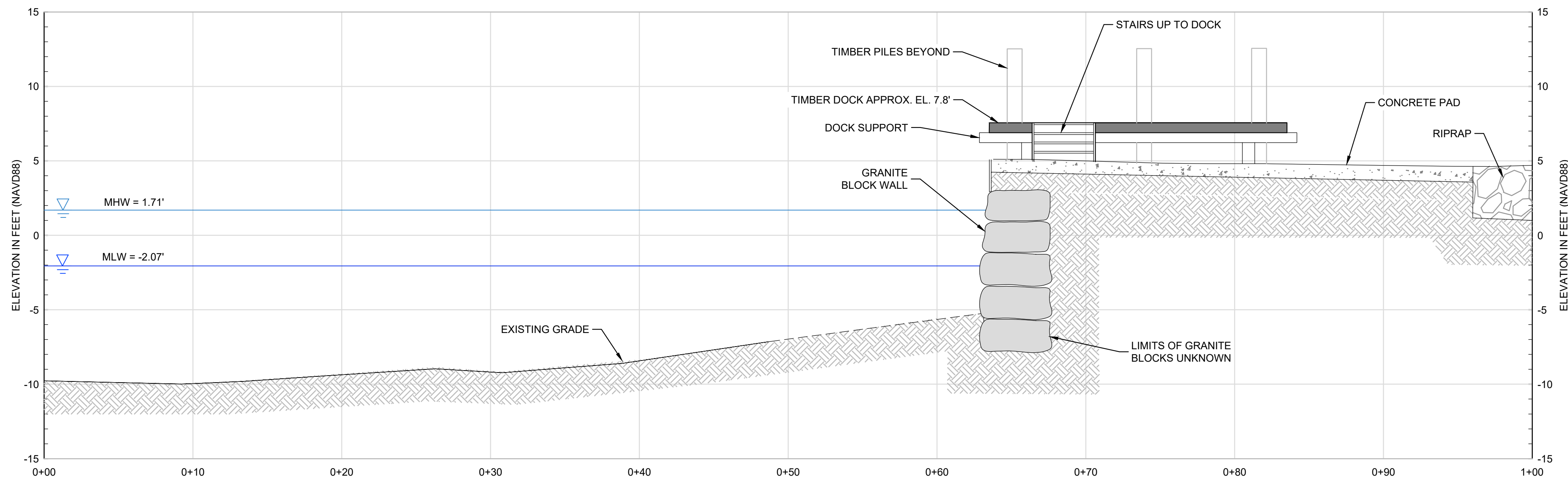
B GRANITE BLOCK WALL - SECTION
SCALE: 1"=5'



C GRANITE BLOCK WALL - SECTION
SCALE: 1"=5'



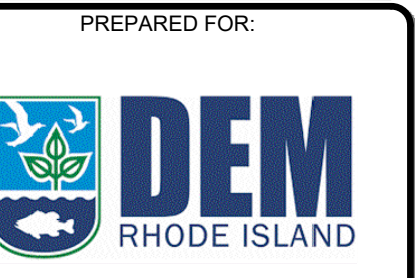
D DOCK PLATFORM - SECTION
SCALE: 1"=5'



E DOCK PLATFORM - SECTION
SCALE: 1"=5'



Known for excellence.
Built on trust



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**90% DESIGN DRAWINGS
NOT FOR CONSTRUCTION**

REV	DESCRIPTION	DATE	BY

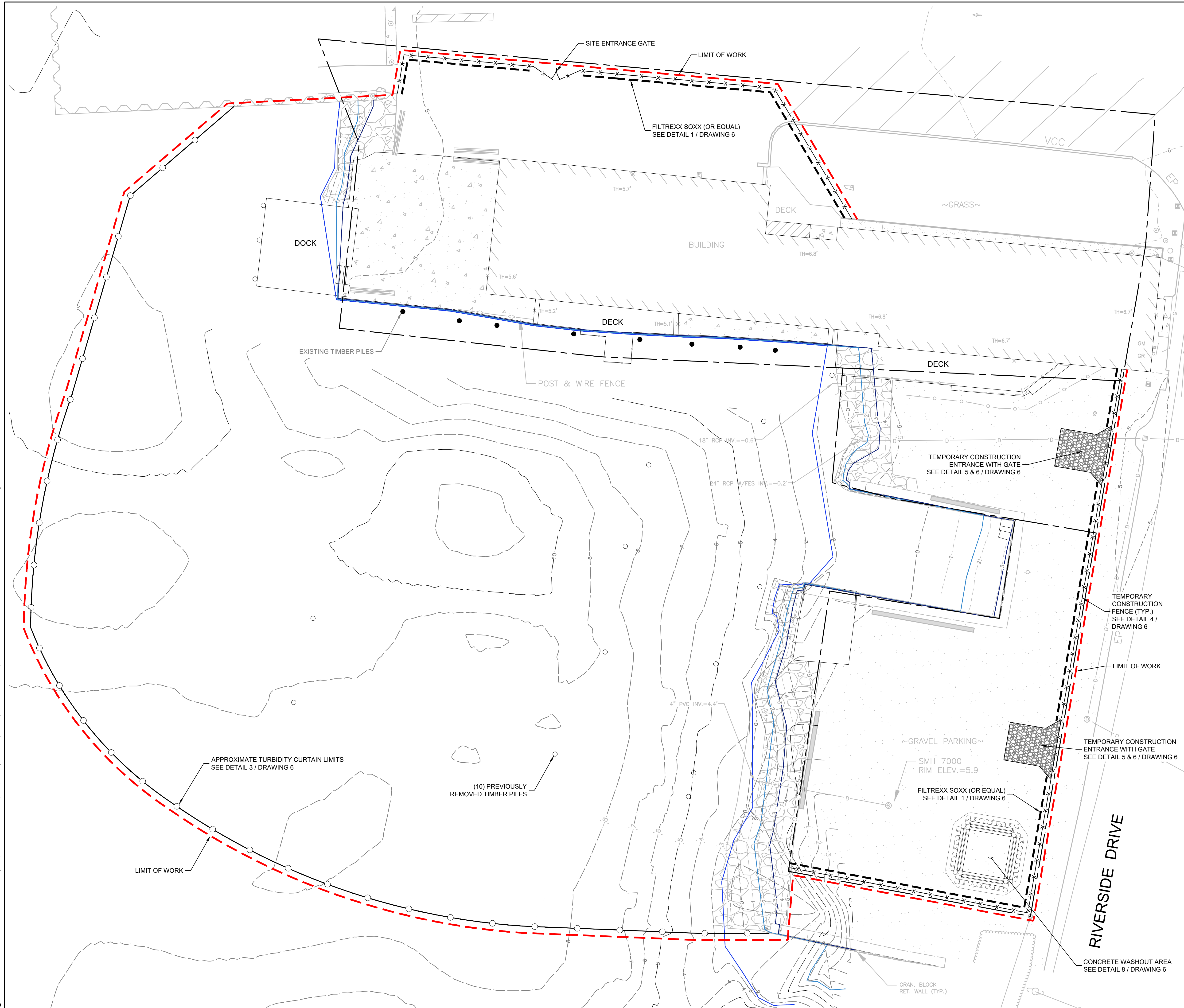
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CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

EXISTING CONDITIONS SECTIONS

DRAWING
4
SHEET NO. 4 OF 15

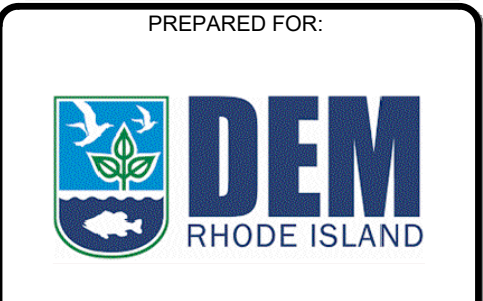
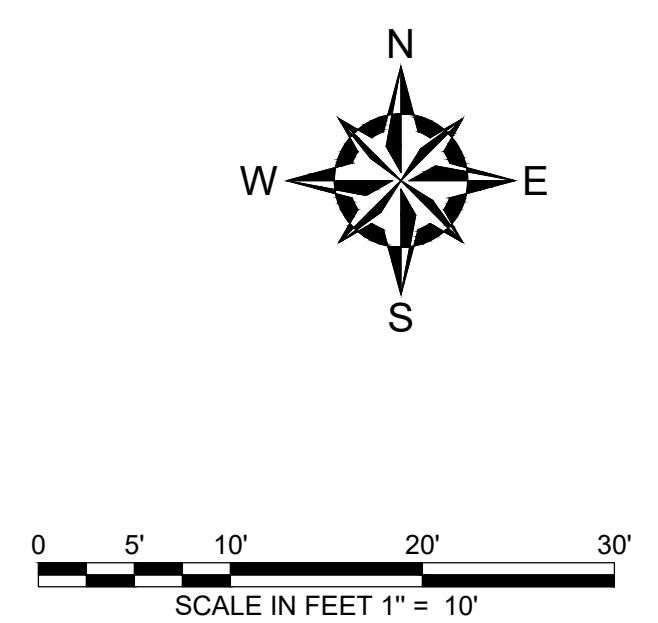
© 2025 - GZA Geoenvironmental, Inc. GZA-J:\GEO\35429.TWS\FIGURES\CAD\DWGS\CURRENT\PLANS\90% DESIGN SET\35429.00_5_TEMPORARY CONTROLS PLAN_PHASE 1.DWG 5 August 29, 2025 GARY BASTIEN



- REFERENCE NOTES:**
1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DORCET SURVEY TRANSMITTED TO GZA ON SEPTEMBER 16, 2024.
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- TEMPORARY CONTROL NOTES:**
1. CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING AND MAINTAINING ALL TEMPORARY EROSION AND SEDIMENT CONTROLS IN ACCORDANCE WITH PERMITS AND CONTRACT DOCUMENTS.
 2. INSTALL EROSION AND SEDIMENT CONTROLS BEFORE DISTURBING ANY SOILS, SEDIMENT OR VEGETATION AT THE SITE.
 3. ADJUST, RELOCATE OR PROVIDE ADDITIONAL EROSION AND SEDIMENT CONTROLS AS WORK PROGRESSES INTO PREVIOUSLY UNDISTURBED AREAS OF THE SITE.
 4. MAINTAIN EROSION AND SEDIMENT CONTROLS UNTIL THE SITE IS RESTORED AND SITE IMPROVEMENTS INCLUDING LANDSCAPING, IF ANY, ARE COMPLETE WITH UNDERLYING SOILS PERMANENTLY STABILIZED.
 5. CARE SHALL BE TAKEN NOT TO PLACE REMOVED SEDIMENTS WITHIN THE PATH OF EXISTING, NEWLY CREATED OR PROPOSED AREAS THAT ARE SUBJECT TO STORMWATER FLOW.
 6. LIMITS OF TURBIDITY CURTAIN ARE SHOW FOR ILLUSTRATION PURPOSES ONLY. CONTRACTOR SHALL PROVIDE AND MAINTAIN TURBIDITY CURTAINS AND IS RESPONSIBLE FOR DETERMINING LAYOUT AND INSTALLATION PROCEDURES IN ACCORDANCE WITH CONTRACT DOCUMENTS.
 7. TURBIDITY CURTAINS SHALL BE INSTALLED TO SURROUND THE LIMIT OF WORK, AS NEEDED, INCLUDING BUT NOT LIMITED TO DEMOLITION AND WALL CONSTRUCTION.
 8. DEWATER LANDWARD OF BULKHEAD USING A PUMP WITH A FILTER BAG. PUMP WATER INTO SAKONNET RIVER.
 9. PROTECTING DEVICES SHALL BE USED AROUND TREES AND TRANSFORMERS ON SITE.

- LEGEND - EROSION CONTROL**
- APPROXIMATE PROPERTY BOUNDARY
 - - - - - LIMIT OF WORK
 - - - - - FILTREXX (R) SILT SOXX (OR EQUAL)
 - x x x x x TEMPORARY FENCE LINE
 - o o o o o FLOATING TURBIDITY CURTAIN



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**90% DESIGN DRAWINGS
NOT FOR CONSTRUCTION**

REV.	DESCRIPTION	DATE	BY

PROJECT NO:	35429.00
DATE:	AUGUST, 2025
PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

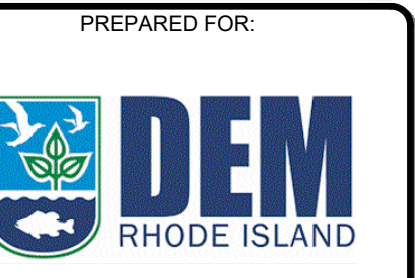
TEMPORARY CONTROLS PLAN

DRAWING
5
SHEET NO. 5 OF 15

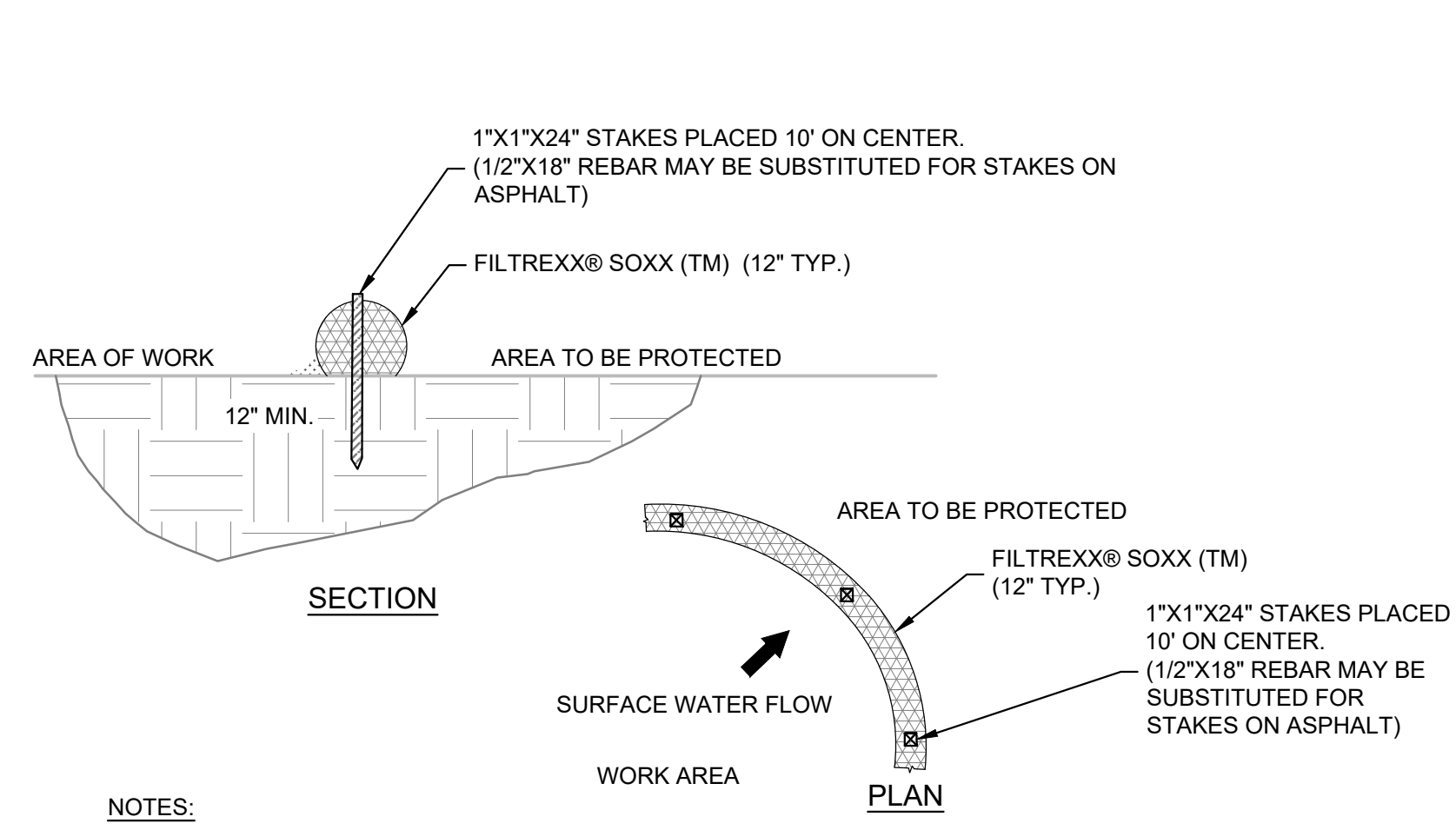
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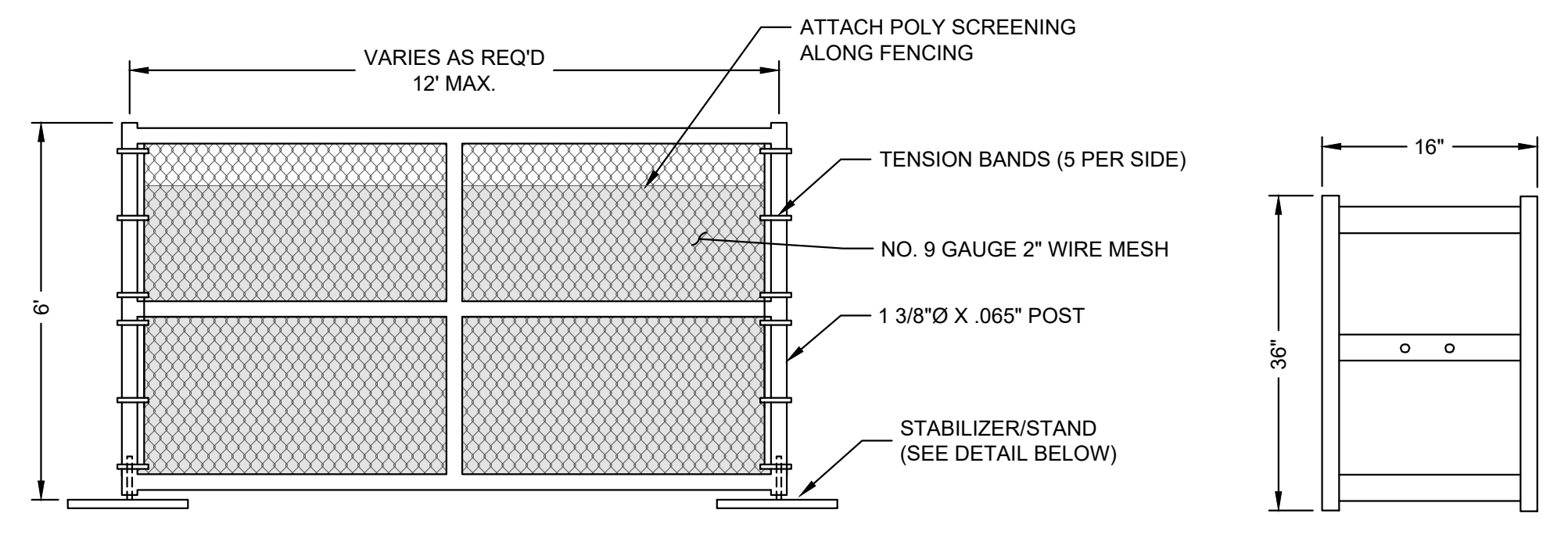


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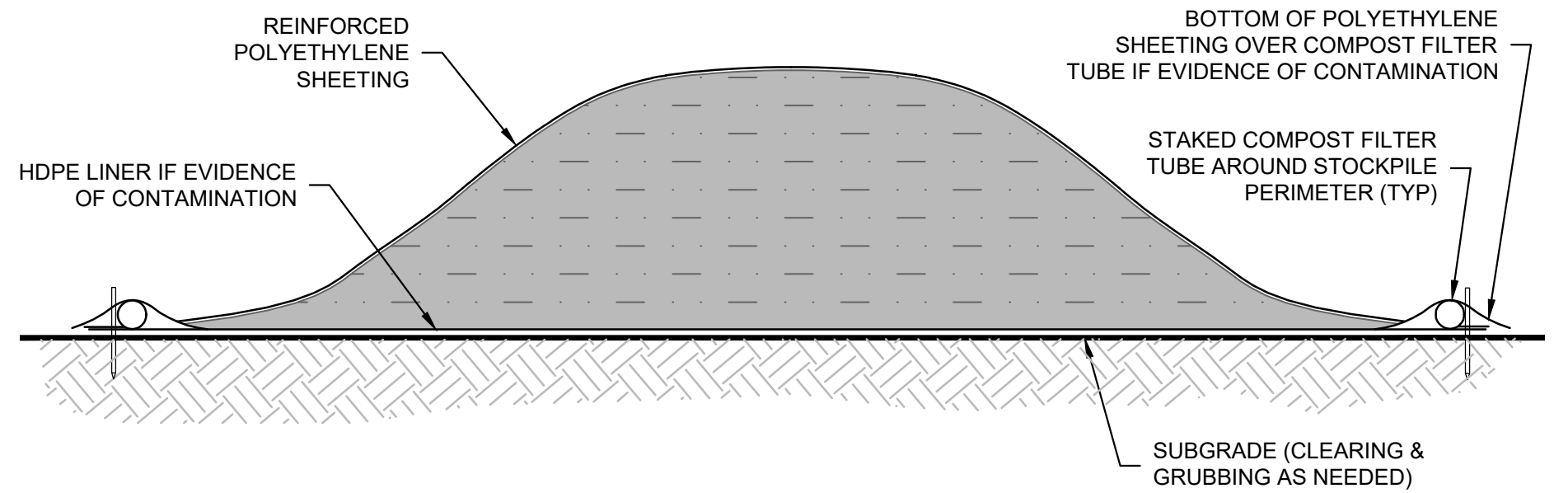
- NOTES:**
- ALL MATERIAL TO MEET FILTREXX® SPECIFICATIONS.
 - STAKES/REBAR PINS SHALL HAVE PROTECTIVE CAPS INSTALLED TO PREVENT FALL INJURY.

1 FILTREXX® SOXX SEDIMENT CONTROL DETAIL
NOT TO SCALE



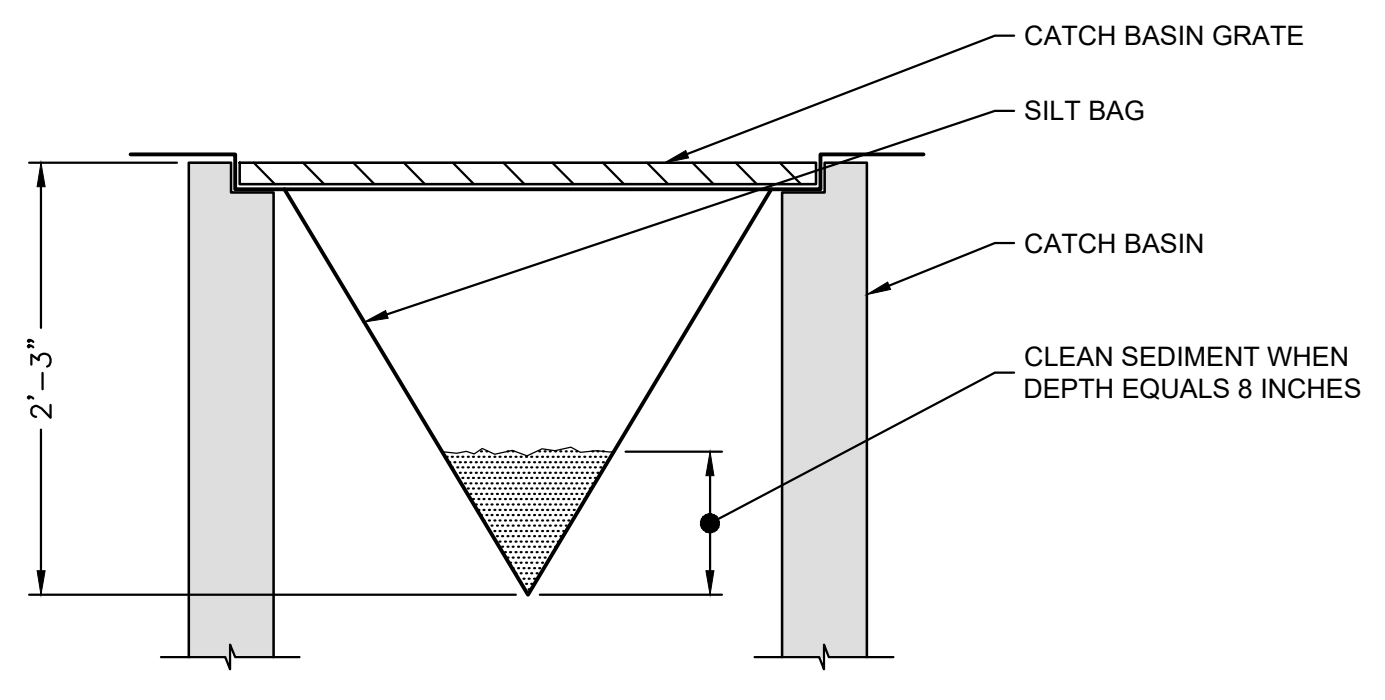
- TEMPORARY CONSTRUCTION FENCING NOTES:**
- FENCES SHALL BE CONSTRUCTED WITH A TOP AND BOTTOM RAIL.
 - GATES MUST REMAIN IN GOOD WORKING ORDER AND MUST BE CLOSED AND SECURED DURING NON-WORKING HOURS.
 - GATES SHALL BE CONSTRUCTED SO THAT THEY SWING IN TOWARDS THE CONSTRUCTION SITE.
 - GATES MUST BE CONSTRUCTED WITH THE SAME DESIGN CHARACTERISTICS AS THE TEMPORARY CONSTRUCTION FENCE.
 - FABRIC SHALL BE ANCHORED IN EACH GROMMET WITH ALUMINUM TIES.

4 TYPICAL TEMPORARY CONSTRUCTION FENCE PANEL
NOT TO SCALE



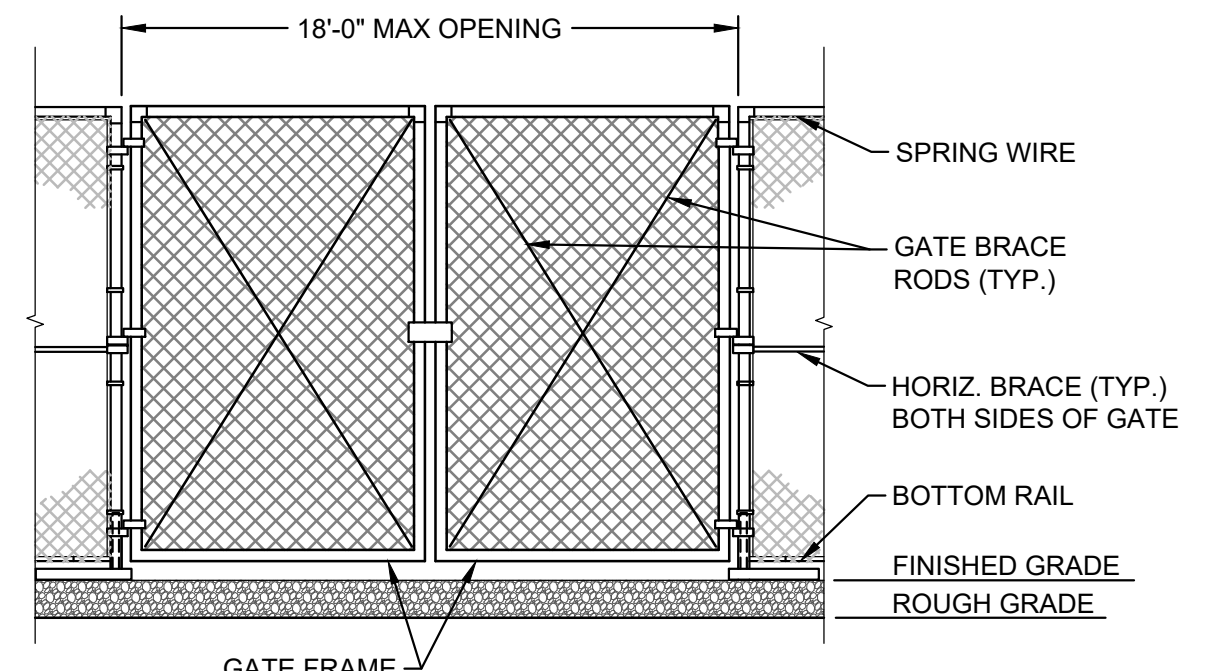
- SOIL STOCKPILE NOTES:**
- SOIL STOCKPILES SHALL BE PLACED ON AND COVERED WITH POLYETHYLENE SHEETING AT ALL TIMES TO PREVENT DUST OR IS SUSPECTED OF CONTAMINATION.
 - SHEETING COVERING STOCKPILE SHALL BE MAINTAINED TIGHTLY IN PLACE BY USING SANDBAGS ON ROPES WITH A MAXIMUM 10'-0\"/>

7 TYPICAL MATERIAL STOCKPILE DETAIL
NOT TO SCALE

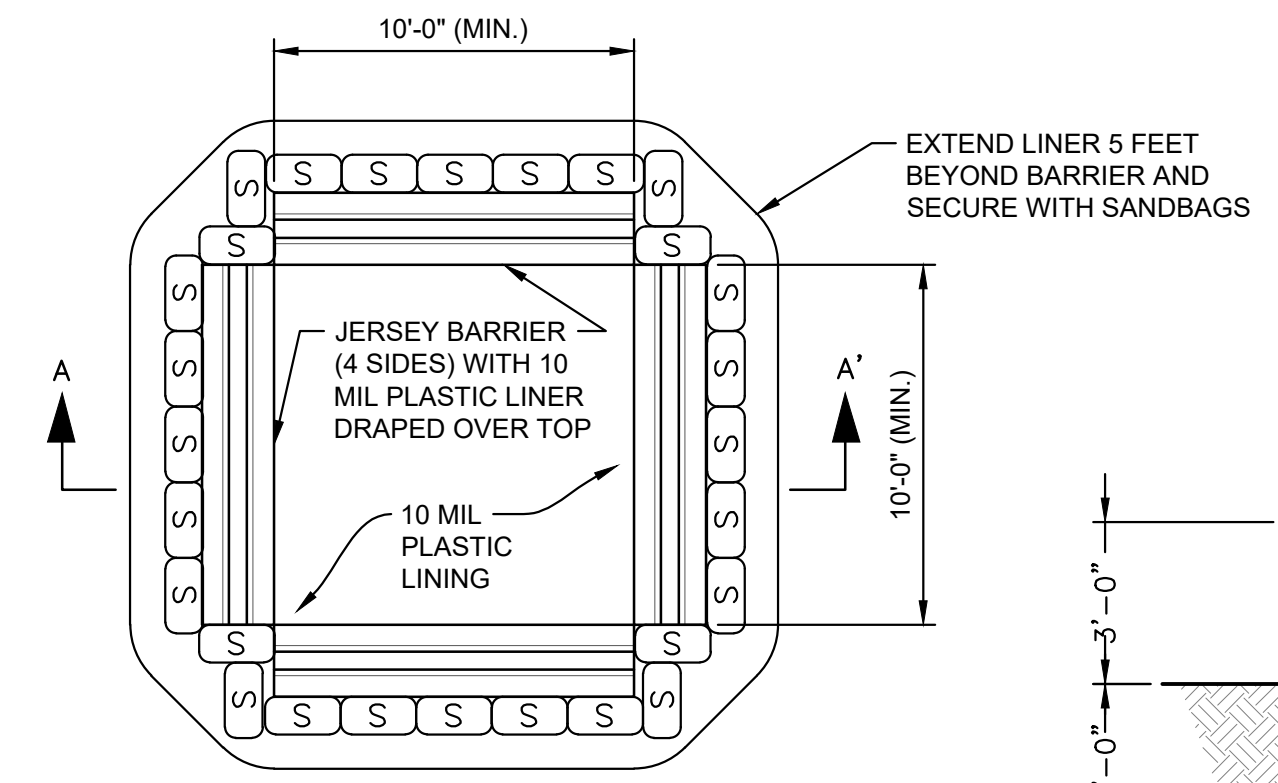


- NOTES:**
- SEDIMENT BAG INLET PROTECTION TO BE SILT SACK MANUFACTURED BY ATLANTIC CONSTRUCTION FABRICS INC. RICHMOND, VA OR APPROVED EQUAL.
 - STORM WATER CATCH BASINS OR DRAINS SHALL BE PROTECTED FROM MATERIALS RUN-OFF. CONTRACTOR SHALL INSTALL SILT SACKS WITHIN EACH CATCH BASIN IN THE VICINITY OF ANY WORK AREAS IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS. CONTRACTOR SHALL USE ALL BMP'S NECESSARY TO PROTECT THESE INLETS FROM SEDIMENT AND DEBRIS.

2 SILT SACK DETAIL
NOT TO SCALE

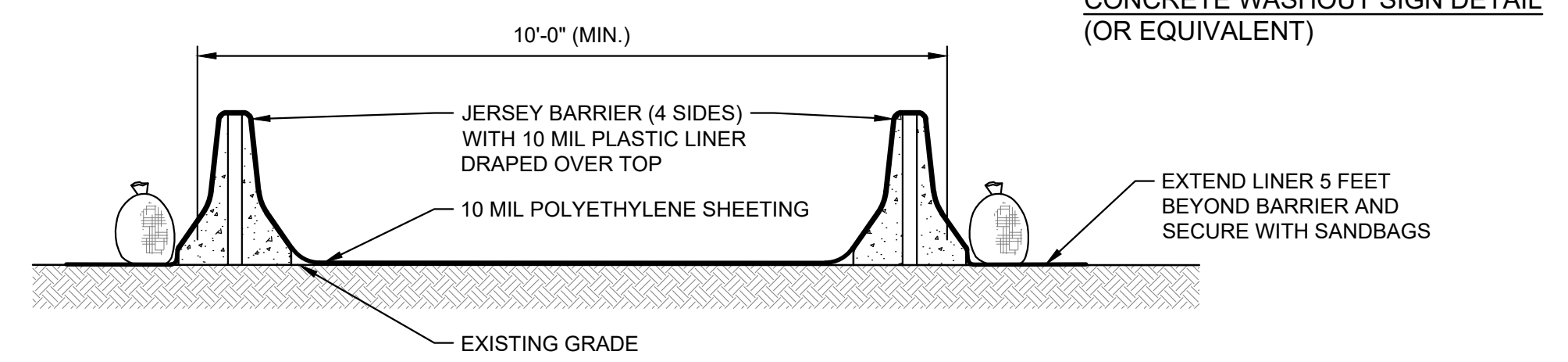


5 TYPICAL TEMPORARY CONSTRUCTION DOUBLE SWING GATE
NOT TO SCALE



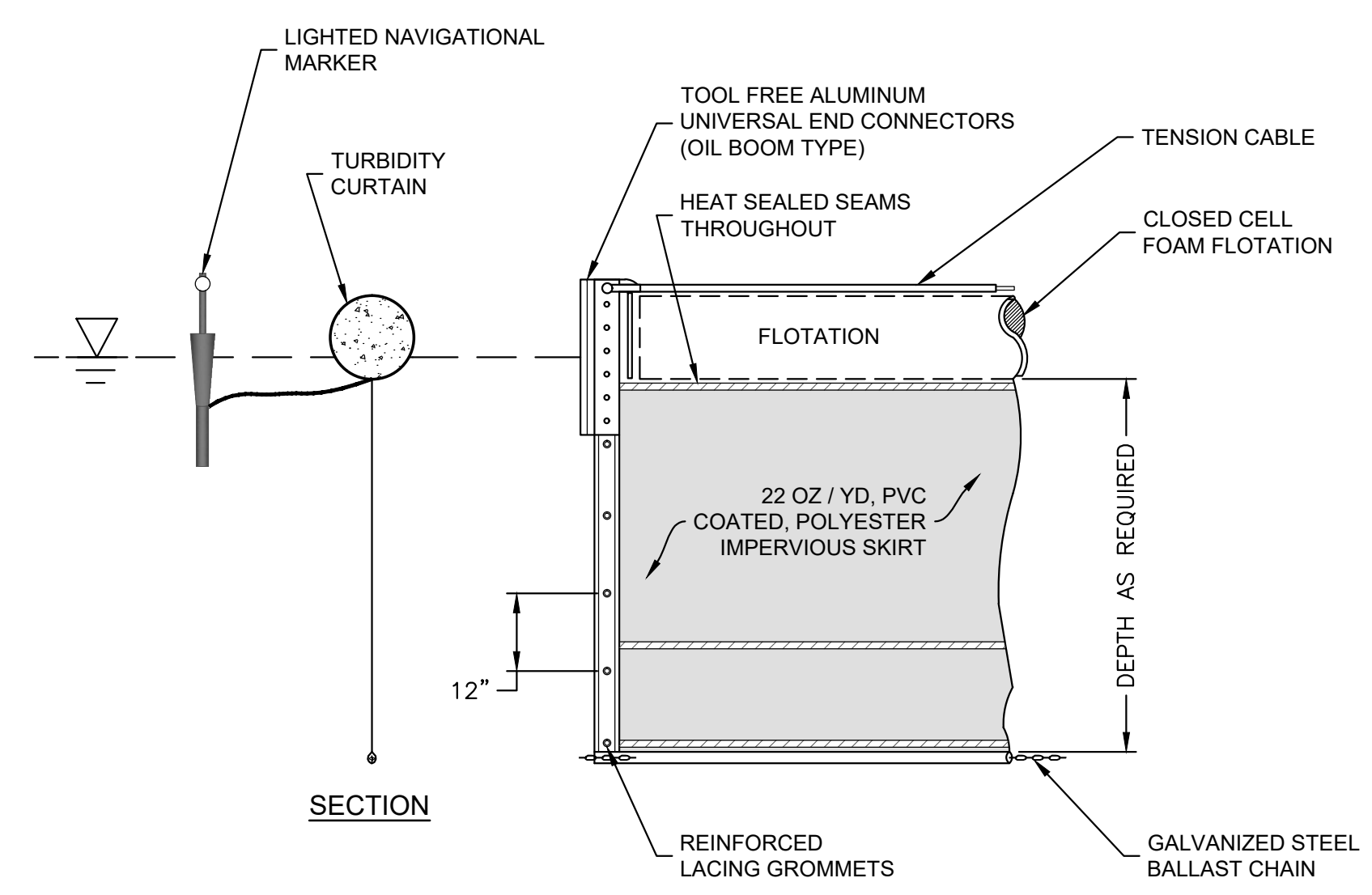
- NOTES:**
- STONE SIZE - USE 2\"/>

6 TEMPORARY CONSTRUCTION ENTRANCE / EXIT
NOT TO SCALE



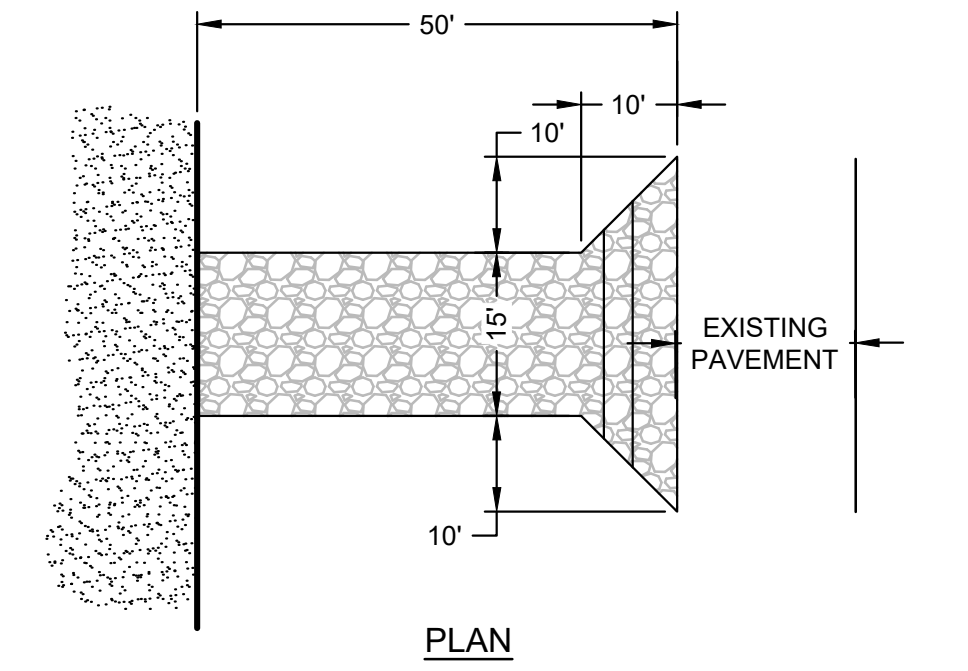
- NOTES:**
- SUMPS TO BE CLEANED AND WASTE CONCRETE AND WATER REMOVED AND PROPERLY DISPOSED OF UPON COMPLETION OF WORK.

8 CONCRETE WASHOUT AREA
NOT TO SCALE



- NOTES:**
- REFER TO SECTION 01 57 00 - TEMPORARY CONTROLS FOR MINIMUM REQUIREMENTS OF TURBIDITY CURTAINS.
 - END OF CURTAIN SHALL BE ANCHORED SECURELY AT THE SHORELINE ABOVE MEAN HIGH WATER ELEVATION IN ACCORDANCE WITH MANUFACTURERS SPECIFICATIONS.
 - TURBIDITY CURTAINS SHALL BE INSPECTED REGULARLY TO DETERMINE IF ALL COMPONENTS ARE FUNCTIONING PROPERLY.

3 TURBIDITY CURTAIN
NOT TO SCALE



- NOTES:**
- STONE SIZE - USE 2\"/>

6 TEMPORARY CONSTRUCTION ENTRANCE / EXIT
NOT TO SCALE

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NOT FOR CONSTRUCTION

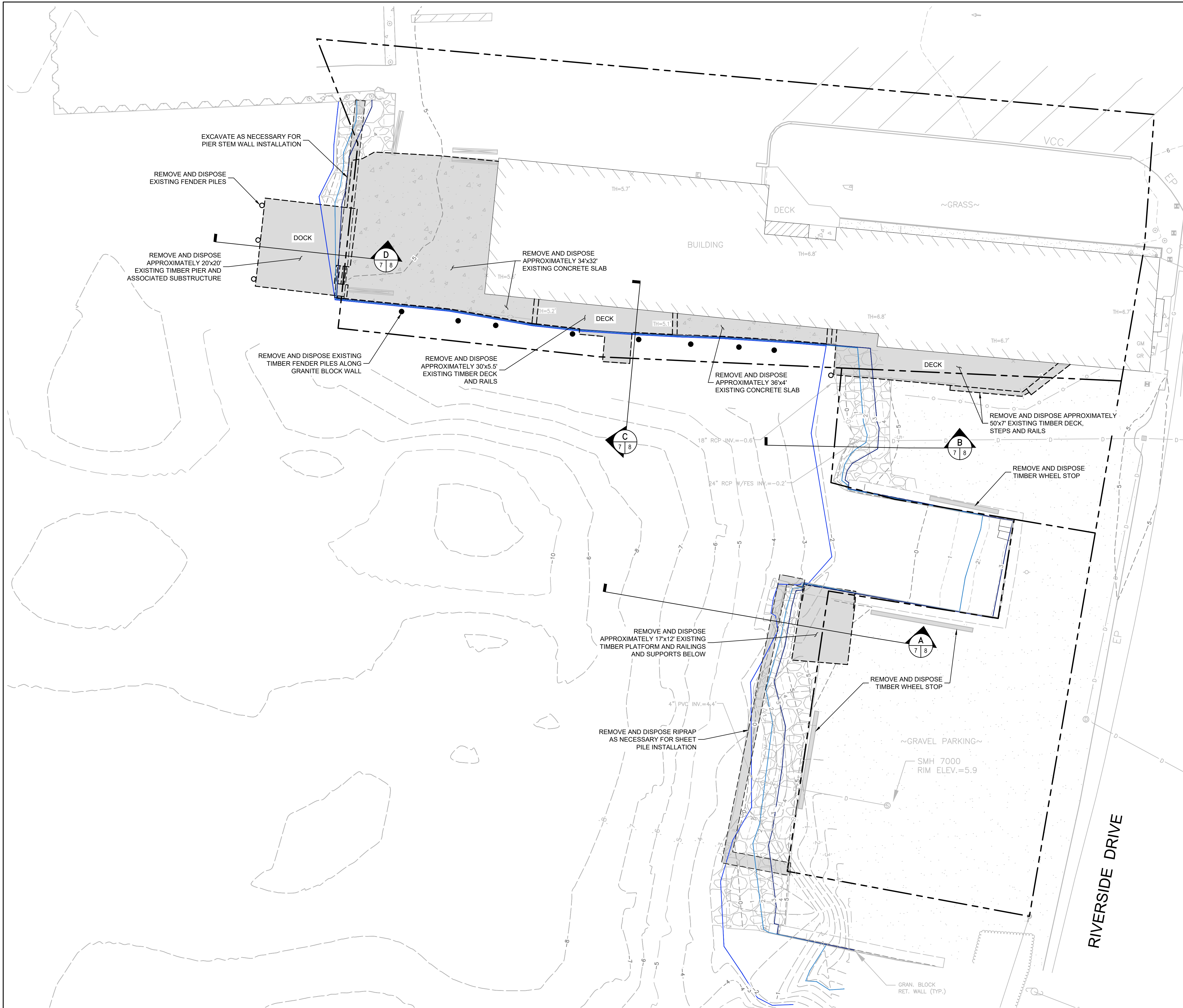
REV.	DESCRIPTION	DATE	BY

PROJECT NO:	35429.00
DATE:	AUGUST, 2025
PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

TEMPORARY CONTROLS DETAILS

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REFERENCE NOTES:

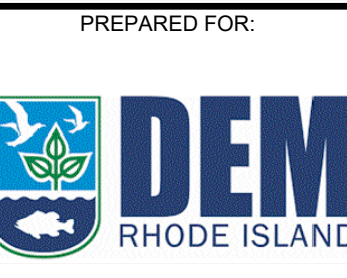
1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DORCET SURVEY SHIPPED TO GZA ON SEPTEMBER 16, 2024.
2. BATHYMETRIC SURVEY OF SITE CREATED BY GZA - AMESBURY.

LEGEND

- APPROX. LOT LINE PER REF. PLAN
- MAJOR CONTOUR LINE
- MINOR CONTOUR LINE
- RETAINING WALL
- CHAIN LINK FENCE
- HANDRAIL
- GUARDRAIL
- OVERHEAD WIRE
- DRAIN LINE
- GAS LINE
- SHRUB LINE
- CONCRETE
- RIP RAP
- CRUSHED STONE
- SPOT GRADE
- PIPE/ROD FOUND
- UTILITY POLE
- UTILITY POLE & GUY WIRE
- UTILITY POLE STUB
- DRAIN MANHOLE
- CATCH BASIN
- FLARED END SECTION
- SEWER MANHOLE
- CLEANOUT
- GAS METER
- GAS REGULATOR VALVE
- ELECTRIC MANHOLE
- MANHOLE
- SIGN
- SQUARE POST
- POST
- WOODEN PILING & TOP ELEVATION
- ACCESSIBLE PARKING SPACE
- FLOW DIRECTION
- JERSEY BARRIER
- CONCRETE
- EDGE OF PAVEMENT
- GRANITE
- POLYVINYL CHLORIDE PIPE
- REINFORCED CONCRETE PIPE
- RETAINING WALL
- SINGLE WHITE LINE
- SINGLE YELLOW LINE
- THRESHOLD ELEVATION
- TYPICAL
- VCC
- INVERT I.D. CONNECTION UNKNOWN
- REMOVE AND DISPOSE EXISTING GUIDE PILES AND FENDER PILES
- REMOVE AND DISPOSE EXISTING TIMBER DECK AND PLATFORMS / CONCRETE DECK



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PROJECT NO: 35429.00

DATE: AUGUST, 2025

PROJECT MGR: TWS

DESIGNED BY: JTF

DRAWN BY: GRB

CHECKED BY: TWS

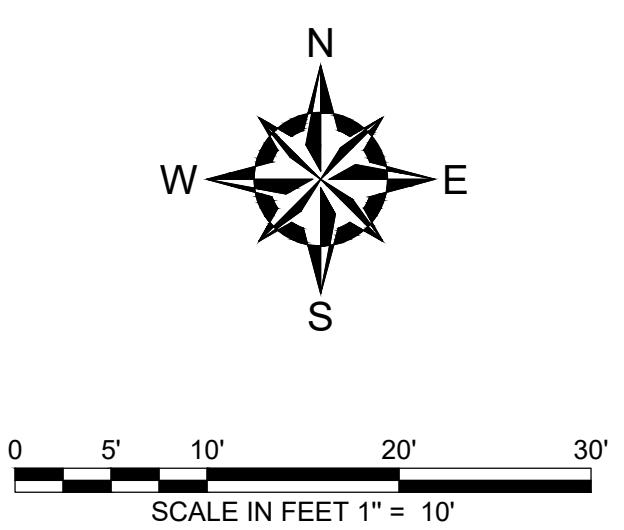
REVIEWED BY: MJP

SCALE: AS NOTED

REVISION NO.

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

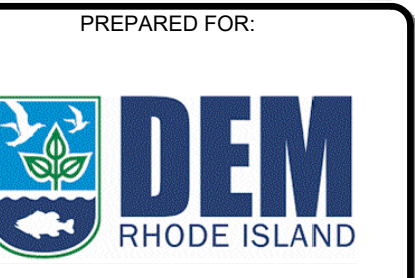
DEMOLITION PLAN



DRAWING
7
SHEET NO. 7 OF 15



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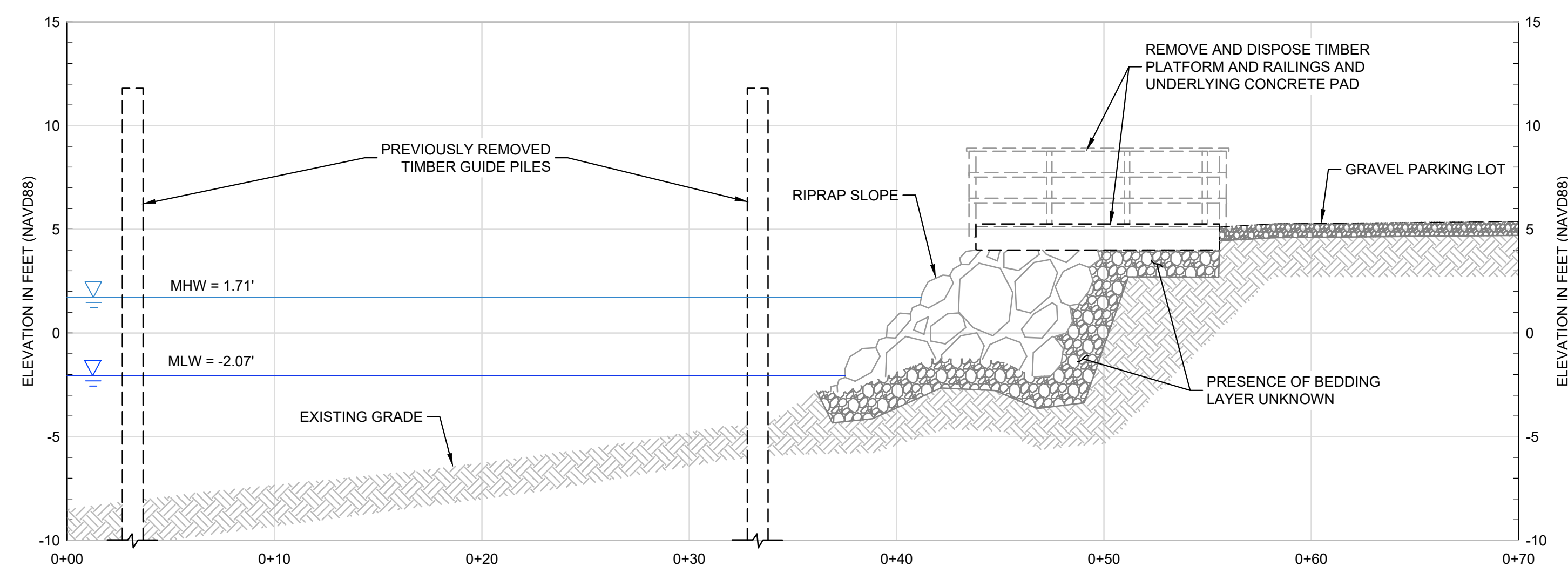
REV. DESCRIPTION DATE BY

PROJECT NO:	35429.00
DATE:	AUGUST, 2025
PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

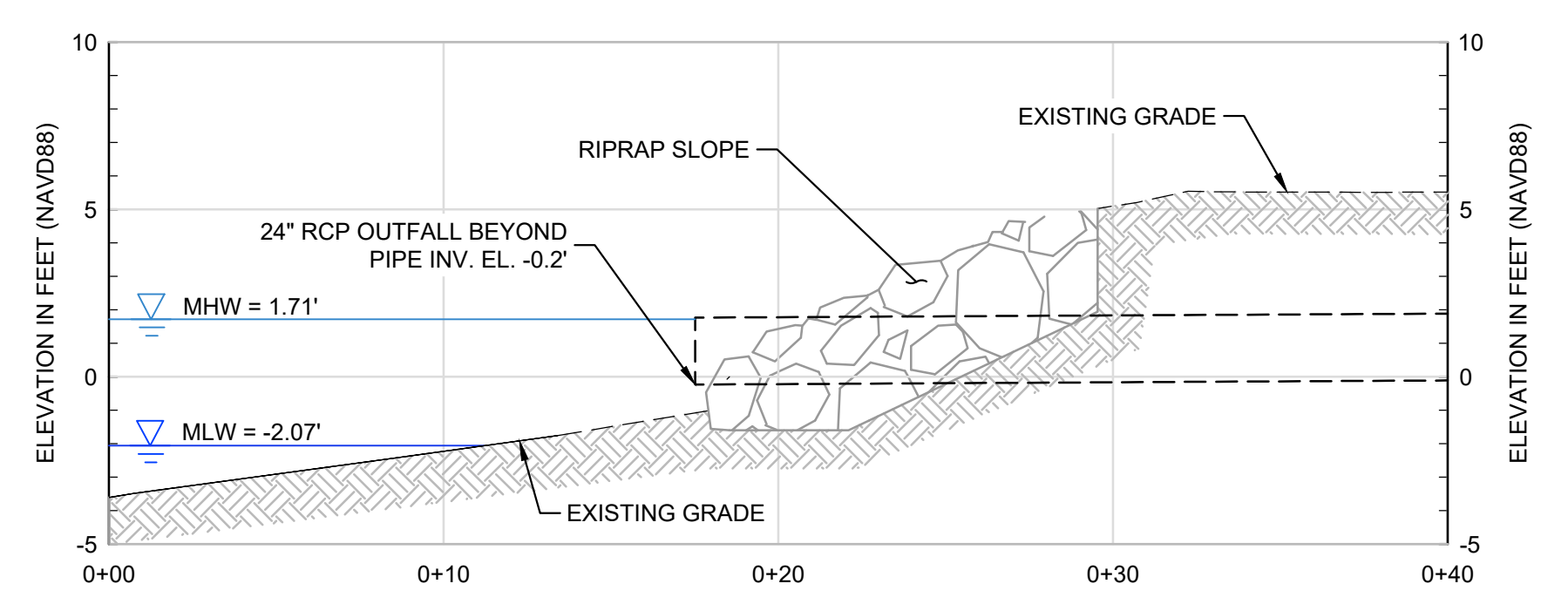
CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

DEMOLITION SECTIONS

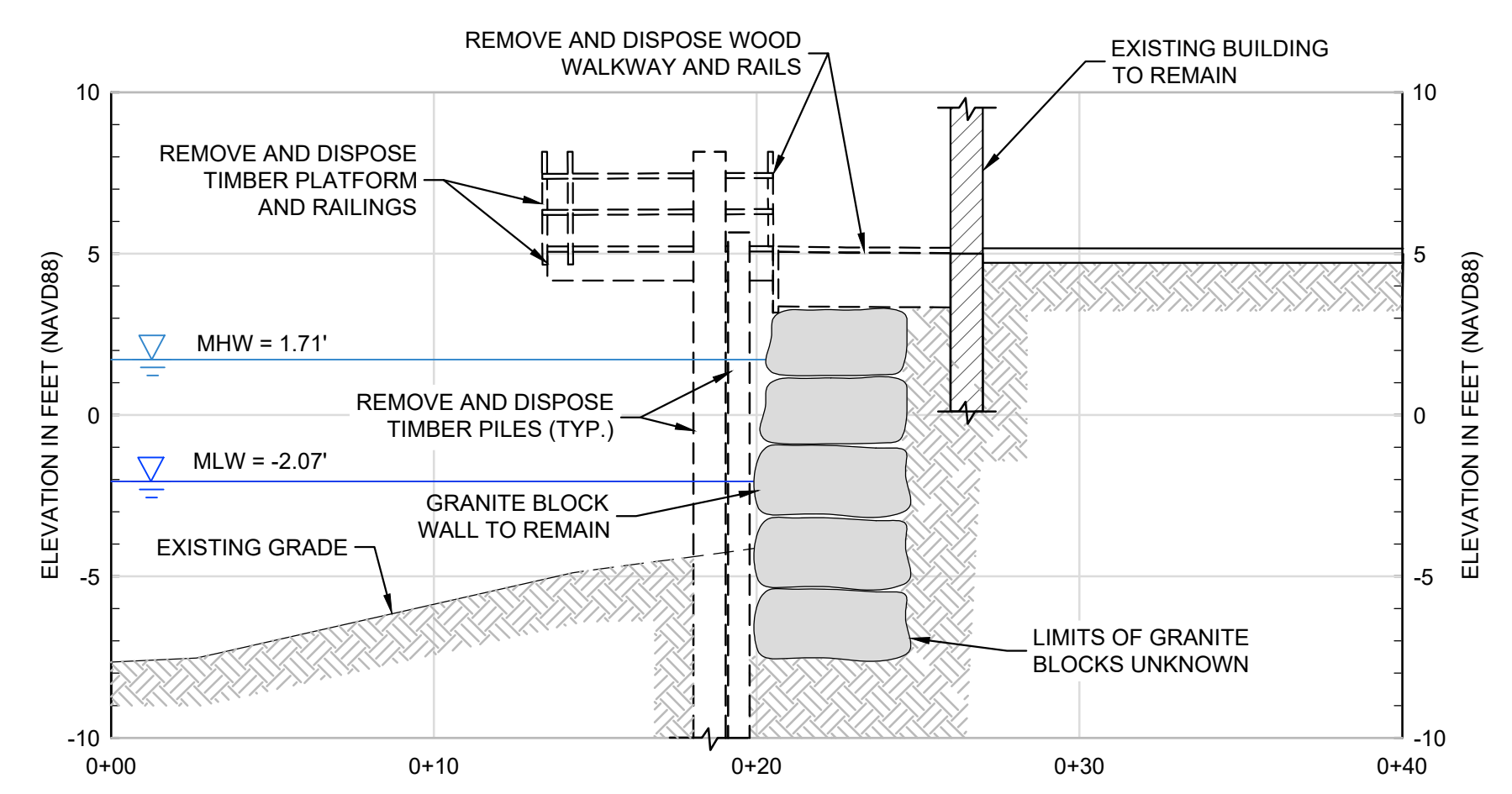
DRAWING
8
SHEET NO. 8 OF 15



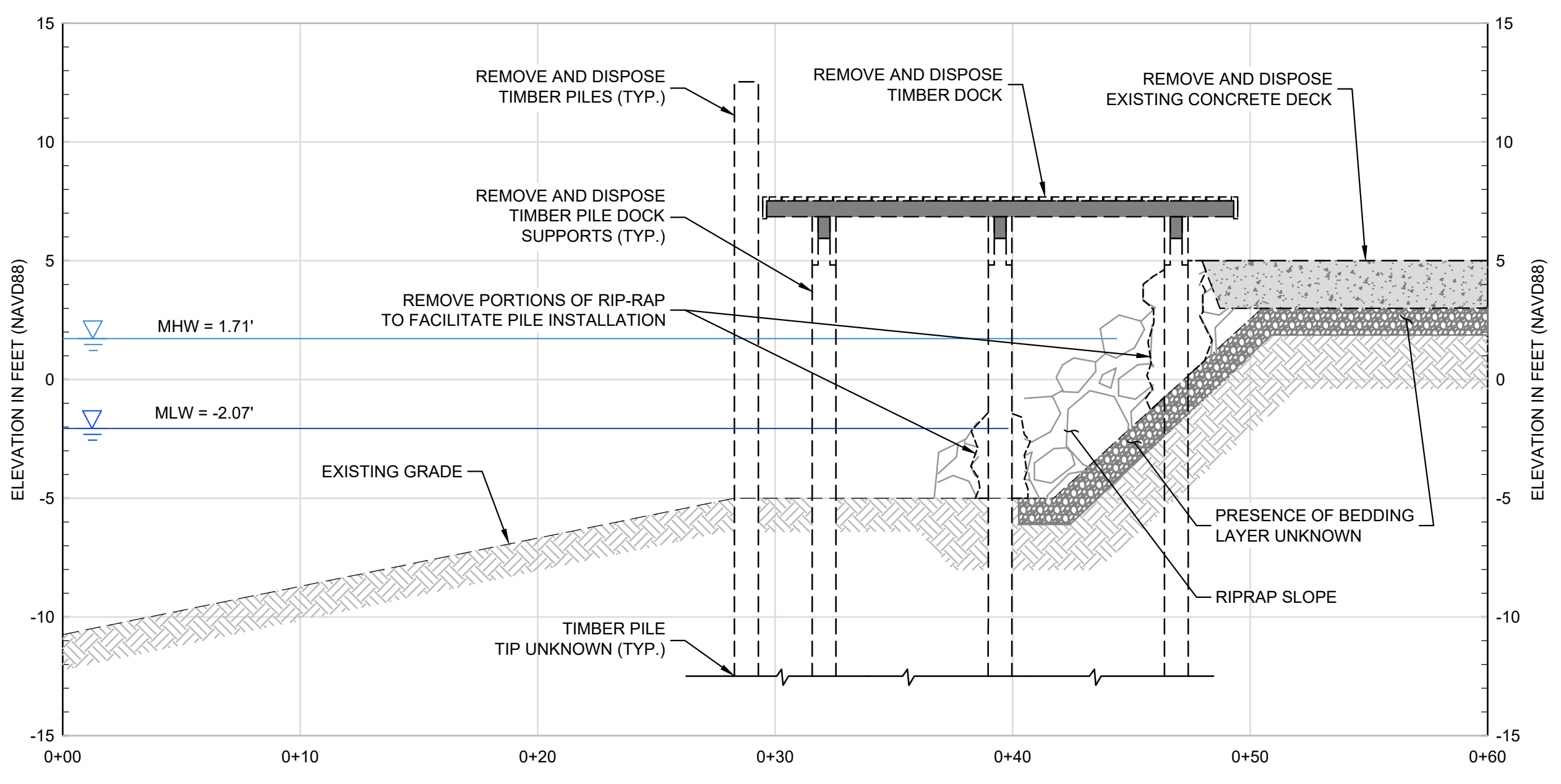
A RIPRAP SLOPE / PARKING LOT - SECTION
SCALE: 1"=5'



B RIPRAP SLOPE / PARKING LOT - SECTION
SCALE: 1"=5'



C GRANITE BLOCK WALL - SECTION
SCALE: 1"=5'



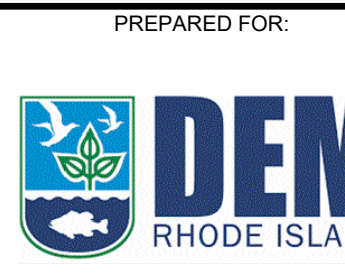
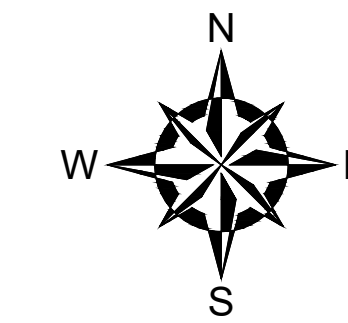
D DOCK PLATFORM - SECTION
SCALE: 1"=5'

NOTES:

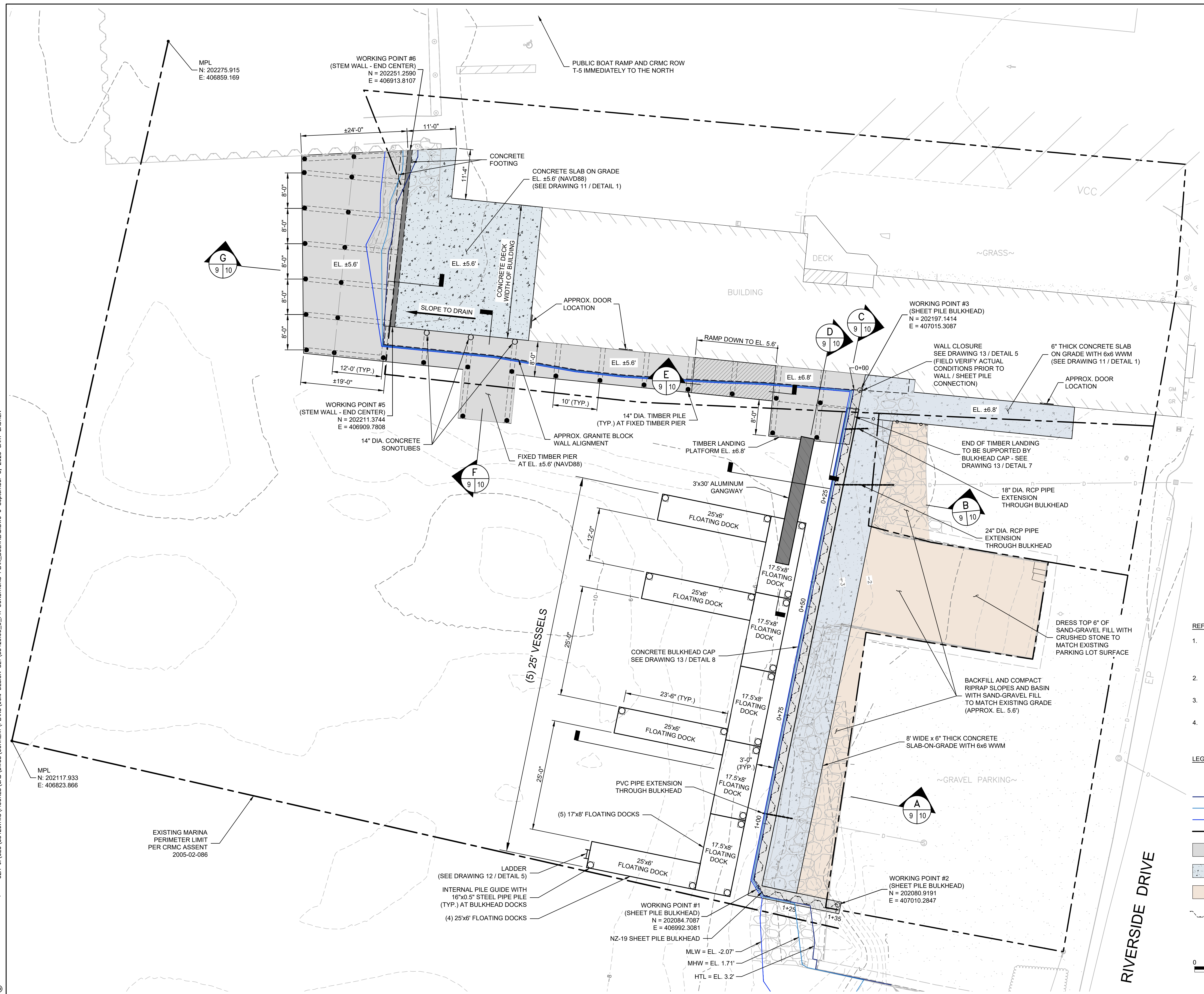
- LIMITS AND EXTENT OF BELOW GRADE RIPRAP, BEDDING LAYERS, BUILDING FOUNDATIONS, AND STONE WALLS ARE UNKNOWN.



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REFERENCE NOTES:

1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DORCET SURVEY TRANSMITTED TO GZA ON SEPTEMBER 16, 2024.
2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024.
3. PROVIDE CONSTRUCTION JOINTS AT 20-FOOT O.C. MINIMUM.
4. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING AND ESTABLISHING NEW SITE CONTROL AND WORKING POINTS IF NECESSARY.

LEGEND:

- STEEL GUIDE PILE
- TIMBER DECK SUPPORT PILE
- HIGH TIDE LINE
- MEAN HIGH WATER
- MEAN LOW WATER
- PROPERTY LINE
- TIMBER DECK
- CONCRETE DECK
- SAND - GRAVEL FILL
- STEEL SHEETPILE BULKHEAD



90% DESIGN DRAWINGS
NOT FOR CONSTRUCTION

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PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
 169 RIVERSIDE DRIVE
 TIVERTON, RHODE ISLAND
**PROPOSED CONDITIONS PLAN
 PHASE 1**

DRAWING
9
 SHEET NO. 9 OF 15

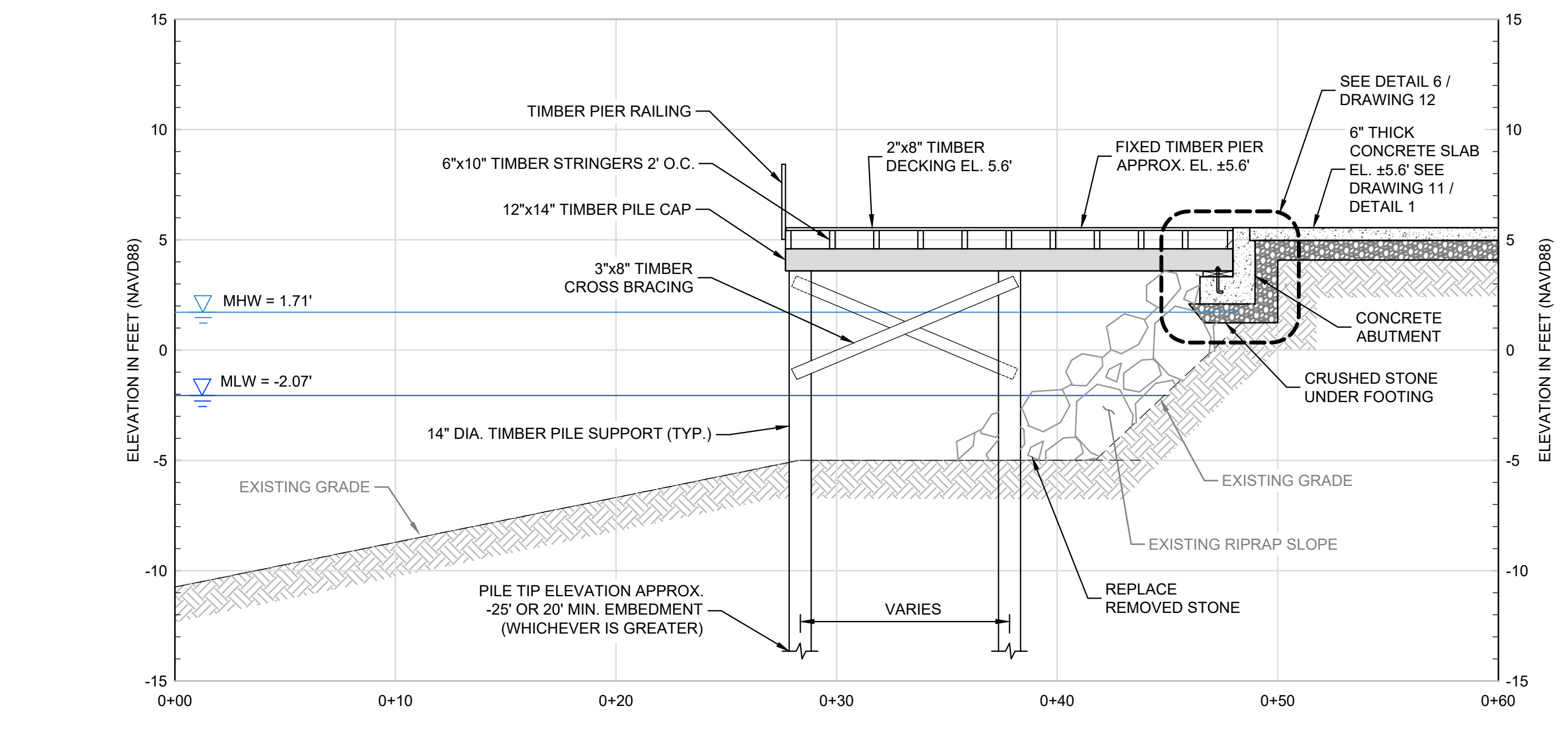
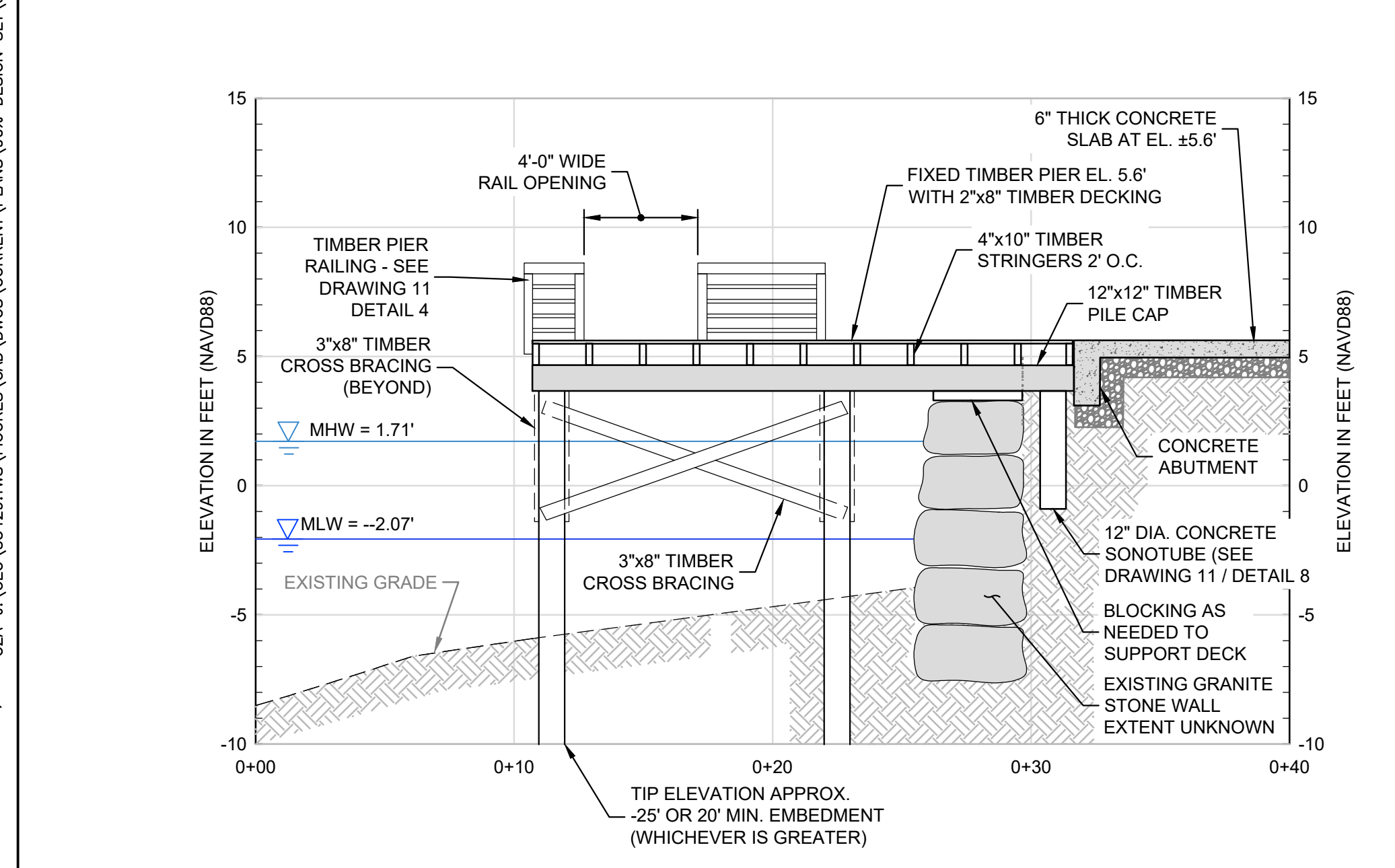
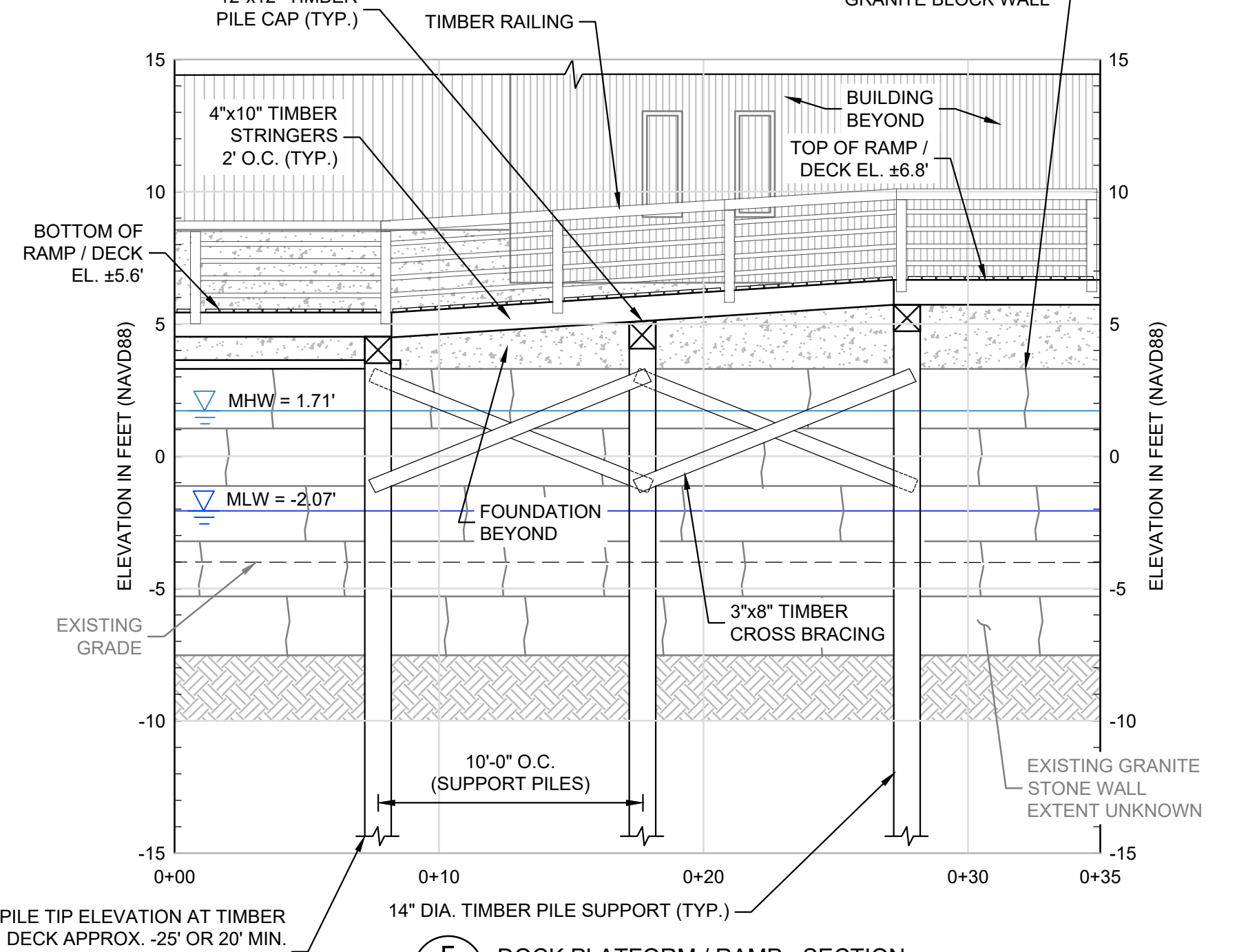
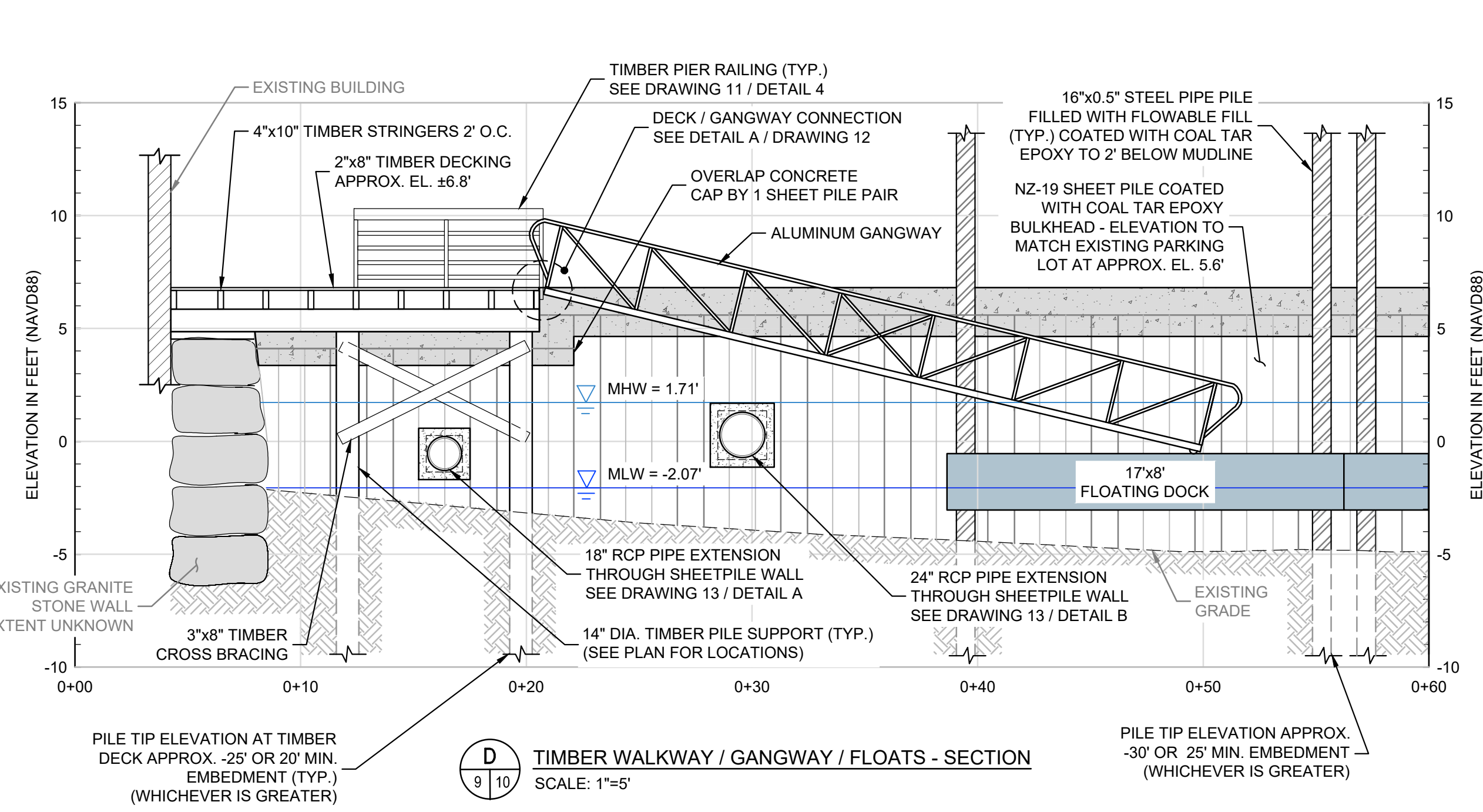
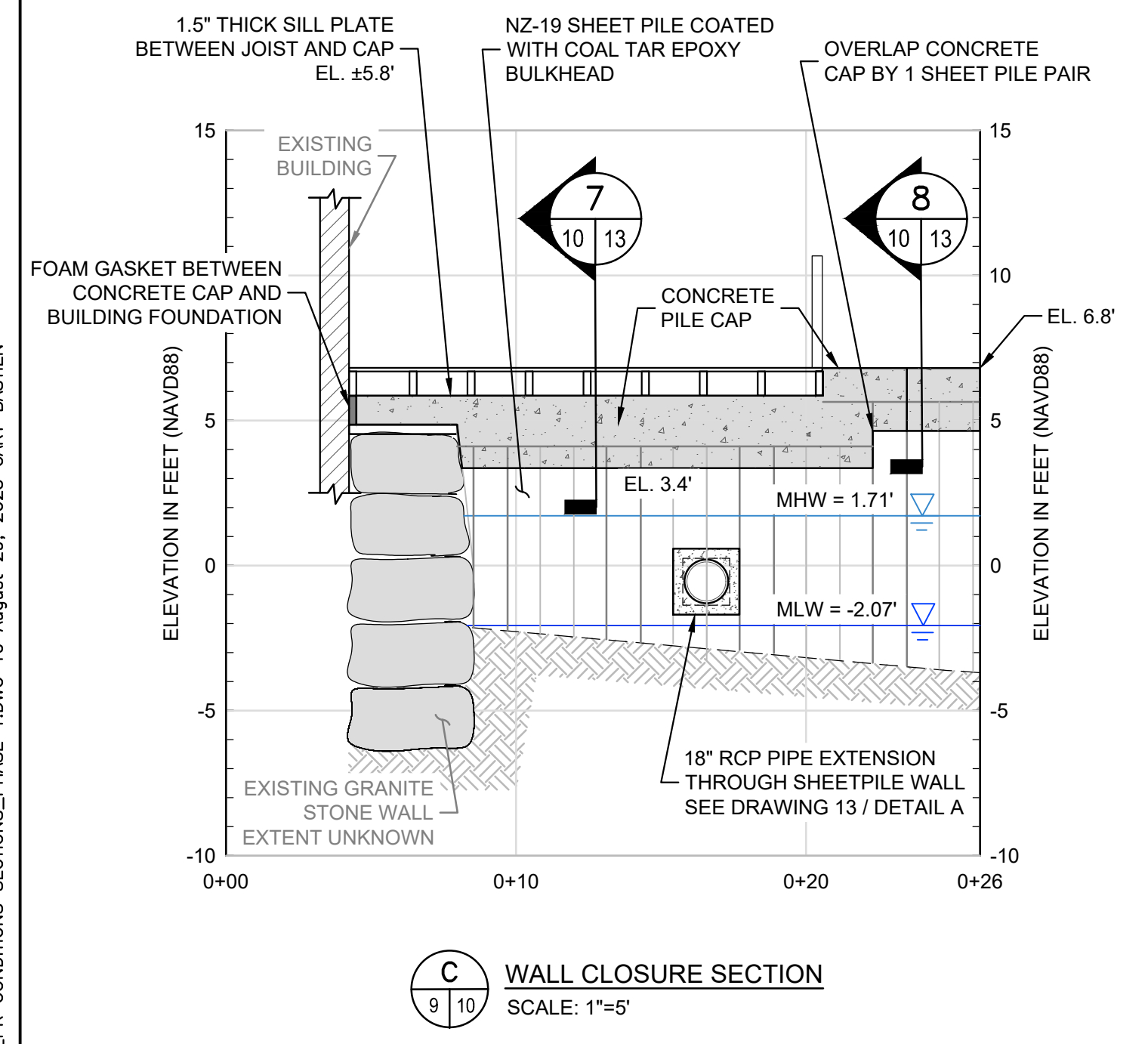
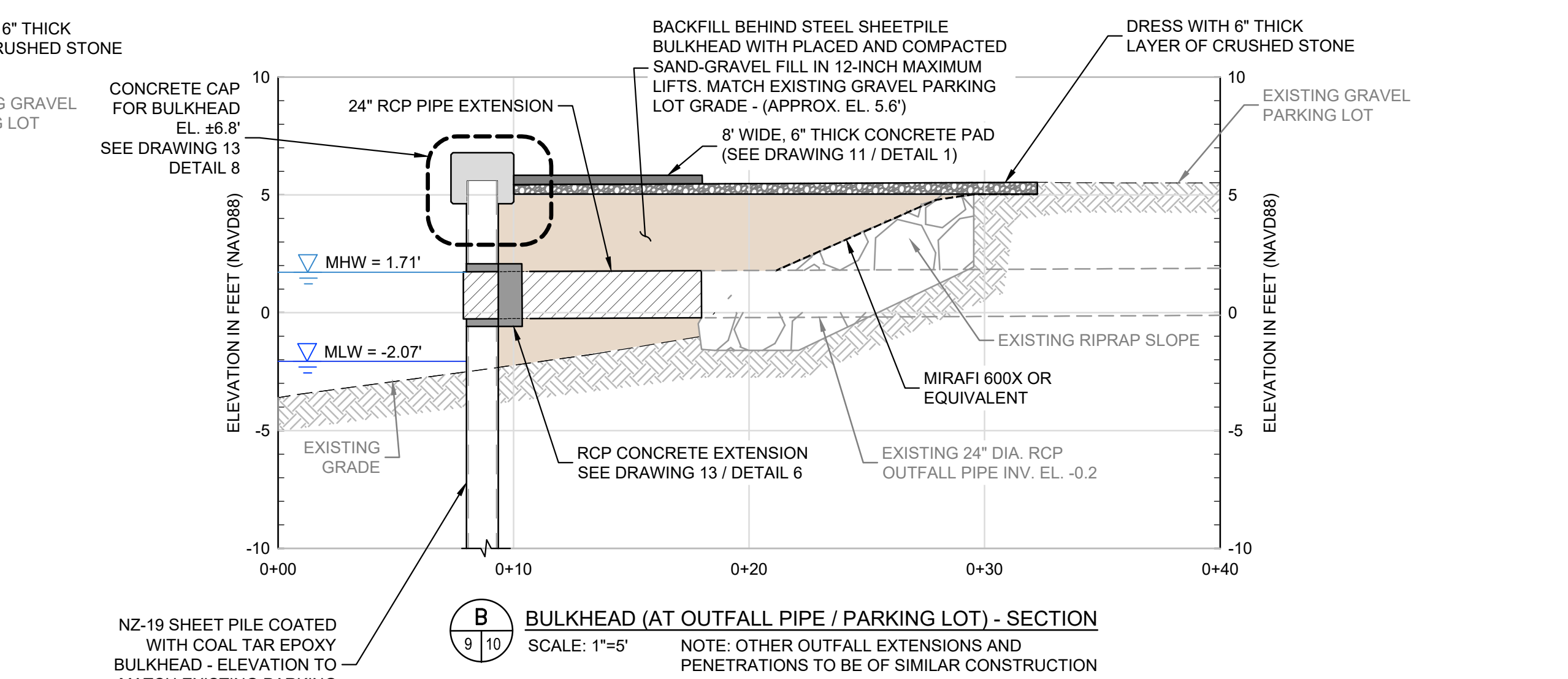
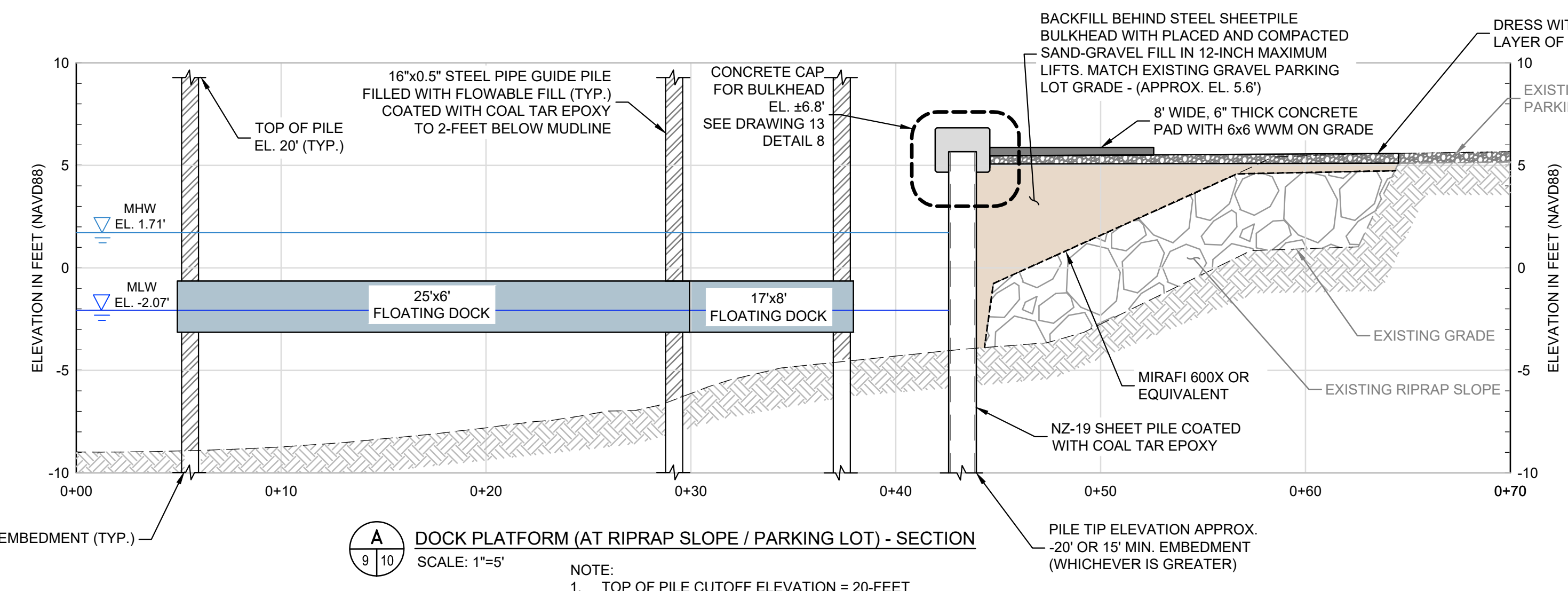


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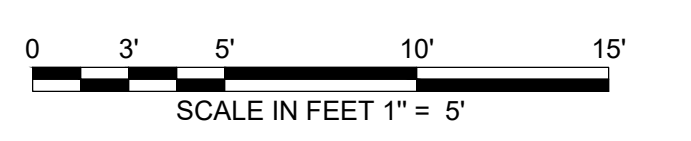
PREPARED FOR:



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- NOTES:
- ALL PILE SUPPORTED CROSS BRACING TO BE 3x8" TIMBER AND ATTACHED TO PILES WITH 1" DIA. HDG. THRU BOLTS.
 - TOP OF BRACING THRU BOLTS EL. 3.0' (NAVD88)
 - BOTTOM OF BRACING THRU BOLTS EL. -0.9' (NAVD88)
 - NZ-19 SHEET PILES WILL BE COATED WITH COAL TAR EPOXY.
 - ALL STEEL PIPE GUIDE PILES TO BE CAPPED WITH A PVC PILE CAP.



90% DESIGN DRAWINGS NOT FOR CONSTRUCTION

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 DATE: AUGUST, 2025
 PROJECT MGR: TWS
 DESIGNED BY: JTF
 DRAWN BY: GRB
 CHECKED BY: TWS
 REVIEWED BY: MJP
 SCALE: AS NOTED
 REVISION NO.

CHASE MARINA
 169 RIVERSIDE DRIVE
 TIVERTON, RHODE ISLAND

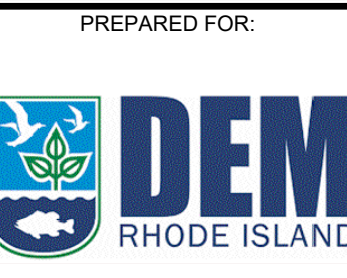
PROPOSED CONDITIONS SECTIONS
 PHASE 1

DRAWING
 10
 SHEET NO. 10 OF 15

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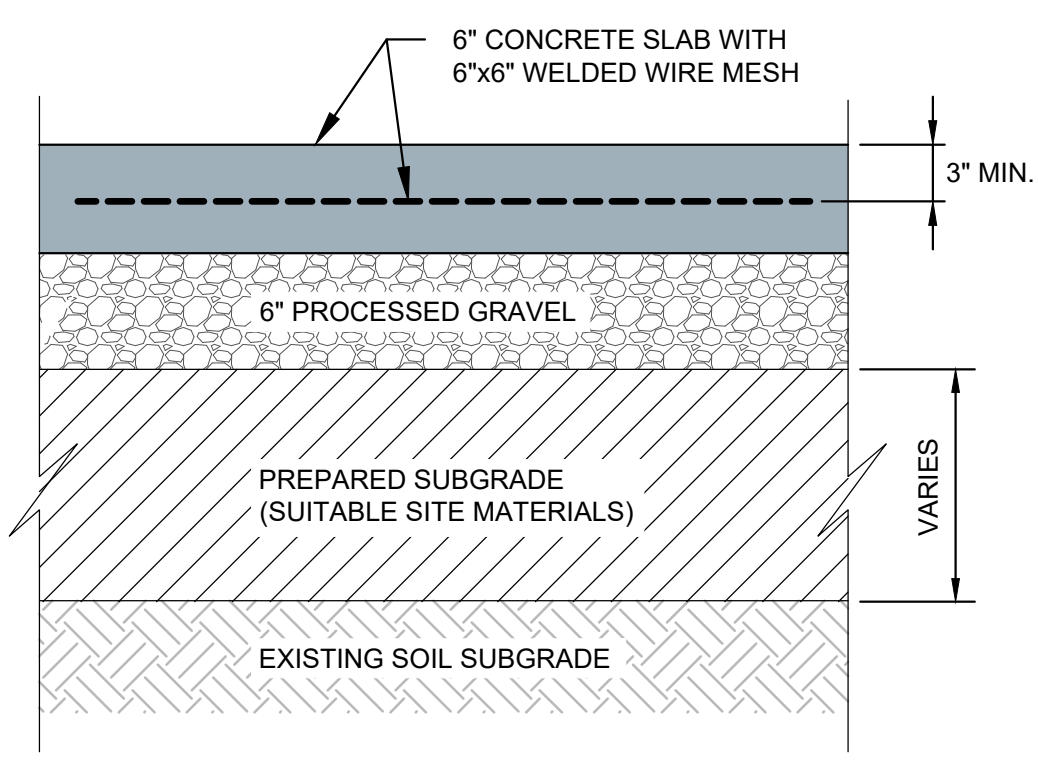
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CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

DRAWING
11
SHEET NO. 11 OF 15

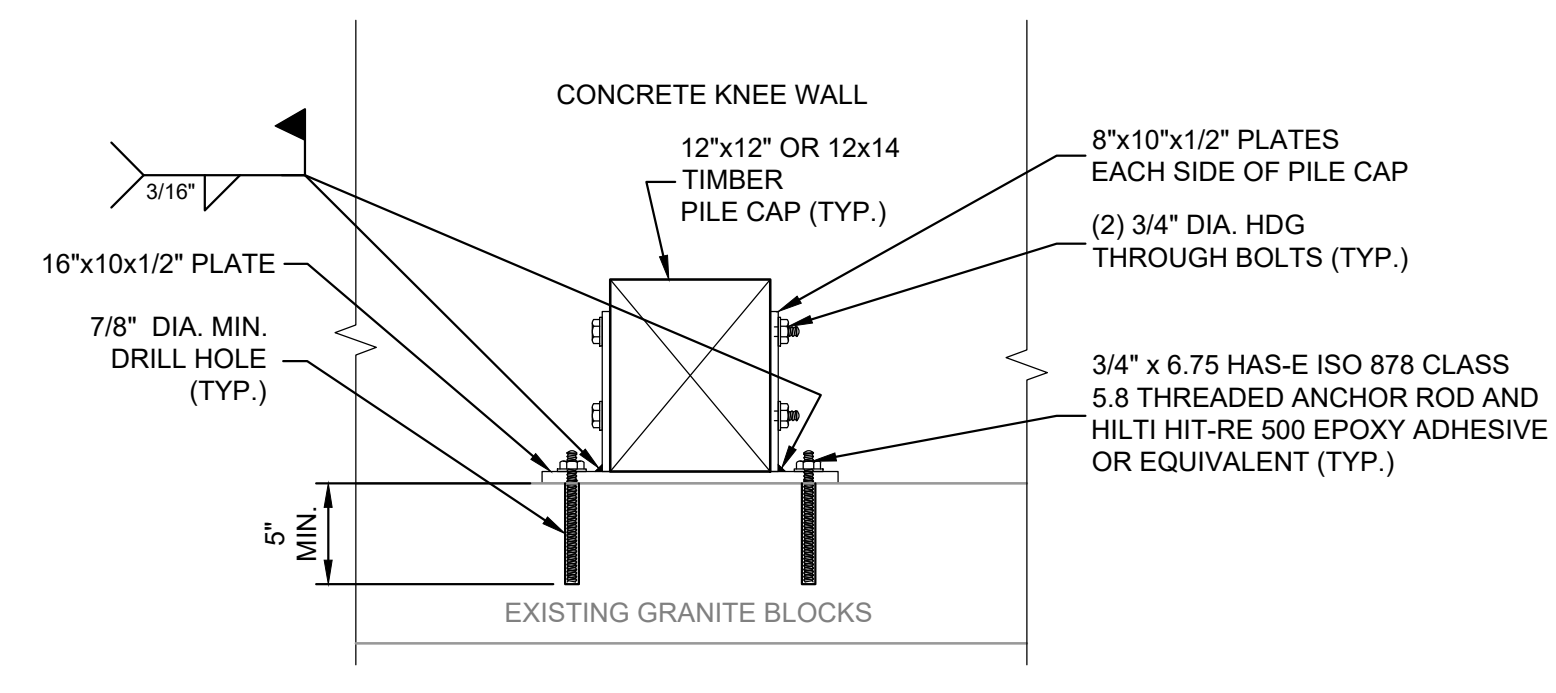
CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

PROPOSED TIMBER DETAILS
PHASE 1

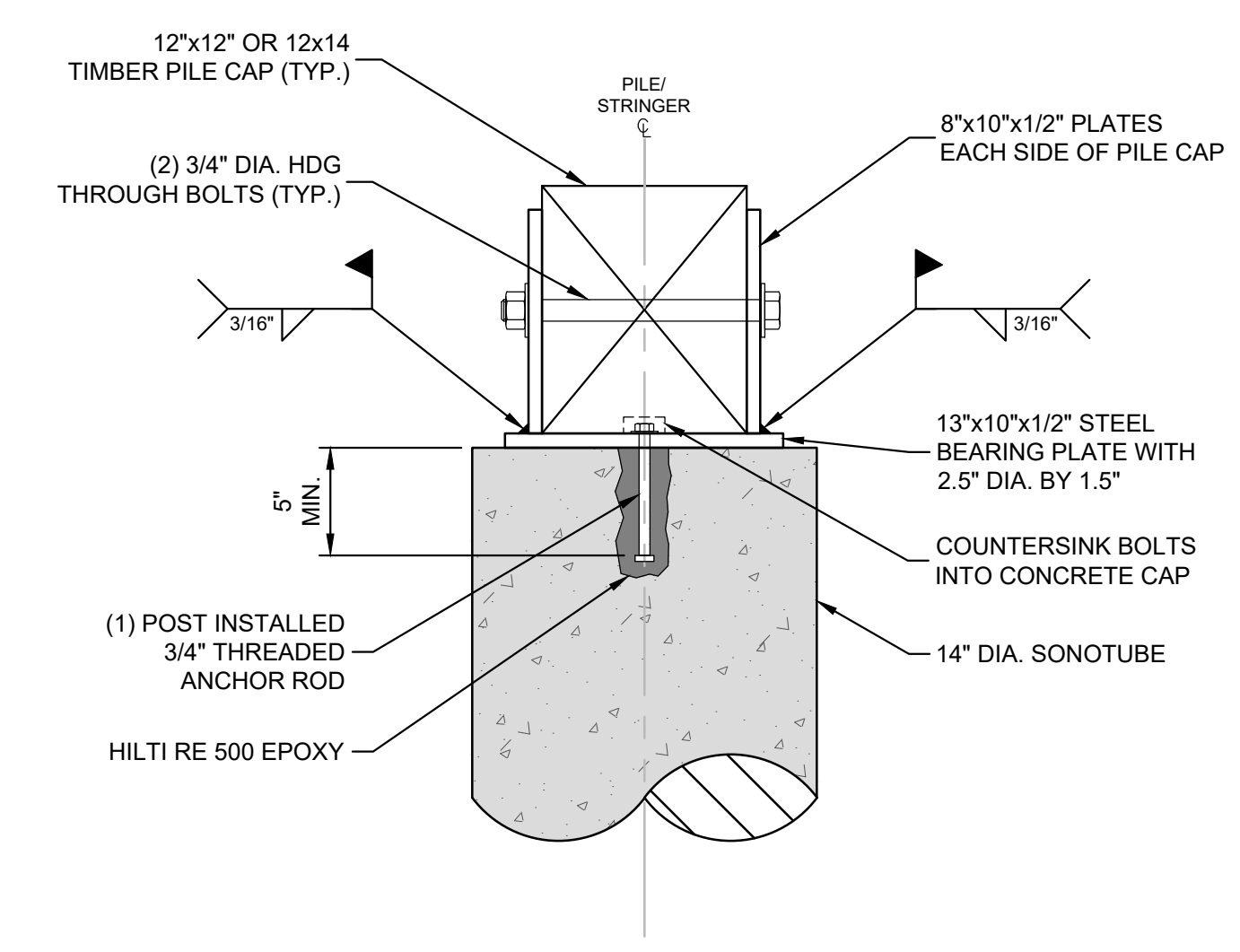
- NOTES:
- TIMBER MEMBERS LOCATED BELOW THE DECKING SHALL BE SOUTHERN YELLOW PINE (SYP) GRADE NO. 2, TREATED WITH 2.5# CCA, UNLESS NOTED OTHERWISE.
 - TIMBER PILE CAPS IN THE ALIGNED IN THE EAST-WEST DIRECTION SHALL BE SYP GRADE NO. 1, TREATED WITH 2.5# CCA.
 - TIMBER MEMBERS LOCATED ON OR ABOVE THE PIER DECKING SHALL BE SYP GRADE NO. 2, TREATED WITH 0.6# ACQ, UNLESS OTHERWISE NOTED.
 - ALL BOLTS, PLATES, AND MISCELLANEOUS METALS IN CONTACT WITH TIMBER TREATED WITH ACQ SHALL BE TYPE 316 STAINLESS STEEL OR HAVE PVC SLEEVES & PVC FLAT STOCK TO PREVENT HDG STEEL CONTACT WITH ACQ-PRESERVED TIMBER.
 - TIMBER DECKING SHALL BE FASTENED TO THE STRINGERS WITH #8 - 2 1/2" LONG, WOOD SCREWS. MINIMUM 2 SCREWS EACH LOCATION PER STRINGER PER PLANK.
 - ALL PILE SUPPORTED CROSS BRACING TO BE 3x8 AND ATTACHED TO PILES WITH 1" DIA. HDG. THRU BOLTS.
 - CONTRACTOR SHALL PLACE 15# ASPHALT ROOFING FELT (TAR PAPER) BETWEEN HORIZONTAL TIMBER TO TIMBER OR CONCRETE INTERFACES AND ALL TIMBER TO STEEL INTERFACES.
 - EPOXY ADHESIVE FOR ANCHORS DRILLED AND SET IN CONCRETE SHALL BE HILTI HIT-RE 500 V3, OR APPROVED EQUIVALENT.



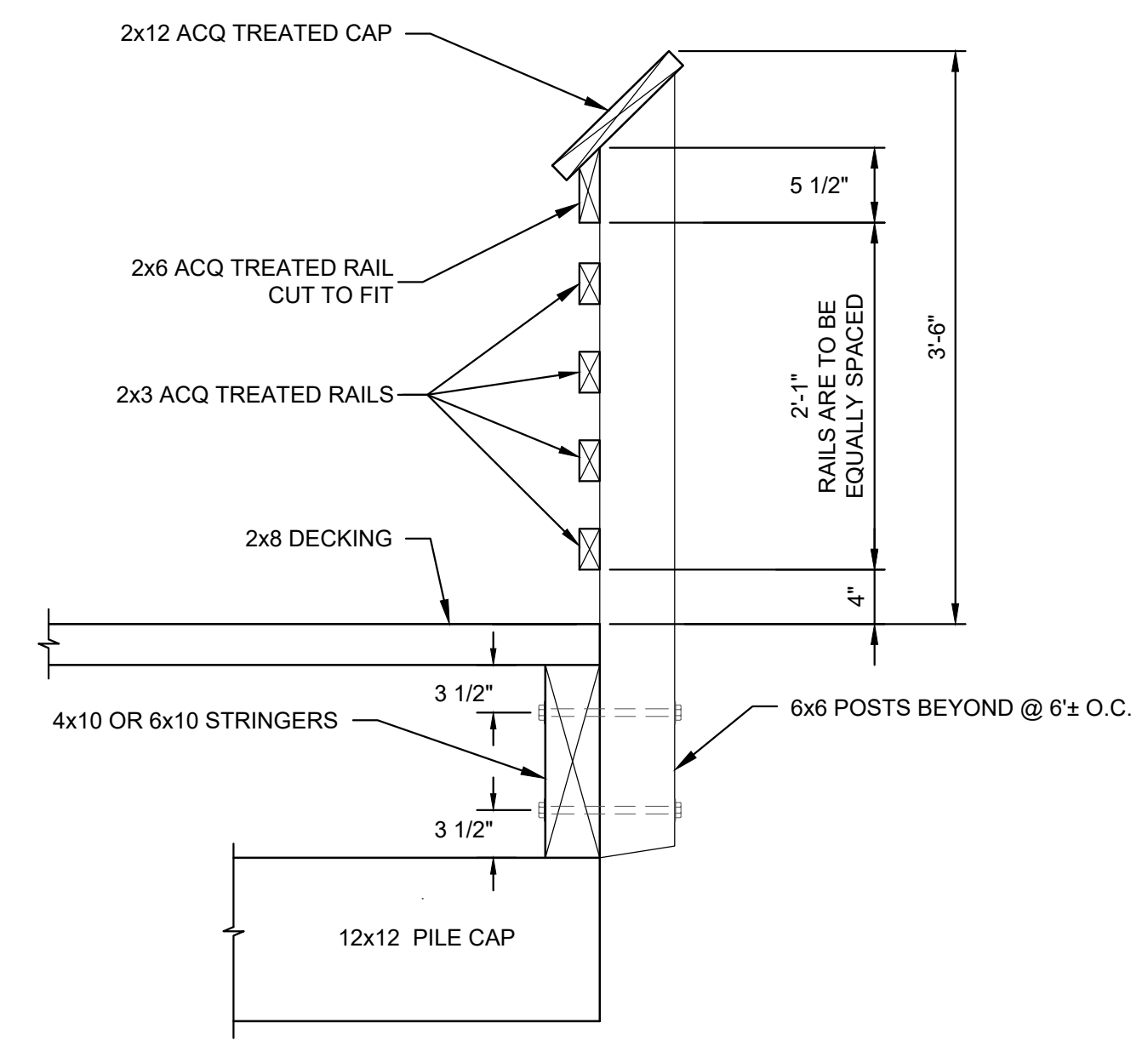
1 CONCRETE SLAB-ON-GRADE DETAIL
SCALE: 1" = 4"



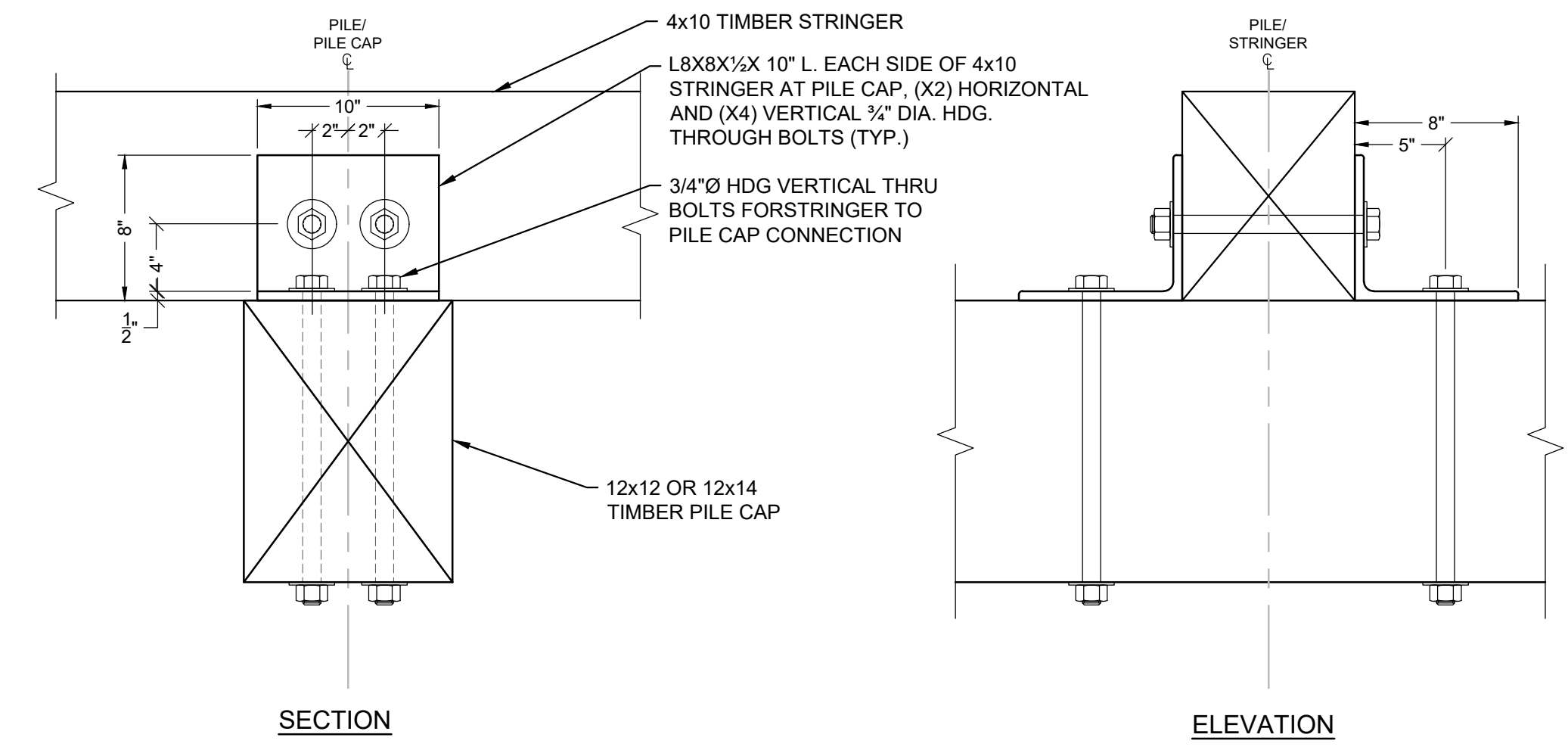
2 TIMBER PILE CAP TO GRANITE BLOCK CONNECTION DETAIL
NOT TO SCALE



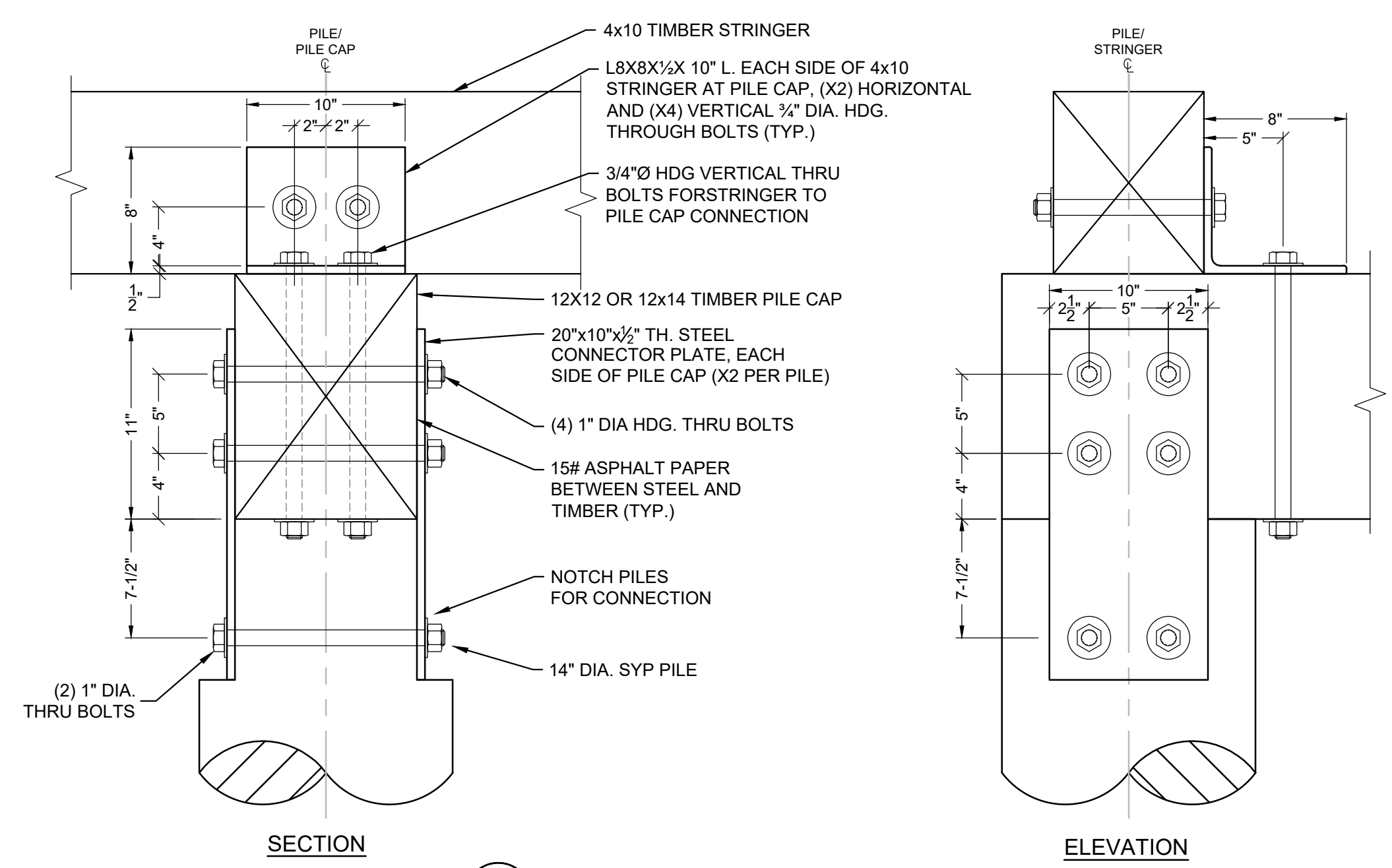
3 TIMBER PILE CAP TO SONOTUBE CONNECTION
SCALE: 1-1/2" = 1"



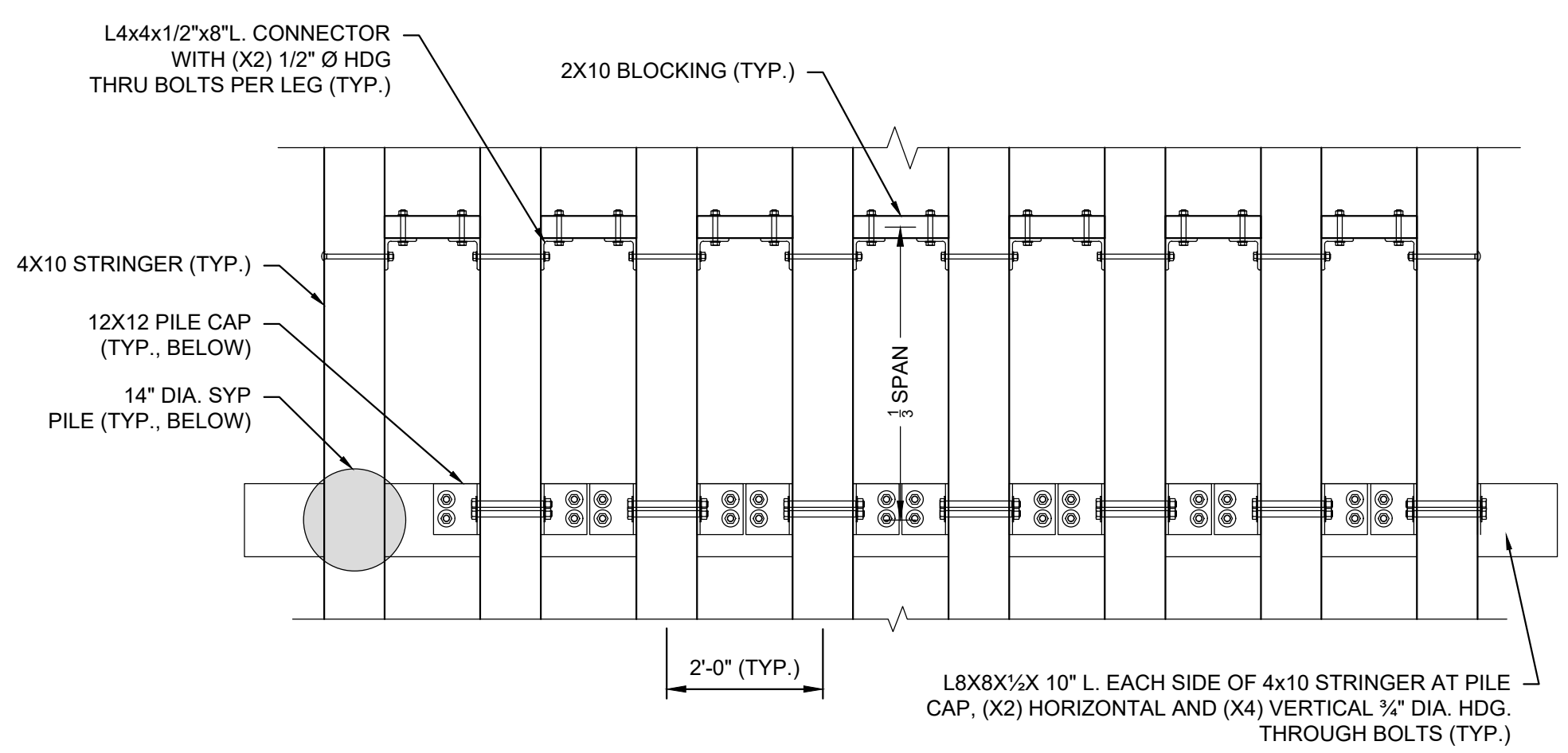
4 TIMBER RAIL - SECTION
SCALE: 1" = 1"
NOTES: SEE NOTE 4



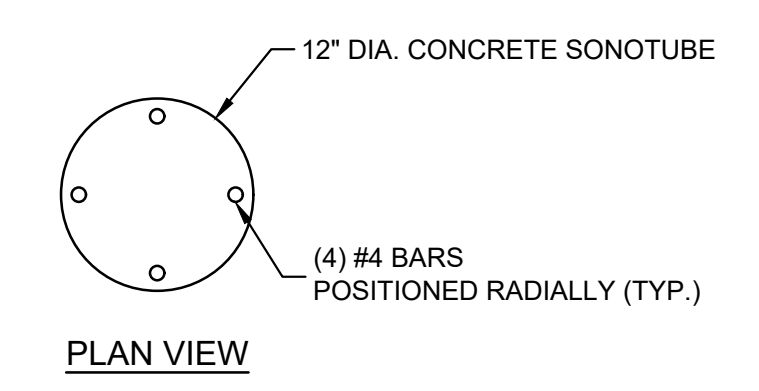
5 INTERIOR STRINGER TO PILE CAP CONNECTION DETAILS
SCALE: 1-1/2" = 1"



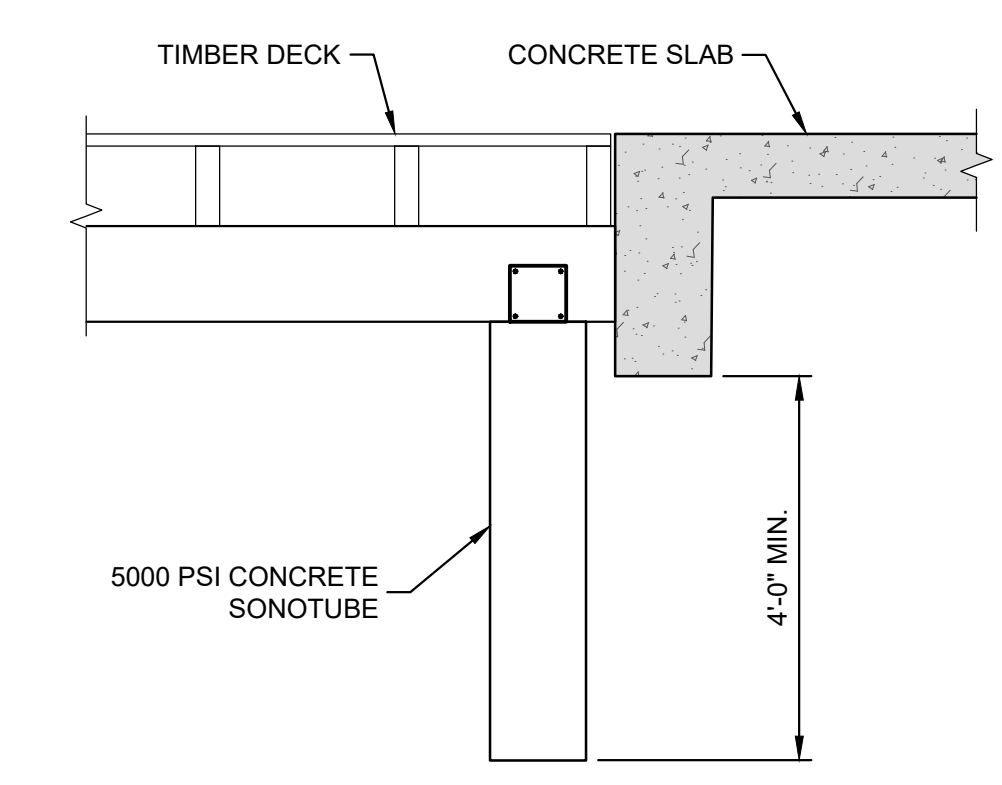
7 EXTERIOR STRINGER TO PILE CAP AND PILE CAP TO PILE CONNECTION - DETAILS
SCALE: 1-1/2" = 1"



6 STRINGER BLOCKING CONNECTION DETAIL (AT GRANITE BLOCK WALL)
SCALE: 1" = 2"



PLAN VIEW

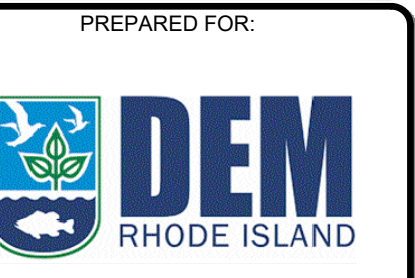


8 SONOTUBE - DETAIL
SCALE: 1" = 1"

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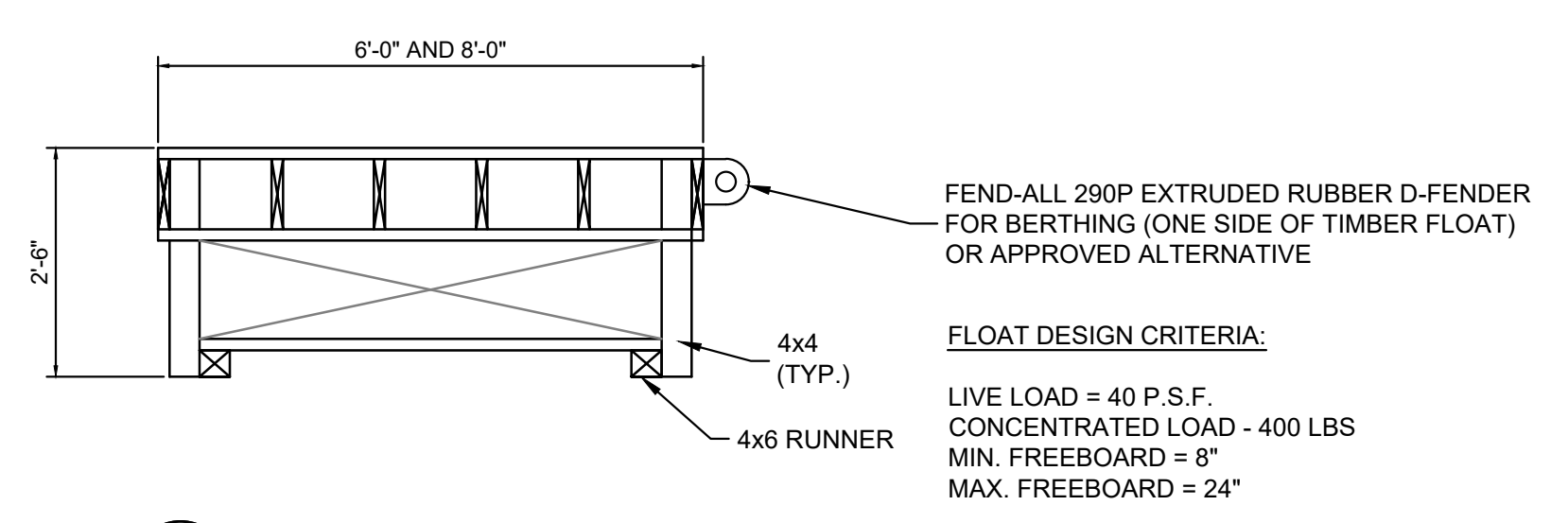
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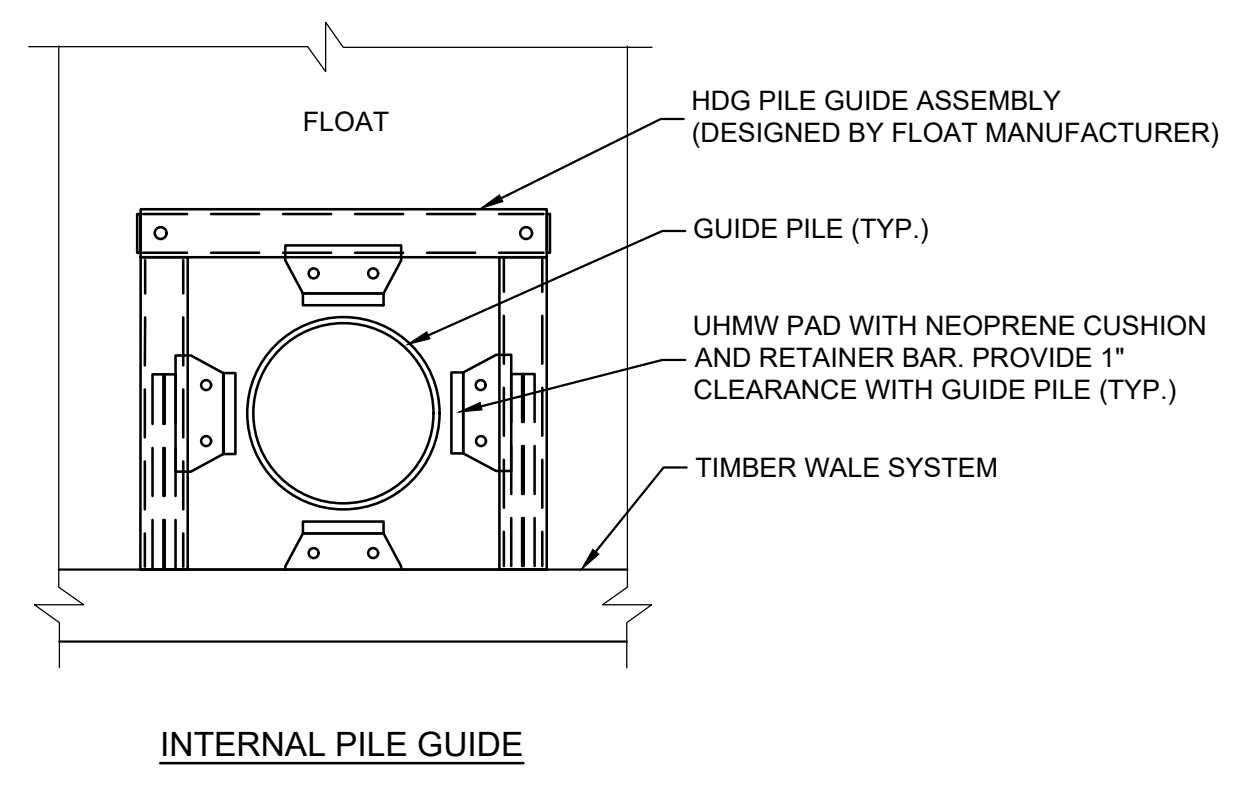
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DATE:	AUGUST, 2025
PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND
PROPOSED GANGWAY - DOCK
DETAILS / PHASE 1

DRAWING
12
SHEET NO. 11 OF 15

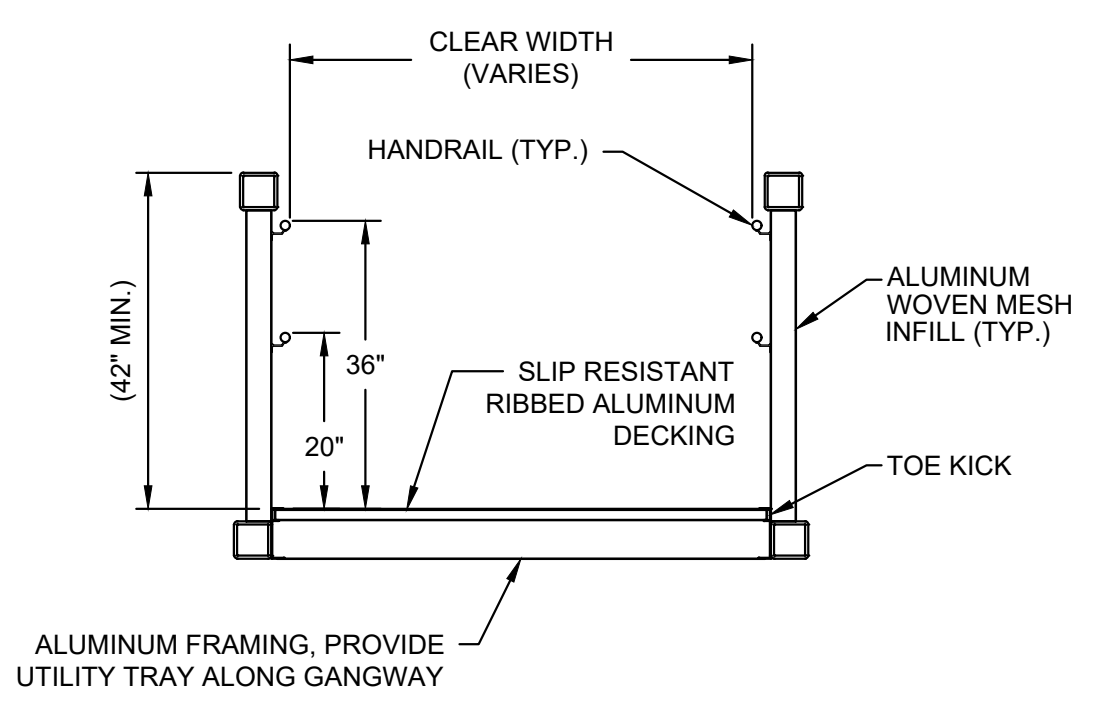


2 TIMBER FLOAT SECTION I-I
SCALE 1"=2'

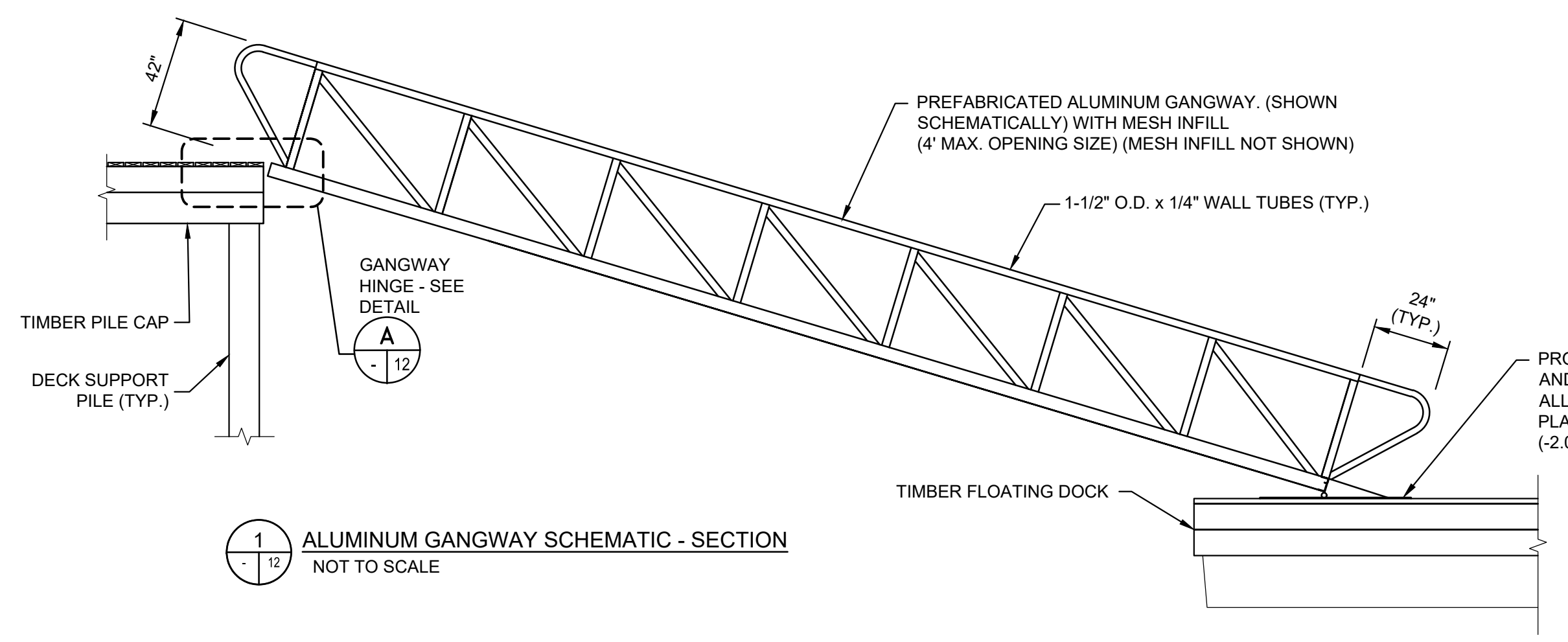


4 TYPICAL PILE GUIDE WITH UHMW PADS SCHEMATIC
SCALE: 1" = 2'

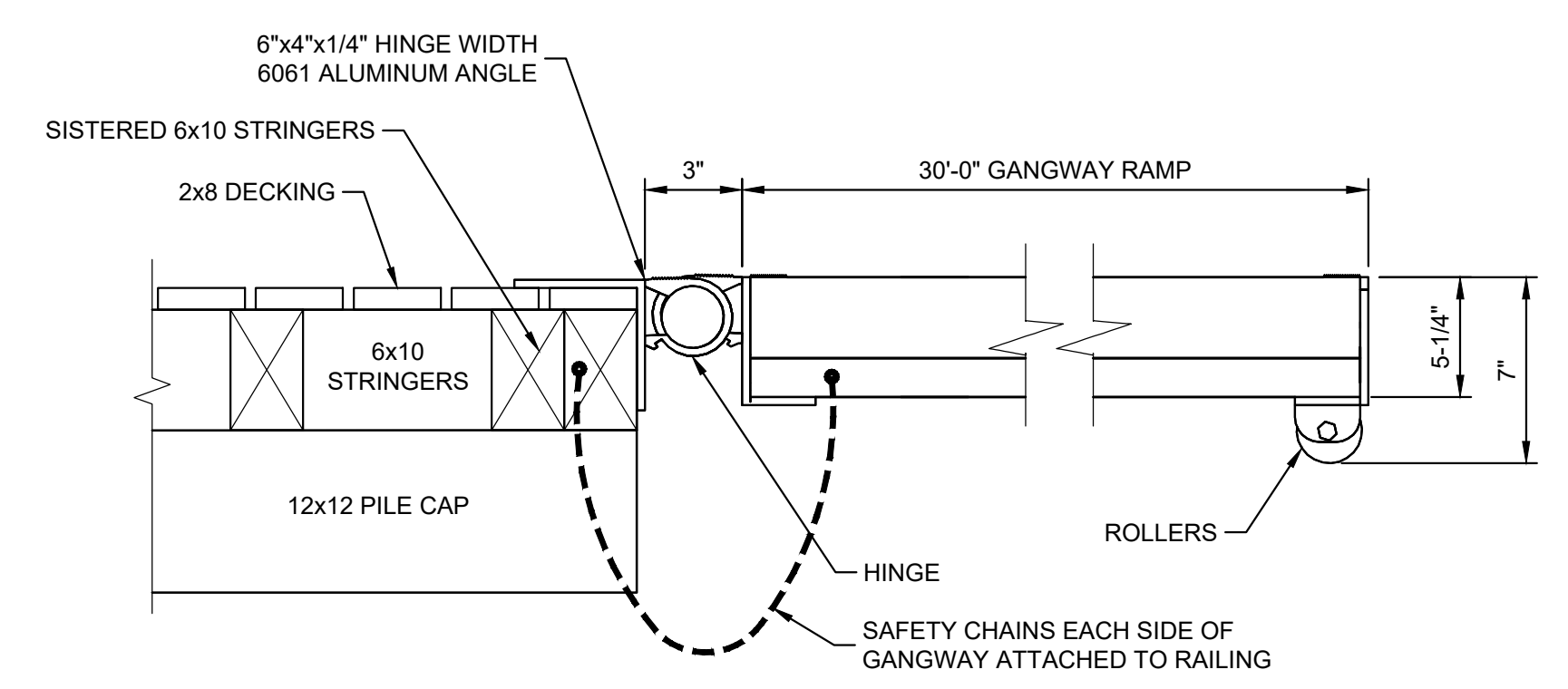
PROVIDE AND INSTALL TRANSITION PLATE AND WEAR PLATE. WEAR PLATE LENGTH TO ALLOW FOR GANGWAY AND TRANSITION PLATE TRAVEL FOR EXTREME TIDAL RANGE (-2.07' TO +1.71' NAVD88)



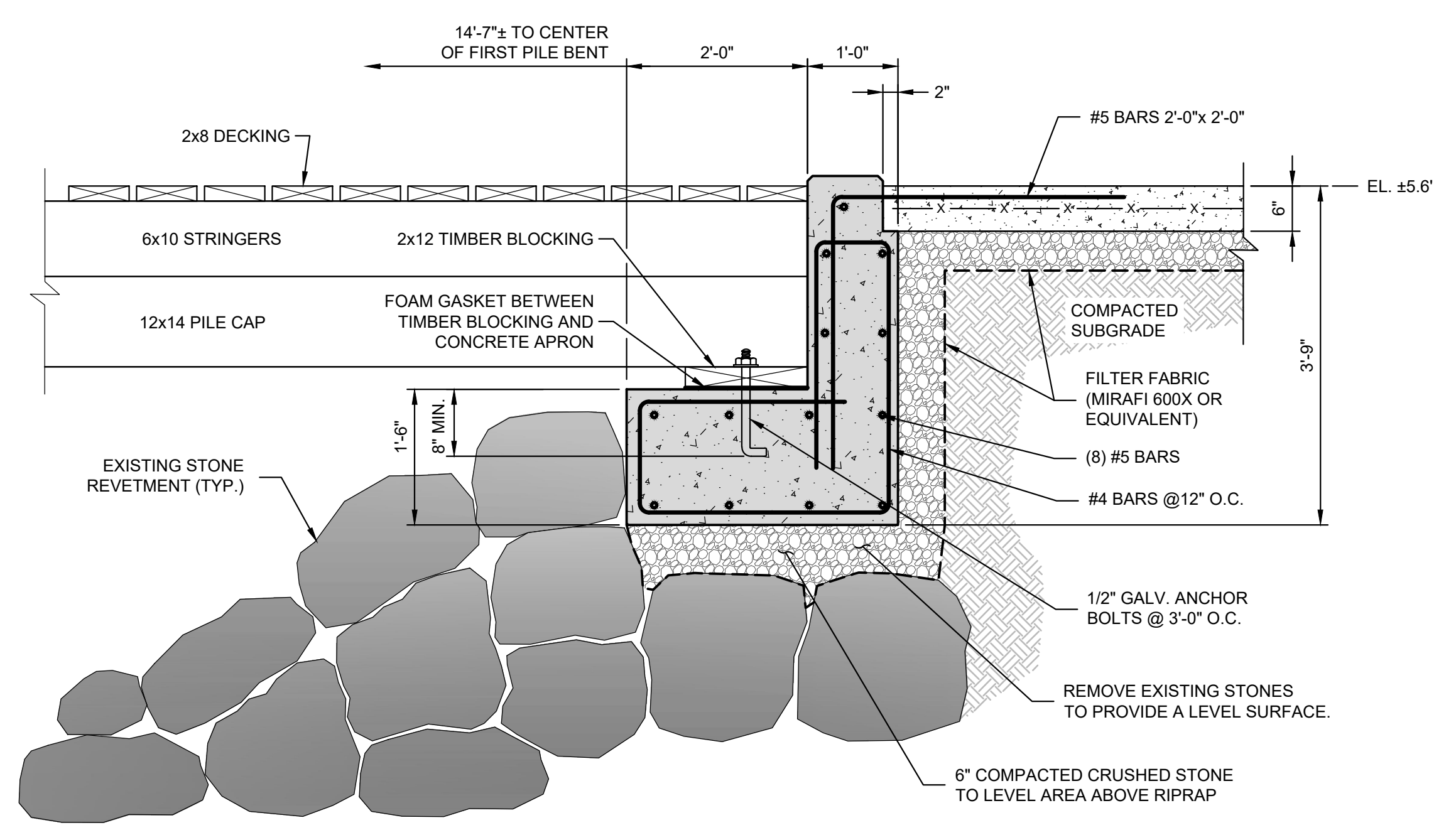
3 TYPICAL GANGWAY - SECTION
SCALE: 1" = 2'



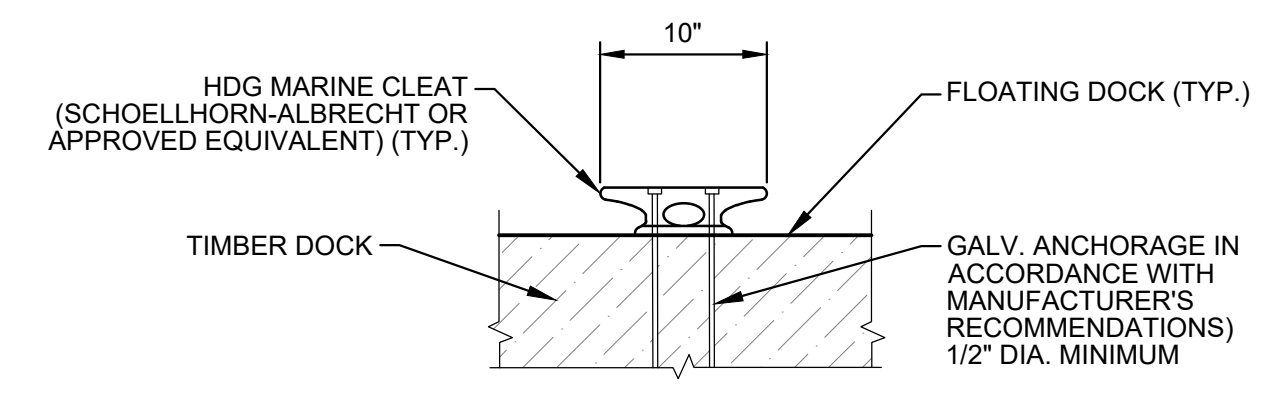
1 ALUMINUM GANGWAY SCHEMATIC - SECTION
NOT TO SCALE



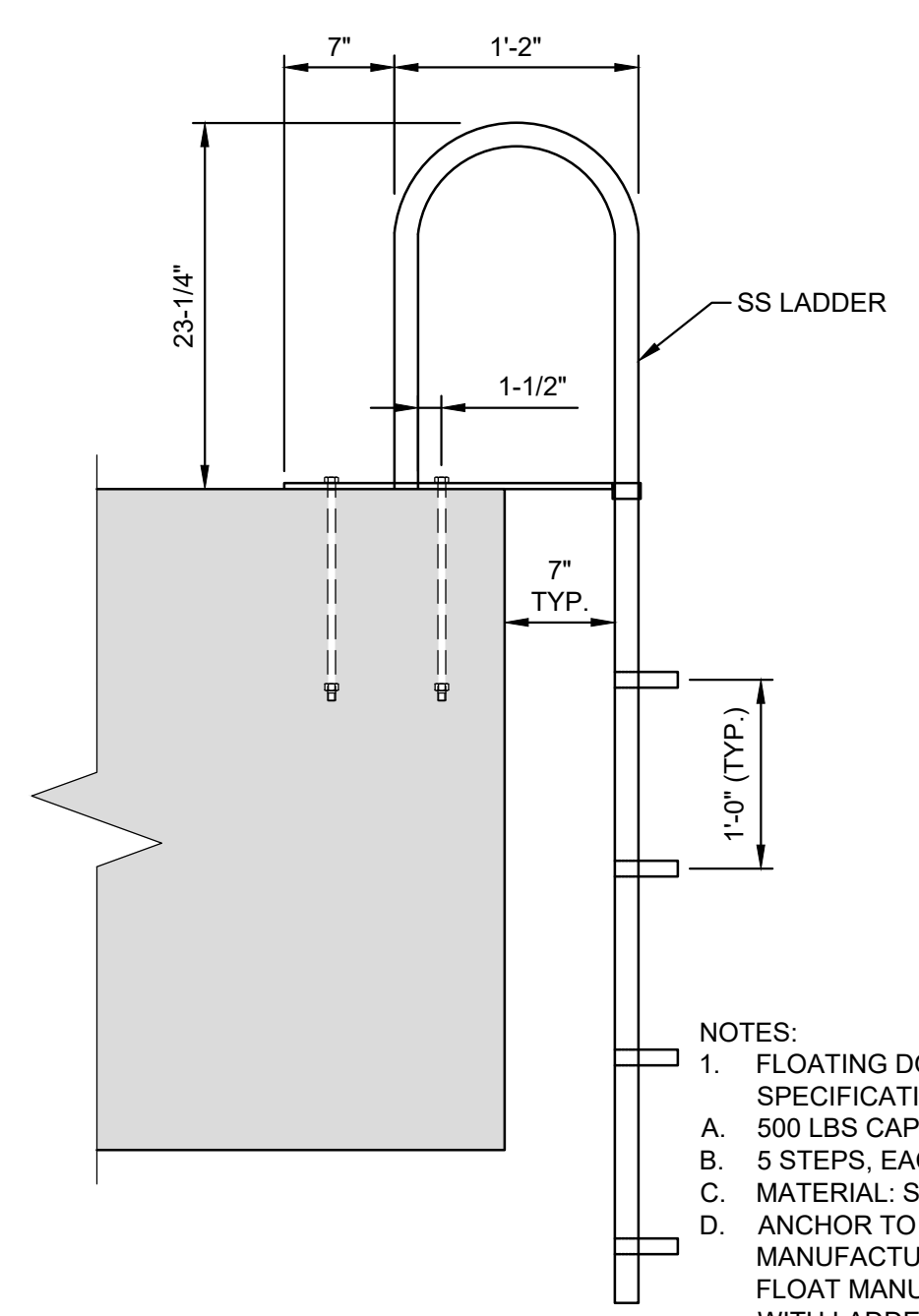
A TYPICAL GANGWAY - SECTION
NOT TO SCALE



6 DECKING / CONCRETE APRON - DETAIL
SCALE: 1" = 1'



7 TYPICAL CLEAT DETAIL
NOT TO SCALE

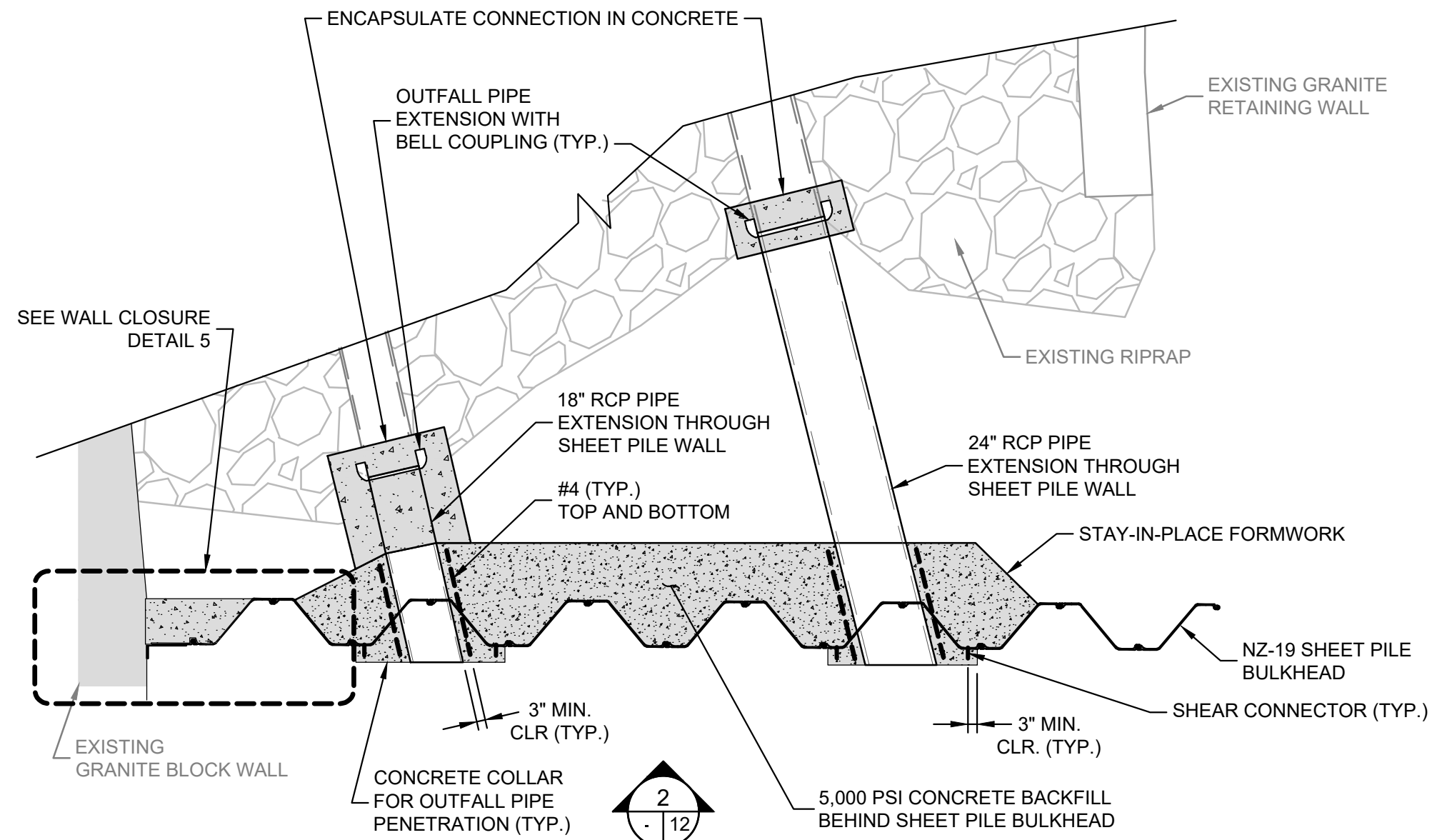


5 LADDER DETAIL
NOT TO SCALE

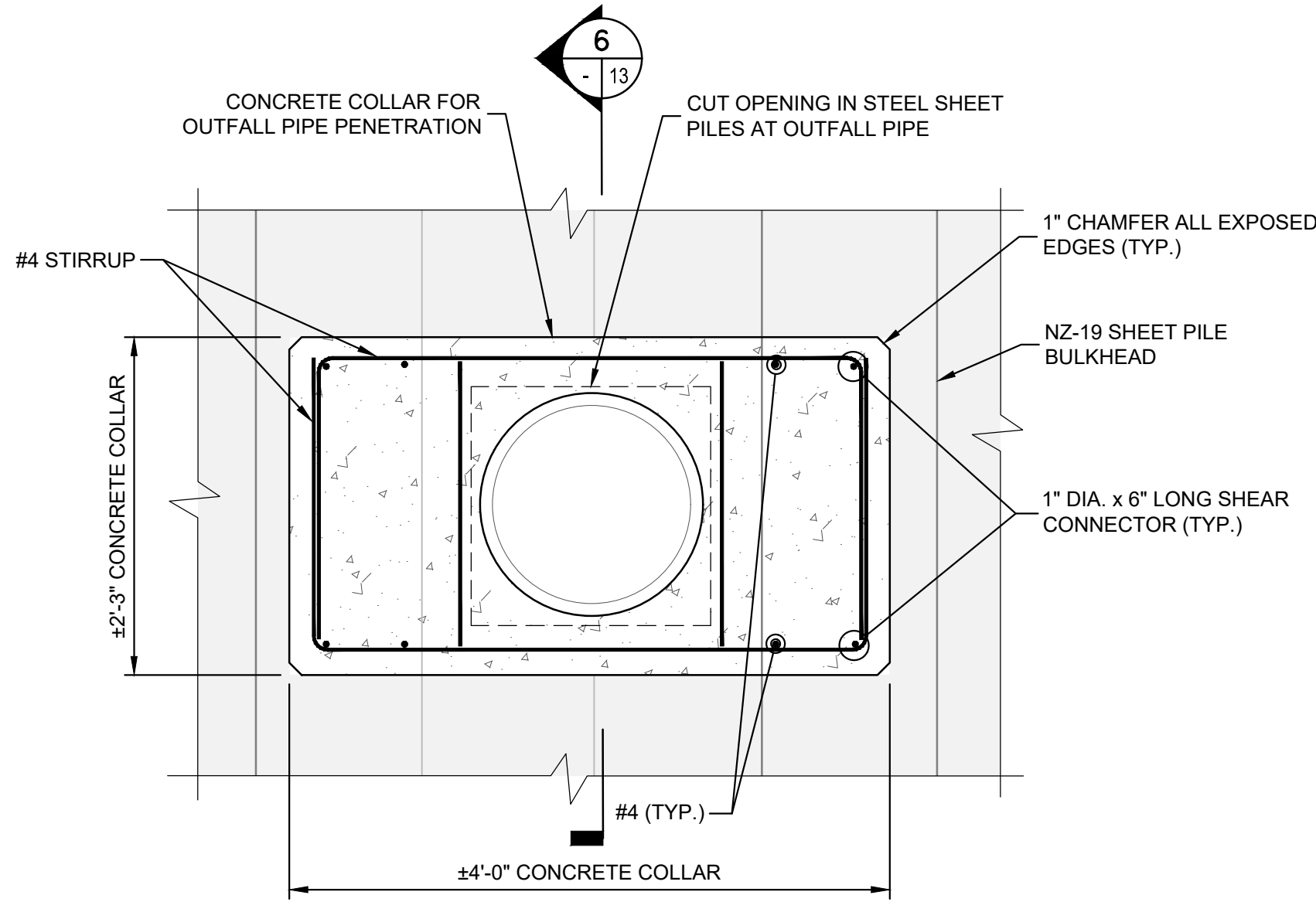
- NOTES:
- FLOATING DOCK LADDER SPECIFICATIONS:
 - 500 LBS CAPACITY
 - 5 STEPS, EACH STEP 4" WIDE
 - MATERIAL: STAINLESS STEEL
 - ANCHOR TO FLOAT PER MANUFACTURER'S RECOMMENDATIONS. FLOAT MANUFACTURER TO COORDINATE WITH LADDER MANUFACTURER FOR ANCHORAGE DETAILS.

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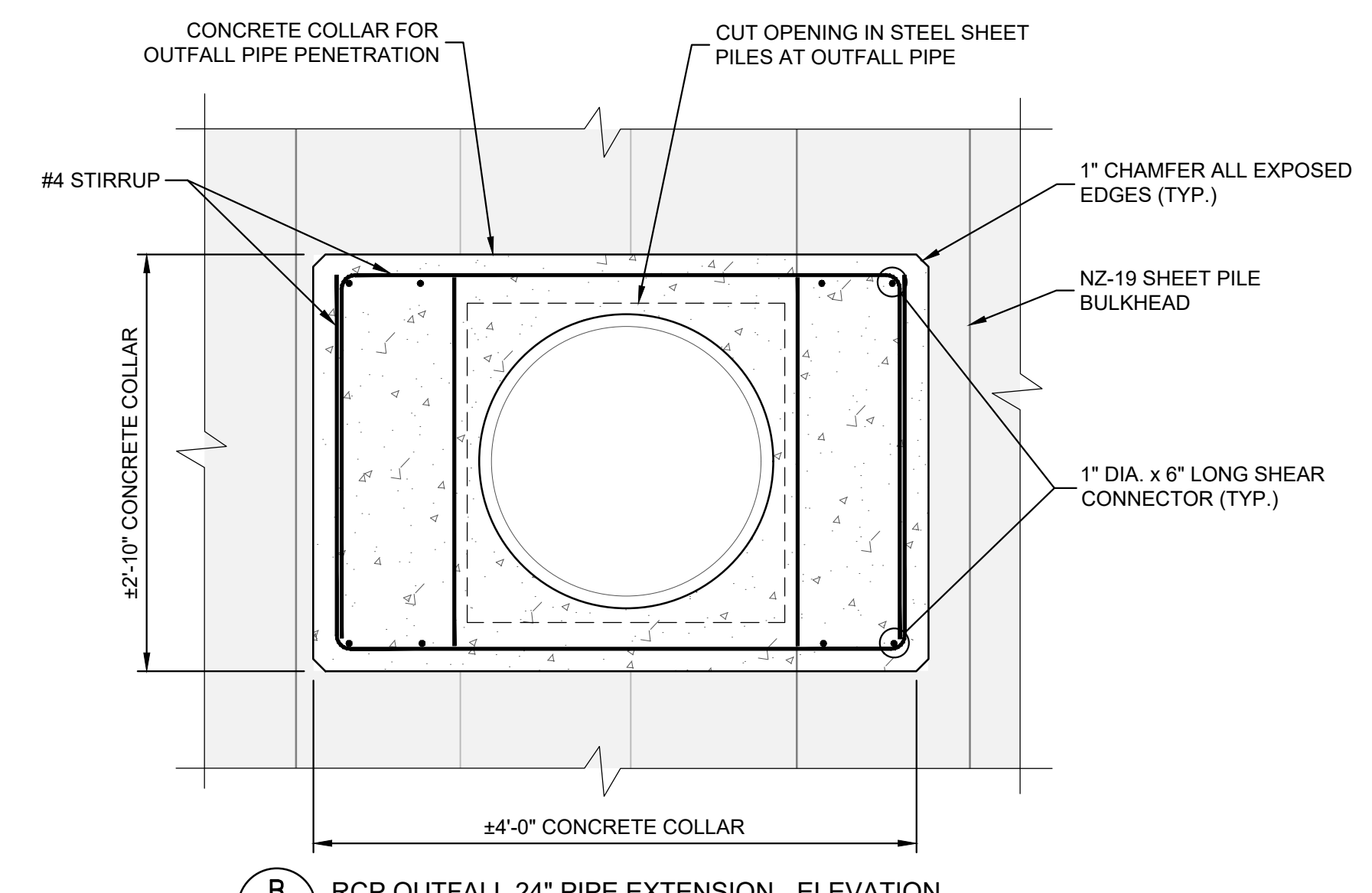
© 2025 - GZA GeoEnvironmental, Inc. GZA-J:\GEO\35429-TWS\FIGURES\CAD\DWGS\CURRENT\PLANS\90% DESIGN SET\35429.00_13_PR BULKHEAD-OUTFALL DETAILS.DWG 13 August 25, 2025 GARY BASTIEN



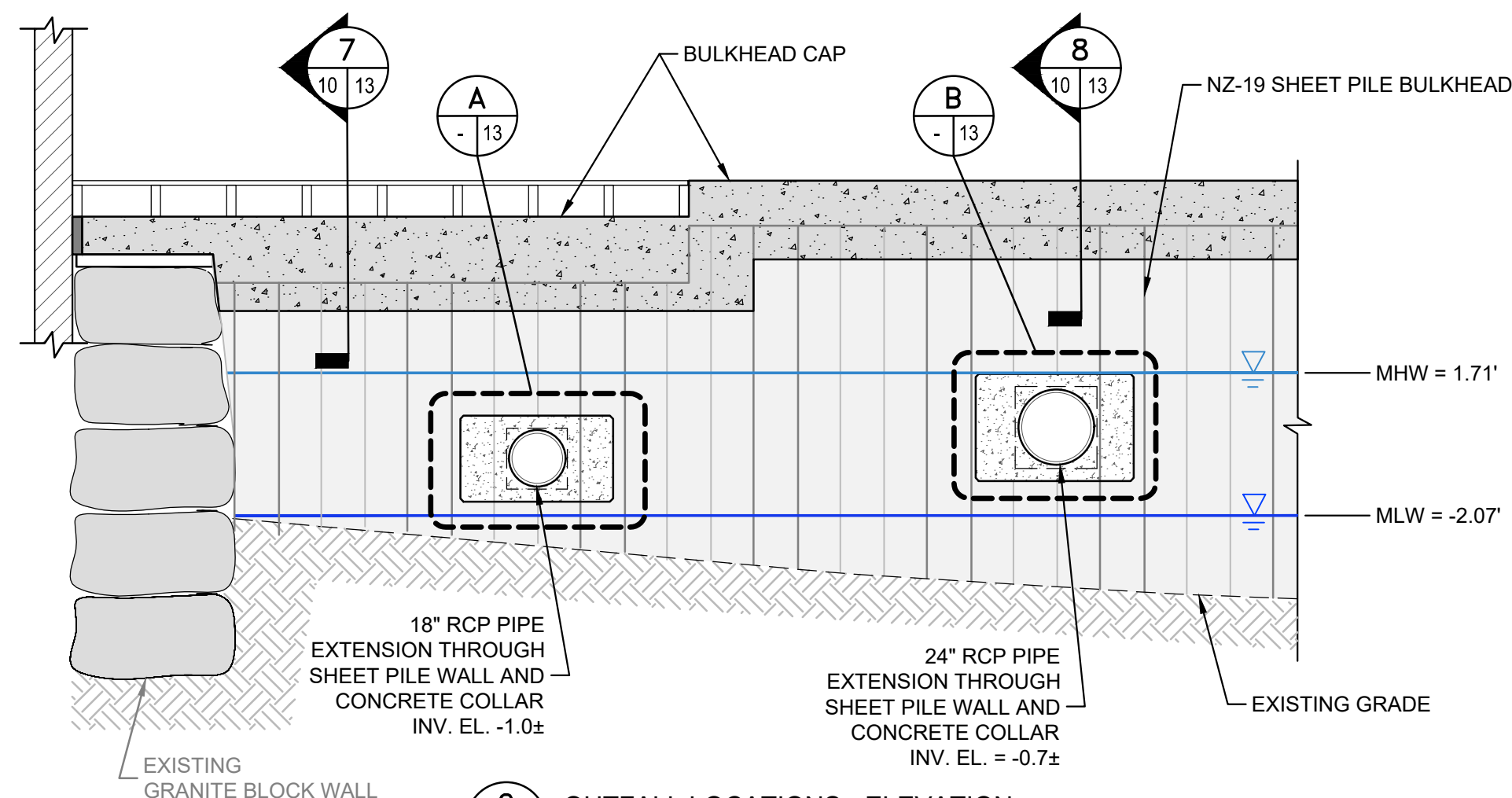
1 OUTFALL LOCATIONS - PARTIAL PLAN
SCALE: 1"=4'



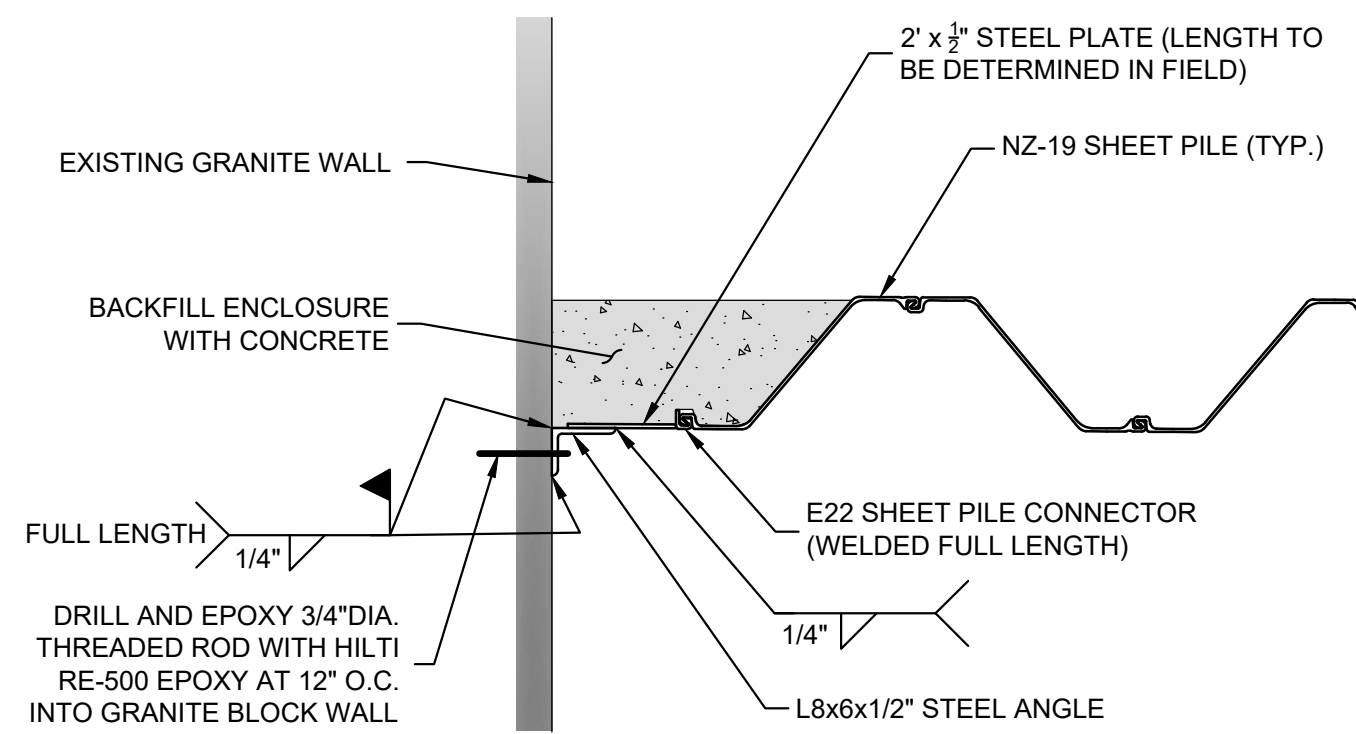
5 BULKHEAD CLOSURE AT GRANITE BLOCK WALL
SCALE: 1"=1'



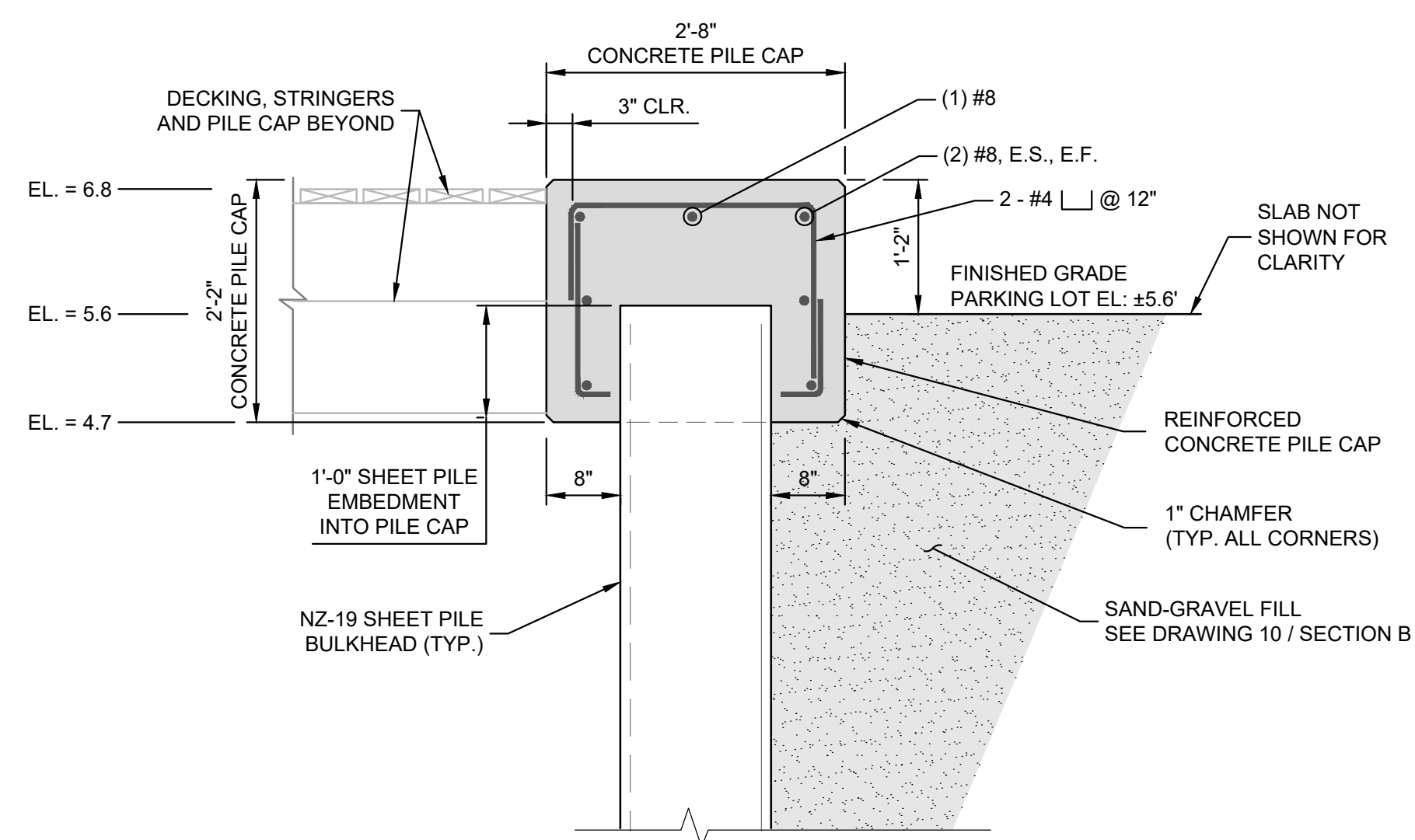
6 TYPICAL RCP EXTENSION THROUGH BULKHEAD - DETAIL
SCALE: 75"=1'



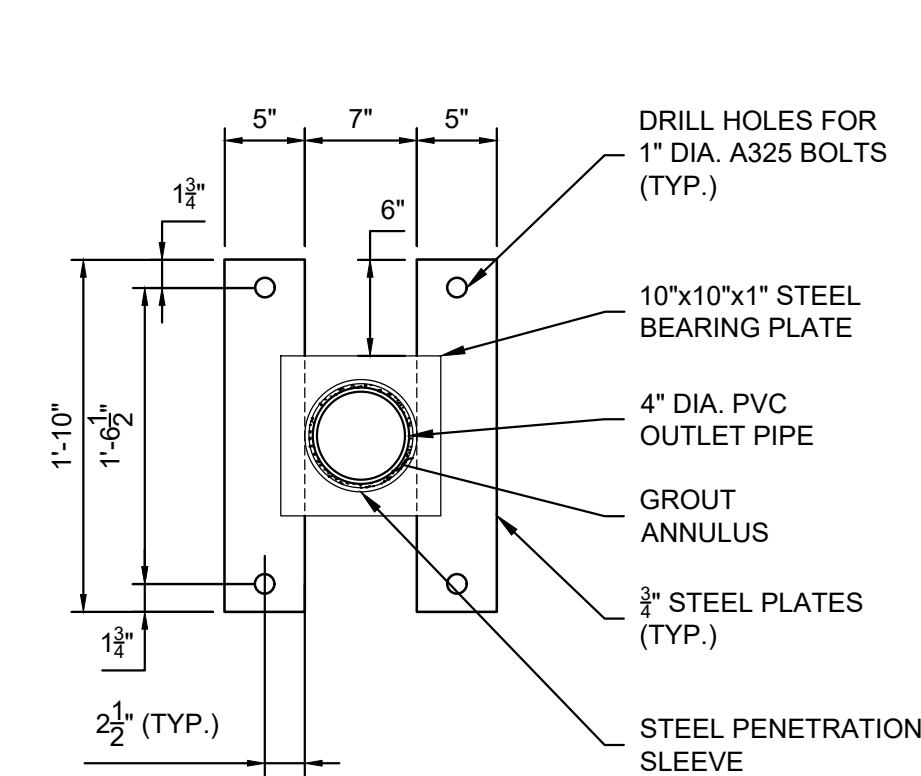
2 OUTFALL LOCATIONS - ELEVATION
SCALE: 1"=4'



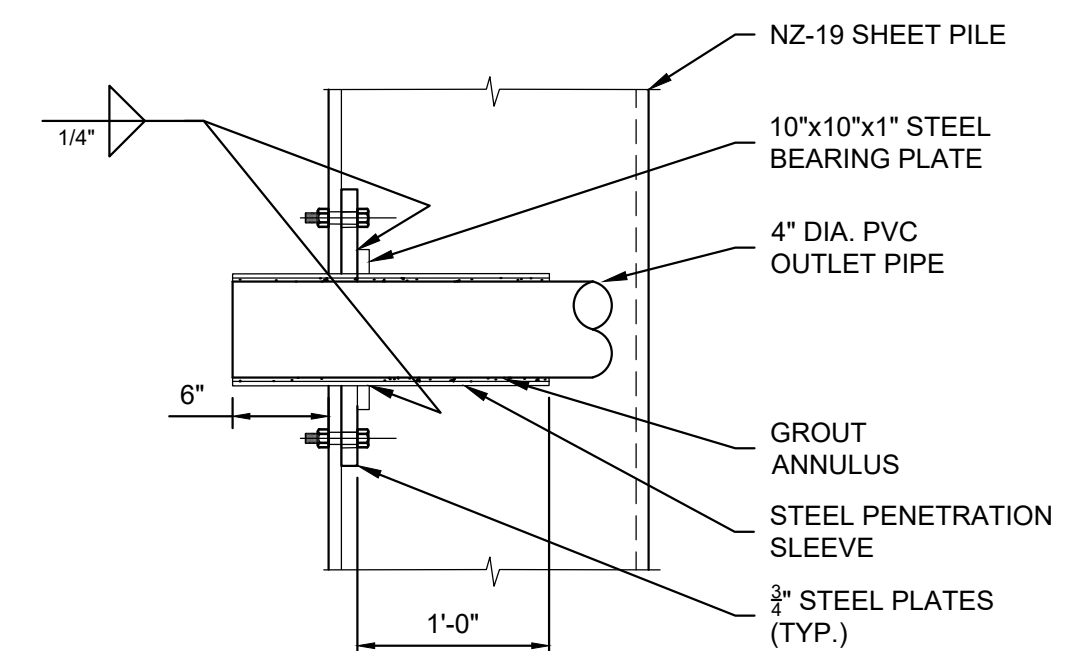
7 SECTION - CONCRETE BULKHEAD CAP WITH CURB
SCALE: 75"=1'



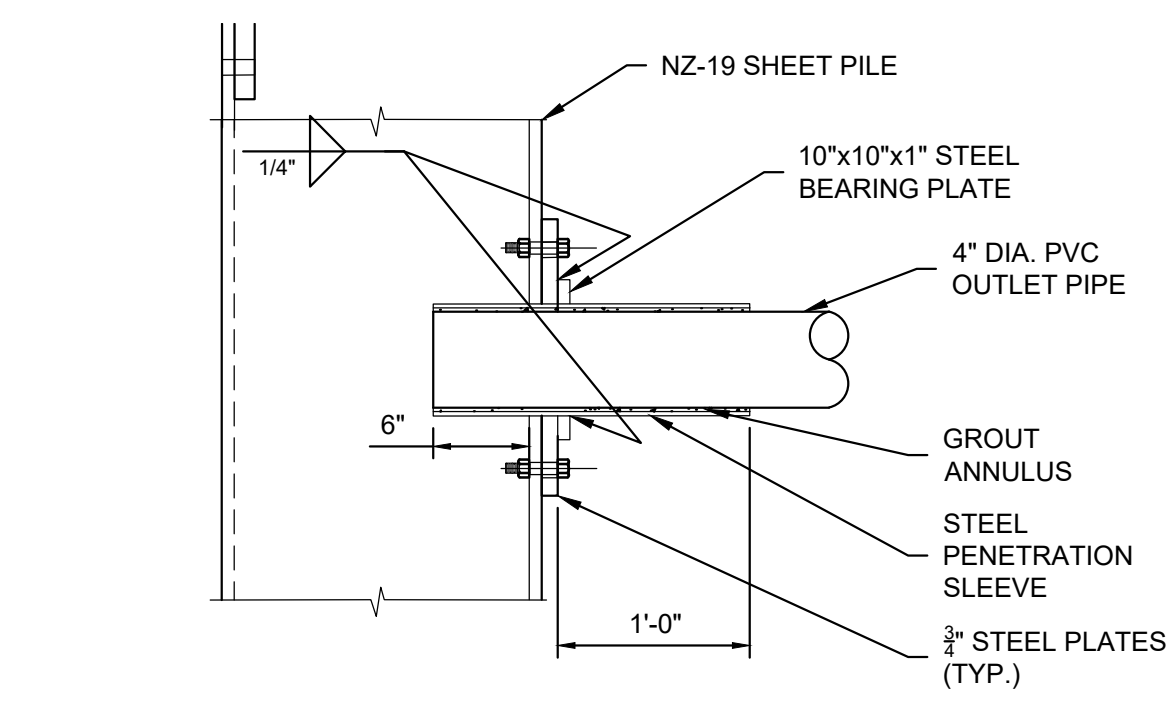
8 SECTION - CONCRETE BULKHEAD CAP
SCALE: 75"=1'



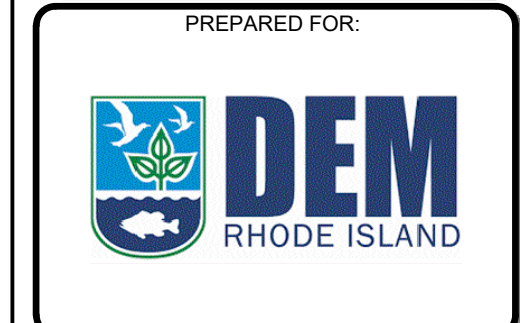
9 DETAIL - PVC OUTFALL PENETRATION
SCALE: 1"=1'



10 DETAIL - PVC OUTFALL PENETRATION
SCALE: 1"=1'



11 DETAIL - PVC OUTFALL PENETRATION
SCALE: 1"=1'



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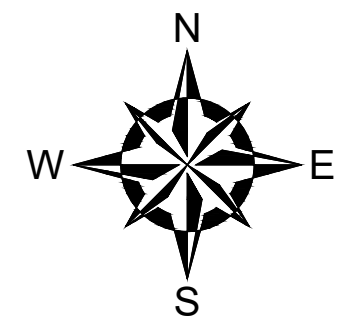
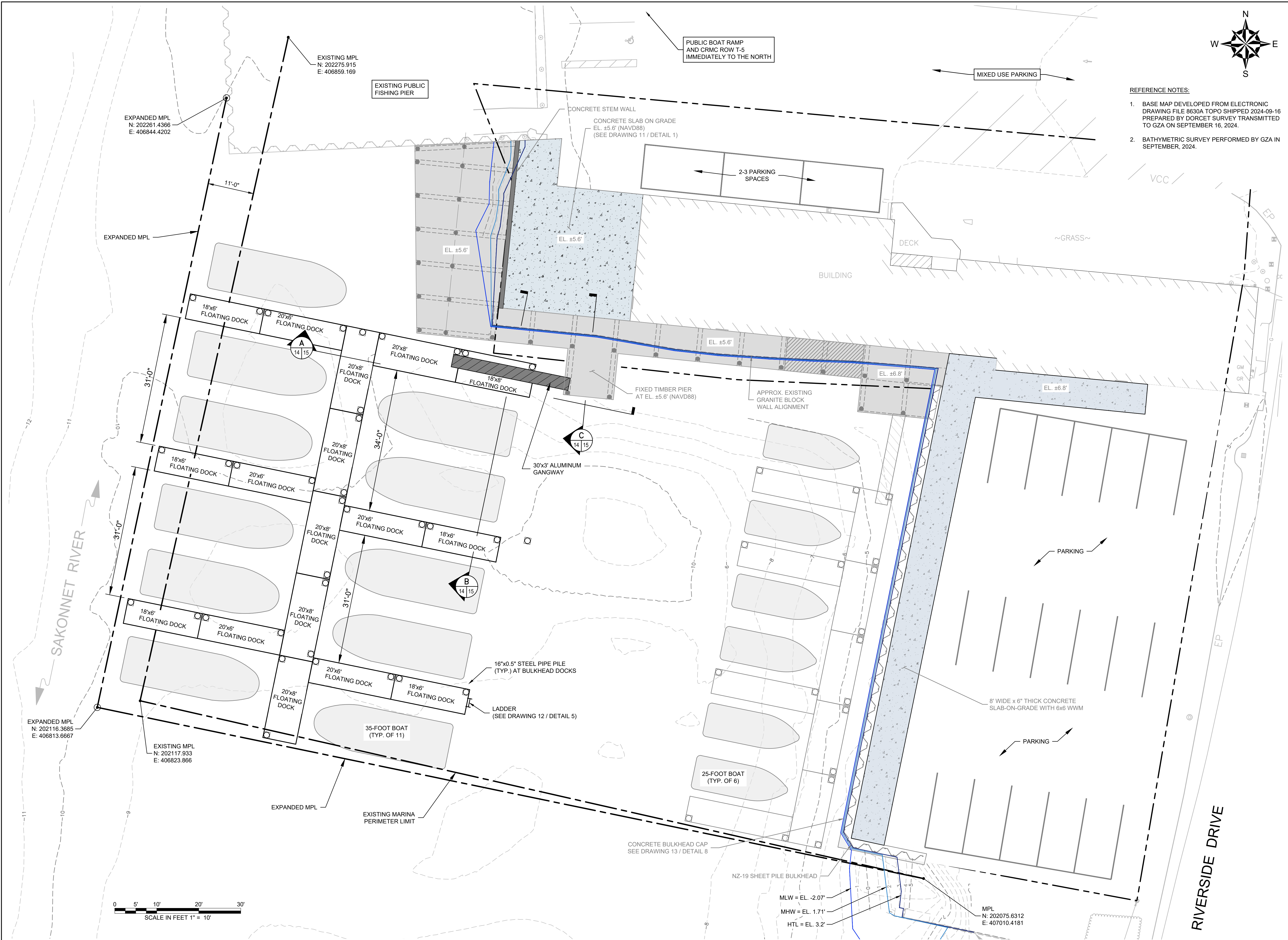
PROJECT NO:	35429.00
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PROJECT MGR:	TWS
DESIGNED BY:	JTF
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REVIEWED BY:	MJP
SCALE:	AS NOTED
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CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

**PROPOSED BULKHEAD - OUTFALL
DETAILS / PHASE 1**

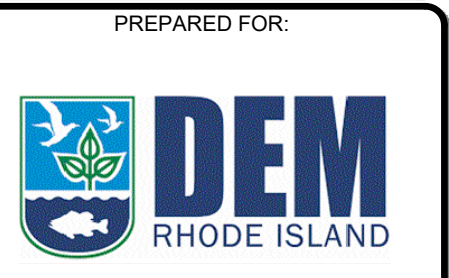
DRAWING
13
SHEET NO. 13 OF 15

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REFERENCE NOTES:

1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DORCET SURVEY TRANSMITTED TO GZA ON SEPTEMBER 16, 2024.
2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024.



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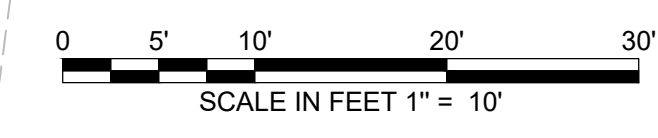
REV	DESCRIPTION	DATE	BY

PROJECT NO:	35429.00
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REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

**PROPOSED CONDITIONS PLAN - PHASE 2
AND FINAL CONDITIONS PLAN**

DRAWING
14
SHEET NO. 14 OF 15

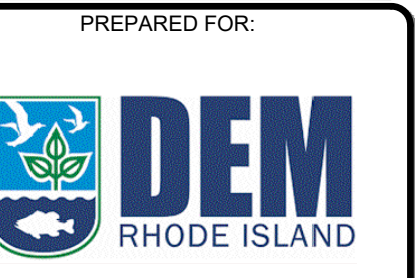


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MHW = EL. 1.71'
HTL = EL. 3.2'

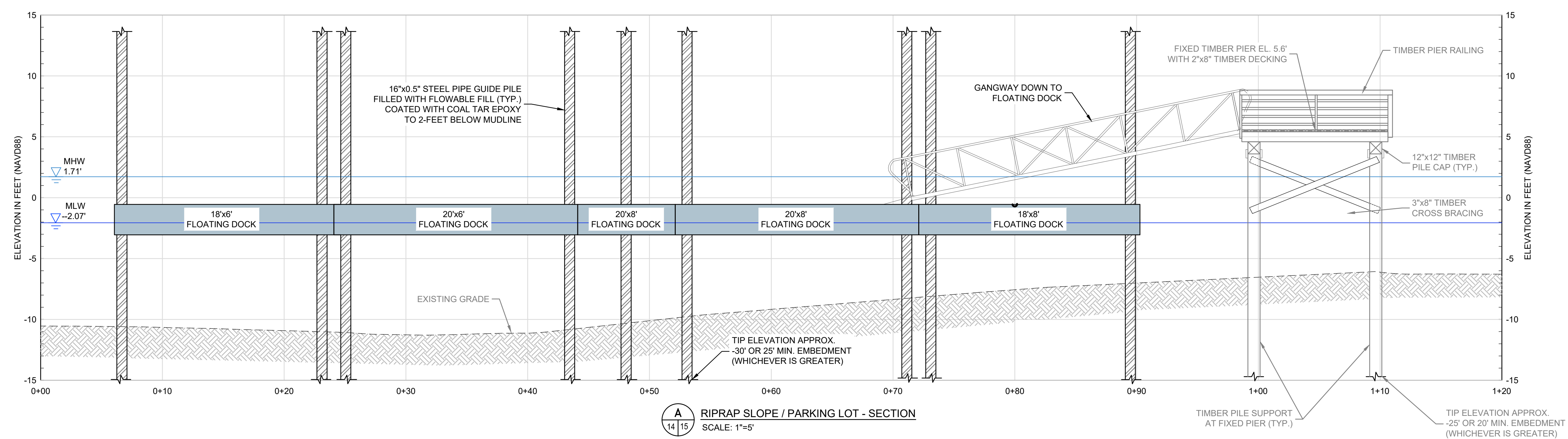
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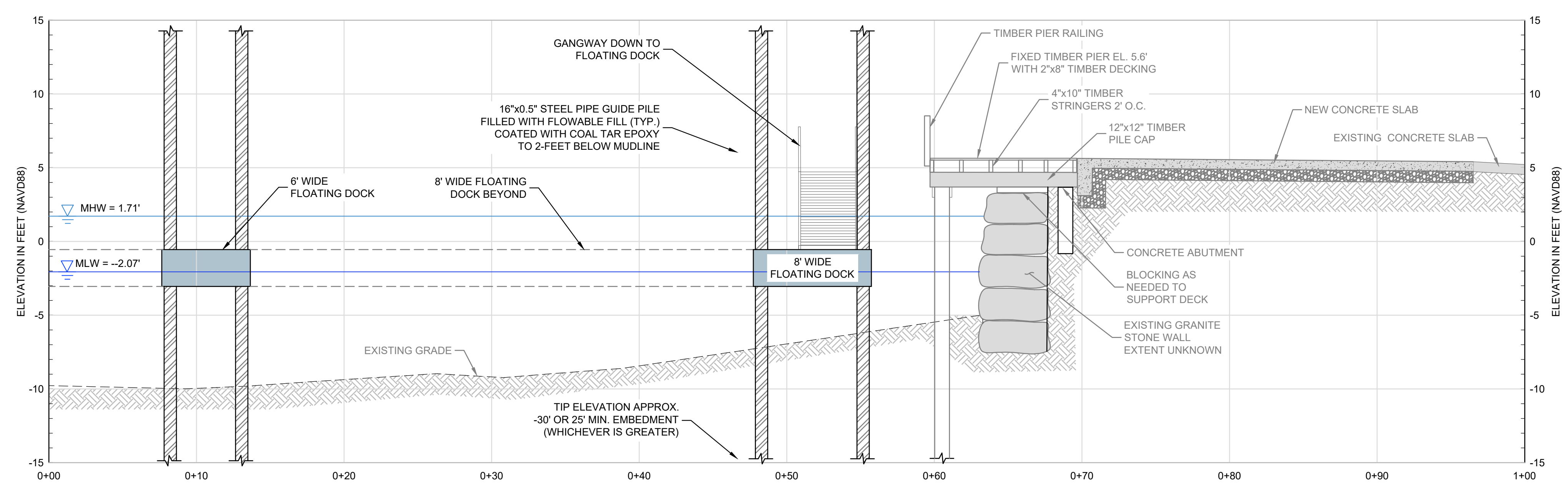
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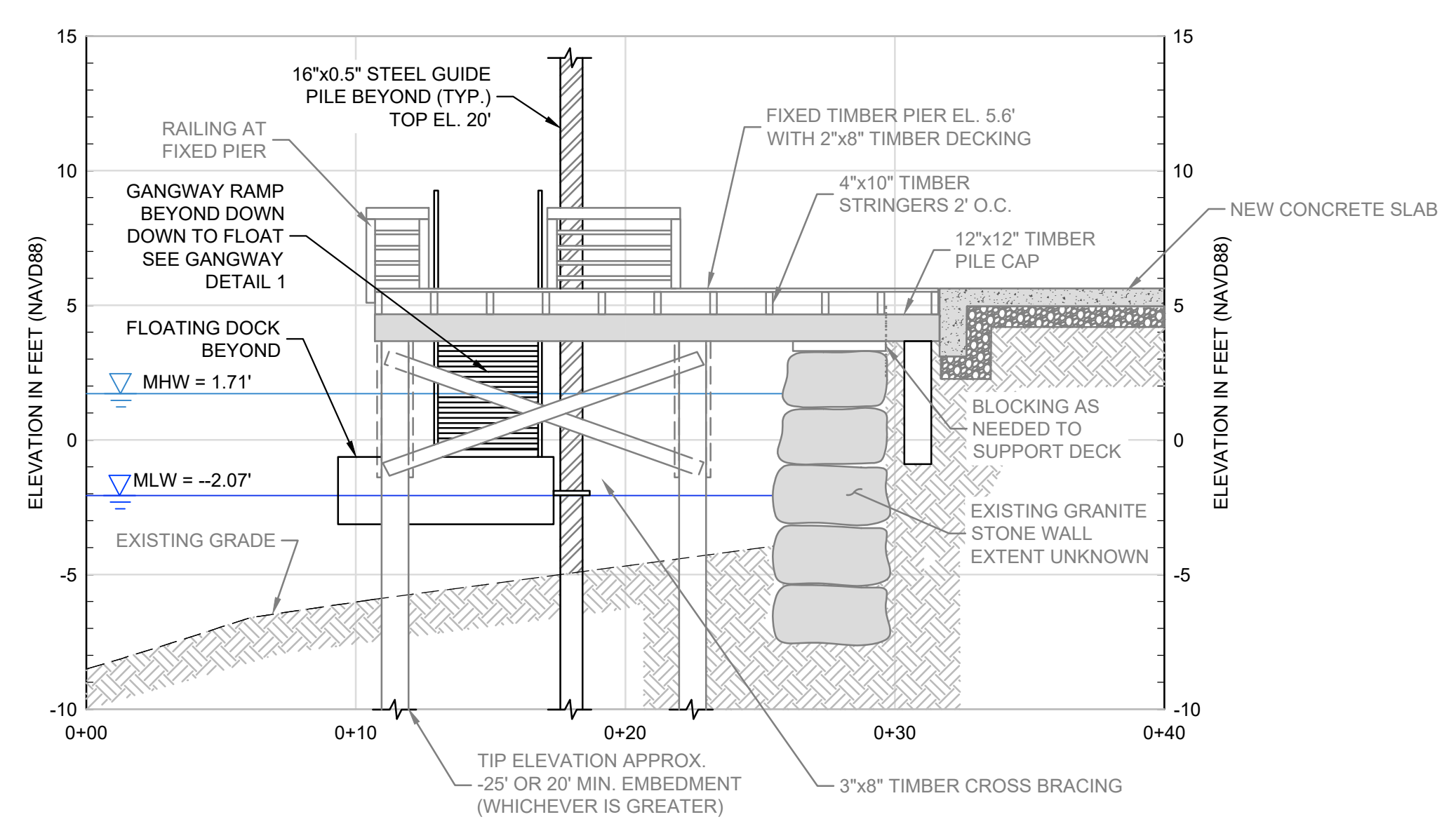
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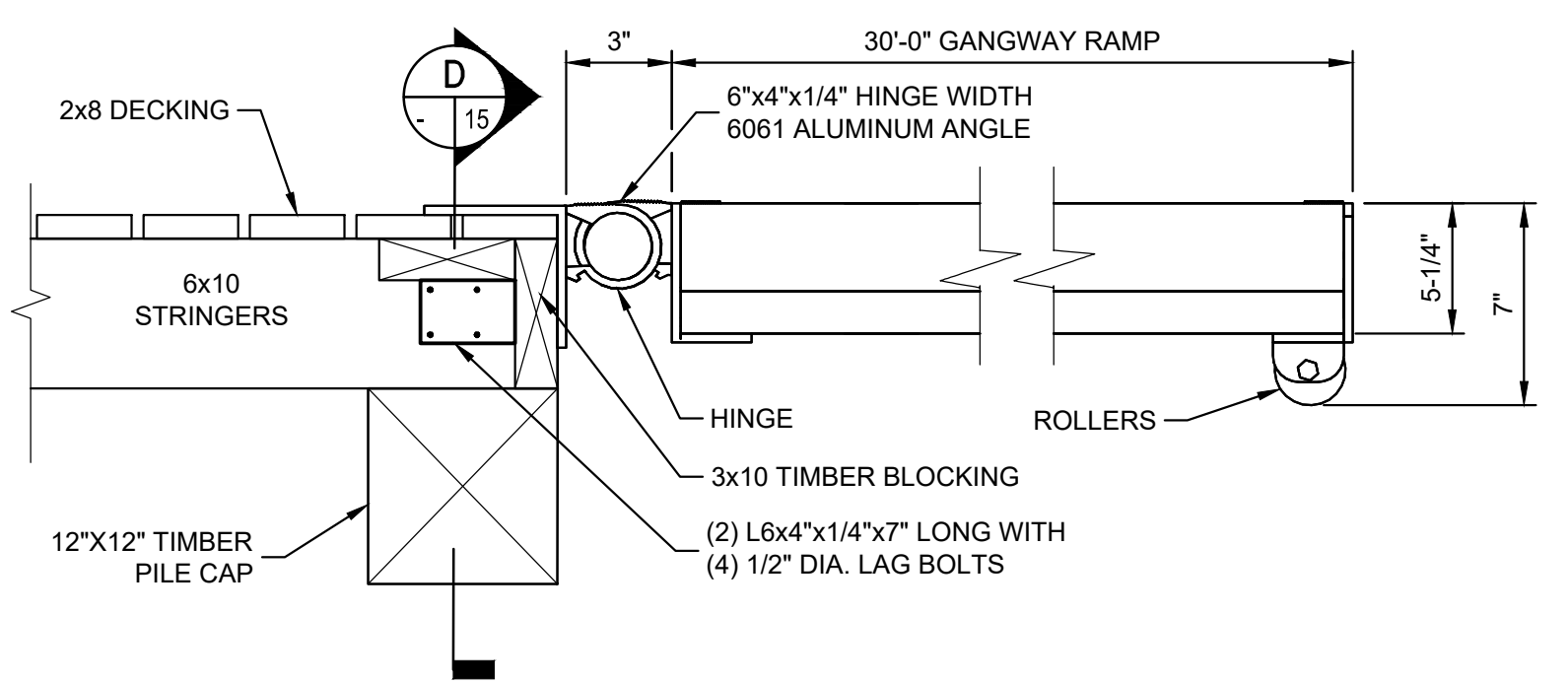
A RIPRAP SLOPE / PARKING LOT - SECTION
SCALE: 1"=5'



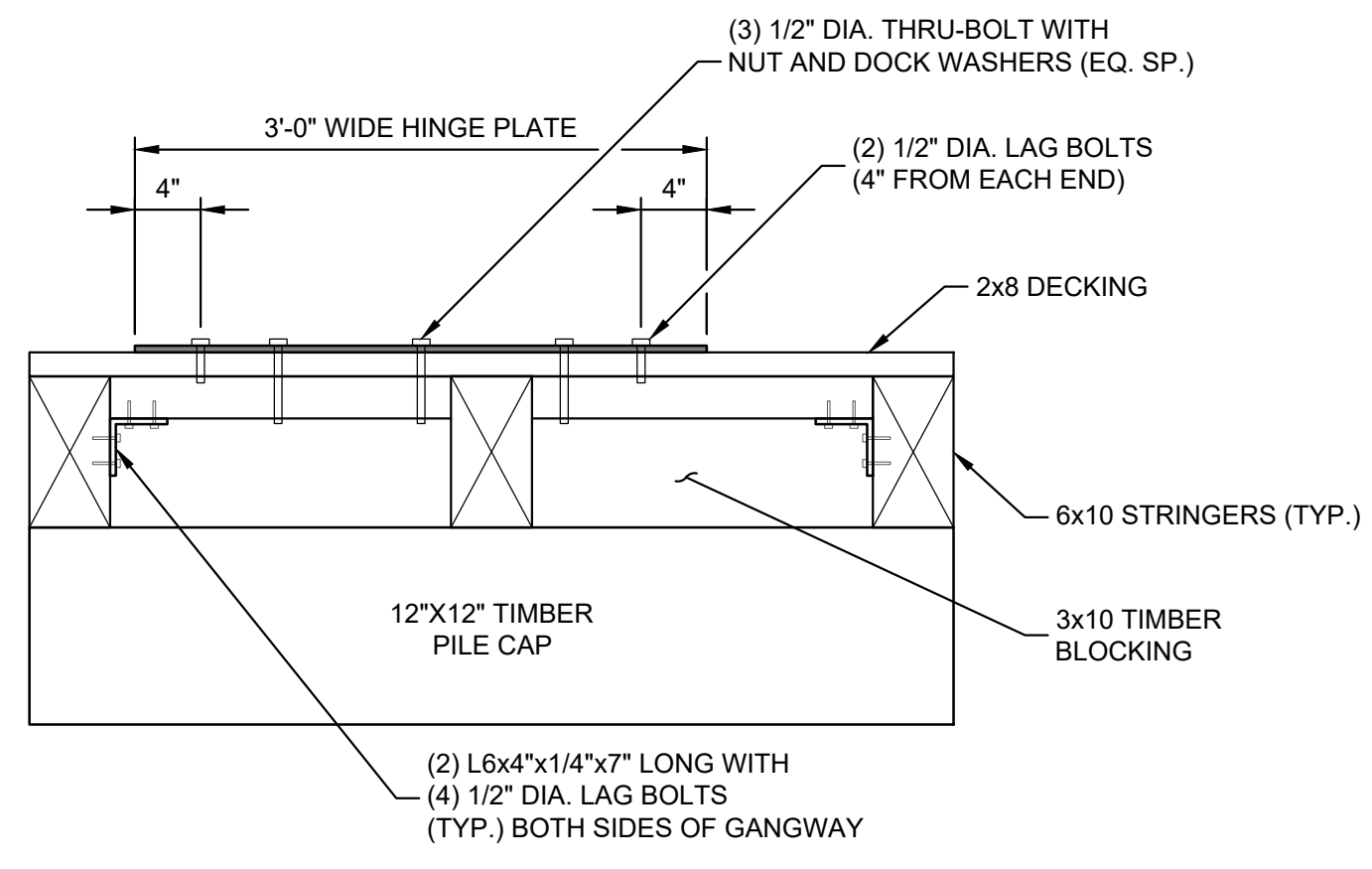
B DOCK PLATFORM - SECTION
SCALE: 1"=5'



C GRANITE BLOCK WALL - SECTION
SCALE: 1"=5'



1 TYPICAL GANGWAY - DETAIL
NOT TO SCALE



D TYPICAL GANGWAY CONNECTION - SECTION
NOT TO SCALE

- NOTE:
1. TOP OF PILE CUTOFF ELEVATION = 20- FEET
 2. ALL STEEL PIPE GUIDE PILES TO BE CAPPED WITH A PVC PILE CAP.



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REV.	DESCRIPTION	DATE	BY

PROJECT NO: 35429.00
DATE: AUGUST, 2025
PROJECT MGR: TWS
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CHECKED BY: TWS
REVIEWED BY: MJP
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REVISION NO.

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PROPOSED CONDITIONS SECTIONS
PHASE 2

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